

DESIGN AND CONSTRUCTION REPORT

Highway 401 Interchange  
& Highway Improvements  
from Hespeler Road to Townline Road  
City of Cambridge

Phase 2 (GWP 3222-15-00)



DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway  
Improvements from Hespeler Road to  
Townline Road, City of Cambridge –  
Phase II (GWP 3222-15-00)

Prepared by:  
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600-171 Queens Avenue  
London ON N6A 5J7

March 2019



**DESIGN AND CONSTRUCTION REPORT**  
**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM**  
**HESPELER ROAD TO TOWNLINE ROAD**  
**CITY OF CAMBRIDGE – PHASE II**

GWP 3222-15-00



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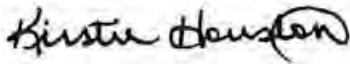
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Scott Howard  
Senior Project Manager  
Ministry of Transportation

March 2019



A copy of this Phase II Design and Construction Report (DCR) has been submitted to the following office of the Ontario Ministry of the Environment, Conservation and Parks to fulfill the requirements of the Ontario Ministry of Transportation *Class Environmental Assessment for Provincial Transportation Facilities (2000)*:

Ministry of Environment, Conservation and Parks  
Southwest Region  
733 Exeter Road, London, ON N6E 1L3

This DCR is available for a 30-day review period from **Tuesday, March 12, 2019** to **Wednesday, April 10, 2019**, during regular business hours only, at the following three review locations:

**City of Cambridge City Hall**

Clerk's Office, 2<sup>nd</sup> Floor  
50 Dickson Street  
Cambridge, ON

**Ontario Ministry of Transportation**

West Region, Front Lobby  
659 Exeter Road  
London, ON

**Cambridge Public Library**

Idea Exchange - Hespeler Branch  
5 Tannery Street East  
Cambridge, ON

A digital copy of this report is also available on the project website at: [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**COMMENTS**

Comments during the review period may be submitted via the project website or by contacting one of the following individuals:

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E-mail: scott.howard@ontario.ca

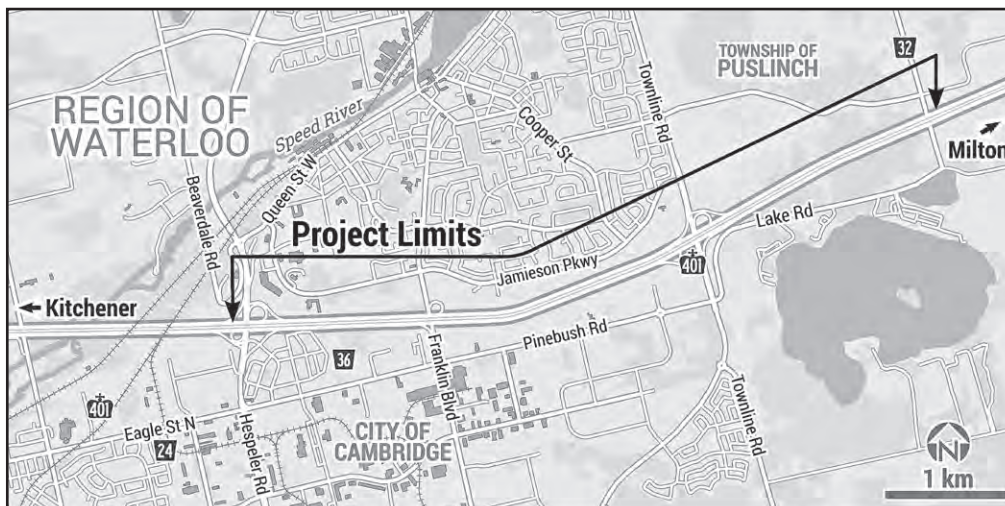


# NOTICE OF SUBMISSION - DESIGN AND CONSTRUCTION REPORT

## Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge - Phase II (GWP 3222-15-00) Detailed Design and Class Environmental Assessment Study

### THE STUDY

The **Ontario Ministry of Transportation (MTO)** has retained **Stantec Consulting Ltd.** to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo as shown on the key plan below. Since the issuance of the Notice of Study Commencement in May 2014 (formerly GWP 3060-11-00), the project has been divided into two phases to support construction staging. Phase I construction, GWP 3125-07-00, was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard.



Phase II, GWP 3222-15-00, includes:

- Widening of Highway 401 to ten lanes, including Managed Lanes (previously named High Occupancy Vehicle lanes)
- Reconstruction of the Highway 401 interchange at Hespeler Road
- Replacement of the Hespeler Road bridge (with provision for pedestrian/cyclist travel across Highway 401, in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge)
- Reconstruction of ramp tie-ins at Franklin Boulevard and Townline Road interchanges
- Retaining walls as needed
- Expansion of the commuter parking lot at the Townline Road interchange
- Drainage improvements, illumination, overhead signs and traffic signals
- A section of new noise wall constructed along the south side of Highway 401, west of Franklin Boulevard
- Rehabilitation of the Wellington Road 32 Underpass

Phase II construction will start in the fall 2019 to 2022, subject to completion of the Study, funding and approvals.

### PROCESS

This project was approved in November 2013 following the preparation and filing of a Transportation Environmental Study Report (TESR) in November 2012 (GWP 8-00-00).

This Phase II Design and Construction Report was prepared under the Ministry of Transportation's *Class Environmental Assessment (EA) for Provincial Transportation Facilities, 2000* as a Group 'B' project. The Phase II DCR is available for a 30-day review period from **Tuesday, March 12, 2019 to Wednesday, April 10, 2019**, during regular business hours only, at the following three review locations, and on the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)):

**City of Cambridge City Hall**  
Clerk's Office, 2nd Floor  
50 Dickson Street, Cambridge, ON

**Ontario Ministry of Transportation**  
West Region, Front Lobby  
659 Exeter Road, London, ON

**Cambridge Public Library**  
Idea Exchange - Hespeler Branch  
5 Tannery Street East, Cambridge, ON

The Notice of Submission of Design and Construction Report documenting Phase I of the project (GWP 3125-07-00) was issued in November 2017.

### COMMENTS

Comments during the review period may be submitted via the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)) or by contacting one of the following individuals:

**Mr. Kevin Welker, P.Eng., ENV SP**  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue, London, ON N6A 5J7  
tel: 519-675-6652 (+0 collect)  
e-mail: kevin.welker@stantec.com

**Mr. Scott Howard**  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3rd Floor, London, ON N6E 1L3  
tel: 1-800-265-6072, ext. 519-873-4568  
e-mail: scott.howard@ontario.ca

If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.



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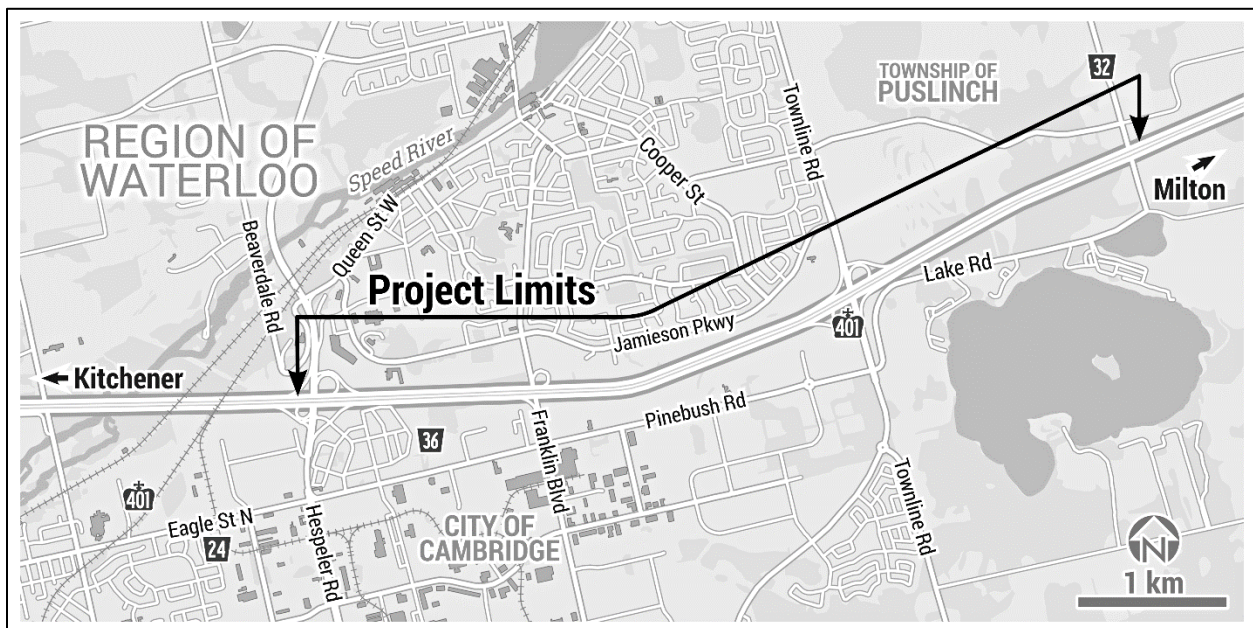
# 1.0 Project Overview

## 1.1 SUMMARY DESCRIPTION OF THE UNDERTAKING

Highway 401 from Hespeler Road to Townline Road, in the City of Cambridge, is a six-lane divided freeway with a concrete median barrier and design speed of 120 km/h with a posted speed of 100 km/h. It is part of a key transportation corridor in Ontario, linking major cities and providing a route for international trade and economic development.

In response to growing traffic congestion, MTO initiated a Preliminary Design and Environmental Assessment (EA) Study in 2012 to determine the need and timing for operational and safety improvements required on Highway 401 from 1.0 km west of Hespeler Road to the Wellington County/Halton Region boundary. The Preliminary Design activities for the overall study area formed the basis for proceeding to the Detail Design for Highway 401 from Hespeler Road to Townline Road, in the City of Cambridge, Region of Waterloo, as shown on Exhibit 1.

Exhibit 1: Location Plan



The study was carried out in accordance with the approved environmental planning process for Group 'B' projects under the MTO's Class EA for Provincial Transportation Facilities (2000). The Preliminary Design was documented in a Transportation Environmental Study Report (TESR), dated November 2012 (GWP 8-00-00). The TESR review period ended on December 21, 2012 and MTO issued "Environmental Clearance for Right-of-Way Designation and Property Expropriation" in November 2013.

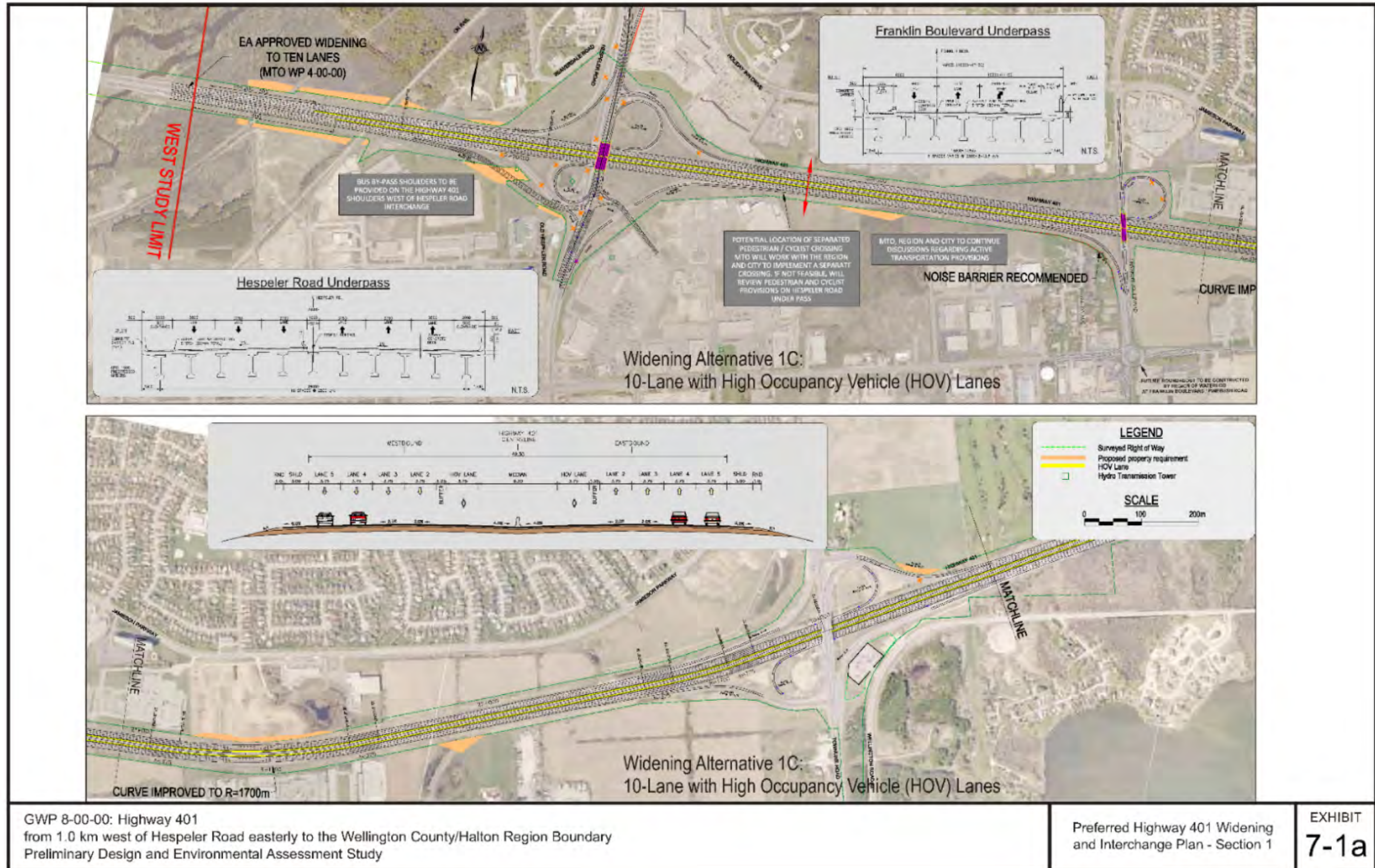
The study identified various needs and opportunities within the larger study area, including:

- Need to increase capacity to accommodate existing and projected travel demands;
- Ability to accommodate the high percentage of heavy trucks that use the highway for interregional and international movement of goods;
- Need to improve the pavement structure as it is nearing the end of its service life;
- Need to increase the length of existing bridges and culverts to accommodate a widened Highway 401;
- Managed (HOV) lanes are needed to improve highway efficiency and will connect to the HOV network in the Greater Golden Horseshoe; and,
- Opportunity to upgrade the horizontal and vertical curves to meet a design speed of 120 km/h, improve crossing road vertical alignments.

## 1.2 PROPOSED IMPROVEMENTS

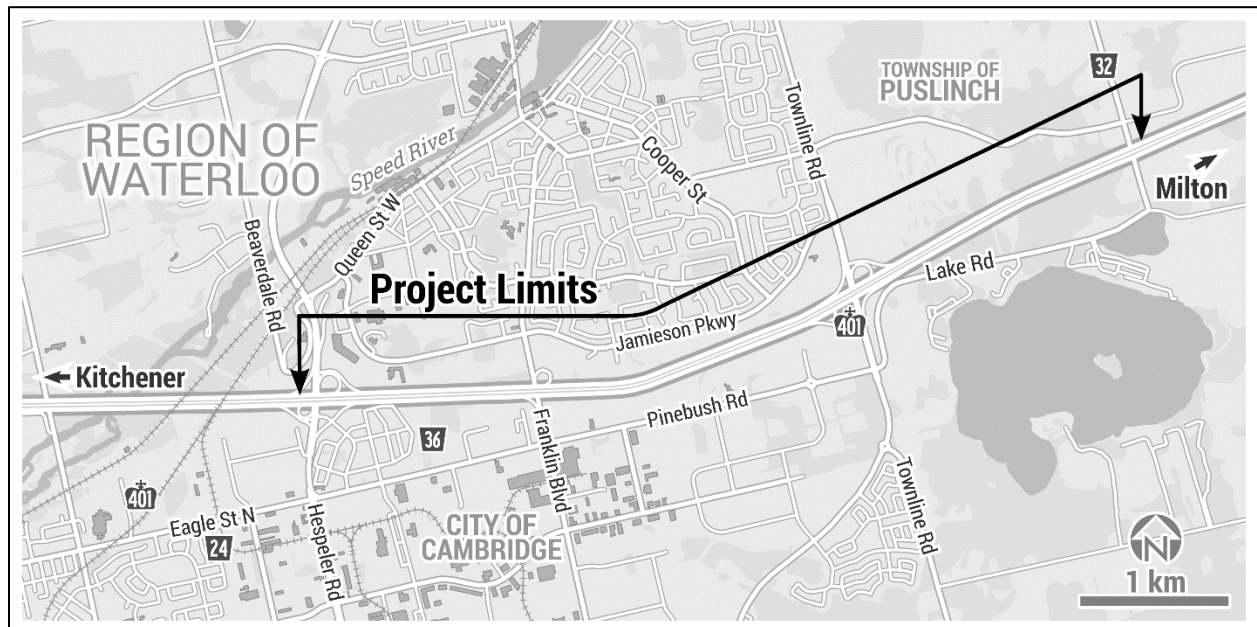
The Preliminary Design activities formed the basis for proceeding to the Detail Design. The Approved Plan from the TESR, as shown on Exhibit 2, was carried forward with minor refinements. The Preliminary Design recommendations required updating to include the accommodation of pedestrian/cyclist travel across Highway 401 on Franklin Boulevard and Hespeler Road in accordance with the active transportation goals of the Region of Waterloo and City of Cambridge.

Exhibit 2: Approved Plan from TESR



The study area was expanded since the TESR was approved in 2013 from 1.0 km east of Townline Road to Wellington Road 32, as shown in Exhibit 3.

Exhibit 3: Expanded Study Area



The project was divided into two phases for construction staging purposes. Phase I construction commenced in 2018 and included the replacement of the Franklin Boulevard bridge over Highway 401, including some work at the interchange, replacement of guiderail and municipal illumination on Franklin Boulevard. The Phase I DCR (GWP 3125-07-00), dated December 7, 2017, was on the public record for review from December 8, 2017 to January 10, 2018.

The improvements planned in Phase II are expected to commence in 2019 and be completed in 2022, and include the following:

- Widening of Highway 401 to ten lanes, including managed lanes (formerly known as High Occupancy Vehicle lanes);
- Reconstruction of the Highway 401 interchange at Hespeler Road;
- Reconstruction of ramp tie-ins at Franklin Boulevard and Townline Road interchanges;
- Replacement of the Hespeler Road bridge; and retaining walls as needed;
- Expansion of the commuter parking lot at the Townline Road interchange;

- Rehabilitation of the Wellington Road 32 Underpass;
- Drainage improvements, illumination, overhead signs, and traffic signals; and
- Noise wall constructed along the south side of Highway 401, west of Franklin Boulevard.

**This report provides a summary of the Detail Design of Phase II. Further details of the planned improvements are provided in Section 3.0 of this report.**

### 1.3 RELATED STUDIES

The Ministry is proceeding with the expansion of Highway 401 in the Cambridge area. Improvements immediately west of this Detail Design study, under Contract 2014-3014, GWP 4-00-00, include widening of Highway 401 from six to 10-lanes (including managed lanes), bus bypass shoulders, replacement of the bridges over the Speed River, noise barrier, high mast illumination and other safety improvements. The contract commenced in the Spring of 2015 with completion anticipated for September 2019.

### 1.4 OVERVIEW OF THE ENVIRONMENTAL ASSESSMENT PROCESS

#### 1.4.1 Canadian Environmental Assessment Act

The *Canadian Environmental Assessment Act*, 2012 (CEAA 2012) and its regulations establish the legislative basis for the federal EA process. Under CEAA 2012, an EA is only required for particular projects included in the list of “designated projects.” These types of projects are likely to have significant adverse environmental effects and therefore may be subject to a federal EA.

The proposed improvements to this section of the Highway 401 corridor are not listed as a “designated project” under the CEAA 2012 and therefore CEAA approvals are not required for this undertaking.

#### 1.4.2 Ontario Environmental Assessment Act

The Ontario *Environmental Assessment Act* (EAA) governs the conduct of planning studies in the province of Ontario. The purpose of this Act is the betterment of the people of the whole or any part of Ontario by providing for the protection, conservation and wise management of the environment in Ontario. R.S.O. 1990, c. E.18, s. 2.

The Act mandates clear terms of reference and ongoing consultation with all relevant parties and establishes a ‘Class Environmental Assessment’ process to streamline the planning for certain types of projects.

### 1.4.3 Ministry of Transportation Class Environmental Assessment for Provincial Transportation Facilities, 2000

The MTO *Class Environmental Assessment for Provincial Transportation Facilities (2000)* (Class EA) was approved under the Ontario *Environmental Assessment Act* (EAA) in 1997 and amended in 2000. The planning document defines groups of projects and activities, and the Environmental Assessment (EA) processes that the MTO has committed to follow for these projects. Provided that this process is followed, projects and activities included under the Class EA do not require formal review and approval under the EAA.

The projects and activities in the Class EA are classified into four groups (A, B, C and D); this project has followed the approved planning process for Group 'B' projects, which include: improvements to existing highways and freeways, new interchanges, or modifications to traffic access, improvements to provincial transitways and ferryboat docks/terminal, and the establishment/improvement of provincial transportation facilities.

As required under the Class EA, three formal documents have been developed for this project. They include:

- A Transportation Environmental Study Report (TESR), prepared in November 2012 to document Preliminary Design;
- Phase I DCR (GWP 3125-07-00), dated December 7, 2017, to document Detail Design for Phase I (made available for public review on December 8, 2017); and,
- This DCR, prepared to document Detail Design for Phase II.

## 1.5 PURPOSE OF THE DESIGN AND CONSTRUCTION REPORT

This DCR documents the implications on environmental features associated with the design and construction of the Phase II project, and documents how the commitments outlined in the TESR are being addressed. This DCR includes:

- A description of the project and its purpose;
- An overview of the public consultation undertaken;
- Major features of the proposed work, including any design refinements;
- Anticipated environmental effects and proposed mitigation measures; and,
- Commitments to monitoring associated with the implementation of the project.

The DCR has been prepared based on the recommended Detail Design to:

- Address any minor design modifications that result in environmental impacts or benefits that may not have been anticipated or detailed in the 2012 TESR;
- Further assess environmental impacts and refine mitigation requirements; and
- Address any commitments to future work identified in the 2012 TESR.

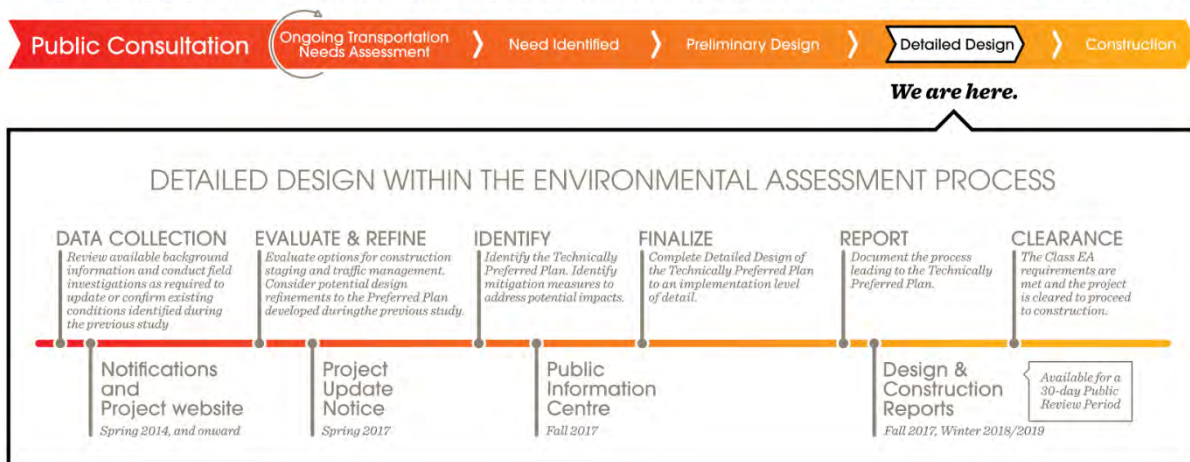
This document deals with design-specific details and issues. For environmental issues of broader concern, reference should be made to the Preliminary Design documentation as noted above.

This DCR is not eligible for a Part II Order (“bump-up”) under the provisions of the Ontario EA Act. However, there is an opportunity at any time during the MTO Class EA process for interested persons to provide comments and review outstanding issues. The DCR is available for public and agency review for a minimum of 30 calendar days, from **Tuesday, March 12, 2019 to Wednesday, April 10, 2019**. Any concerns raised by members of the public, interested groups or agencies during this review period should be discussed with MTO or their consultants identified on the attached *Notice of Submission of the Design and Construction Report*, as all comments received during the review period will be considered by the Ministry.

During the course of this Detail Design study, all reasonable steps were taken to meet environmental technical requirements, formal environmental approvals, environmental reporting obligations, and environmental standards applicable to the project.

Exhibit 4: MTO Class EA Process

THE CLASS ENVIRONMENTAL ASSESSMENT PROCESS for GROUP 'B' PROJECTS



## 2.0 Consultation Process

The Preliminary Design phase of the project involved a broad range of participants with diverse interests, including the Region of Waterloo, City of Cambridge, Township of Puslinch, external agencies, property owners, and members of the general public. The consultation process for this project was designed to provide a smooth transition from Preliminary Design to the current Detail Design phase.

### 2.1 CONSULTATION PLAN OVERVIEW

A Consultation Plan was developed at the start of the project and was designed to include:

- Notice of Study Commencement;
- Communication with external agencies in order to obtain pertinent technical information and confirm the requirement for legislative or regulatory approvals related to the undertaking;
- Communication with adjacent property owners where work proposed is likely to have an impact on their property;
- Communication with residents and businesses in the area that may be affected by construction staging and traffic management;

- A Public Information Centre;
- Notice of Study Completion; and
- A project specific website: [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

Due to the change in project scope, a Notice of Study Update was prepared in June 2017, to explain the changes proposed and to update project contact information.

The project contact list from the Preliminary Design and Class EA study was updated in May 2014 and used for initial study notification. Key stakeholders include local municipalities, institutions, service providers, local business owners, industries and associations, and the general public with an interest in the project, including adjacent property owners and local residents. The list was added to and updated as necessary throughout the study.

Stakeholder input was incorporated into the project findings and recommendations, as appropriate, and responses were provided to all input received. All project correspondence to/from the public was collected in accordance with the *Freedom of Information and Protection of Privacy Act*. Accordingly, with the exception of personal information, all public comments are part of the public record.

Each public notice was published in the Cambridge Times, Waterloo Record, Turtle Island News and Two Row Times.

A description of the consultation plan undertaken and the results of the process to address government agency/municipal interests and public concerns are provided in the following sections. Relevant consultation materials are included in **Appendix A** of this report.

## 2.2 PROJECT WEBSITE

A project website ([highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)) was developed to function as an interactive tool for the project team to provide study updates, and an opportunity for stakeholders to submit comments during the Detail Design period. The content includes a Home page and related links for Project Schedule, EA Process, Project Overview, Notices, Consultation, Reference Materials, Contact the Project Team, FAQs, and Links. Visitors have the opportunity to contact the project team directly through the dedicated project email address [[comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)].

The website conforms to current *Accessibility for Ontarians with Disabilities Act* (AODA) requirements, WCAG 2.0 Level A, allowing all visitors to benefit from the available information.

## 2.3 NOTICE OF STUDY COMMENCEMENT

The Notice of Study Commencement of Detail Design was published in the following newspapers:

- Waterloo Record on Saturday, May 31, 2014;
- Turtle Island News on Wednesday, June 4, 2014;
- Two Row Times on Wednesday, June 4, 2014; and,
- Cambridge Times on Thursday, June 5, 2014.

The Ontario Government Notice (OGN) described the project, including the proposed improvements and the Class EA process. The Notice also requested public involvement and provided names and contact information for representatives from the MTO and Stantec.

Study Commencement notification letters, with a copy of the OGN, were sent to external agencies/stakeholders/property owners on the project mailing list on Wednesday, May 28 and Thursday, May 29, 2014. A comment form was included as a part of the mailed materials. Ted Arnott, MPP (Wellington-Halton Hills) and Kathryn McGarry, MPP (Cambridge) were notified on July 4, 2014.

A copy of the notification materials is contained in **Appendix A**.

## 2.4 NOTICE OF STUDY UPDATE

A Notice of Study Update was published in the following newspapers:

- Turtle Island News on Wednesday, June 14, 2017;
- Two Row Times on Wednesday, June 14, 2017;
- Cambridge Times on Thursday, June 15, 2017; and,
- Waterloo Record on Saturday, June 17, 2017.

The OGN described the two phases of the project, tentative construction schedule for the proposed improvements, and the Class EA process. The Notice also requested public involvement and provided names and contact information for representatives from the MTO and Stantec.

Study update notification letters, with a copy of the OGN, were sent to external agencies/stakeholders/property owners on the project mailing list on Monday, June 12, 2017. A

comment form was included as a part of the mailed materials. Ted Arnott, MPP (Wellington-Halton Hills) and Kathryn McGarry, MPP (Cambridge) were notified on June 8, 2017.

A copy of the notification materials is contained in **Appendix A**.

## 2.5 NOTICE OF PUBLIC INFORMATION CENTRE

A Notice of Public Information Centre (PIC) was published in the following newspapers:

- Waterloo Record on Tuesday, October 31, 2017;
- Turtle Island News on Wednesday, November 8, 2017;
- Two Row Times on Wednesday, November 8, 2017; and,
- Cambridge Times on Thursday, November 9, 2017.

The OGN described the two phases of the project, tentative construction schedule for the proposed improvements, PIC details, and the Class EA process. The Notice requested public involvement and provided names and contact information for representatives from the MTO and Stantec.

PIC notification letters, with a copy of the OGN, were sent to external agencies/stakeholders/property owners on the project mailing list on Monday, October 30, 2017. A comment form was included as a part of the mailed materials. Ted Arnott, MPP (Wellington-Halton Hills) and Kathryn McGarry, MPP (Cambridge) were notified on Tuesday, October 24, 2017.

## 2.6 NOTICE OF DCR FOR PHASE I

A Notice of Submission of Design and Construction Report (Phase I) was published in the following newspapers:

- Turtle Island News on Wednesday, November 8, 2017;
- Two Row Times on Wednesday, November 29, 2017;
- Cambridge Times on Thursday, November 30, 2017; and
- Waterloo Record on Saturday, December 2, 2017.

The notice announced the commencement of the minimum 30-day public and regulatory agency review period for the DCR, the locations where the DCR was available for viewing, and the closing date for comments to be submitted. The Notice requested public involvement and provided names and contact information for representatives from the MTO and Stantec.

Notification letters, with a copy of the OGN, were sent to external agencies/stakeholders/property owners on the project mailing list on Wednesday, November 29, 2017. Ted Arnott, MPP (Wellington-Halton Hills) and Kathryn McGarry, MPP (Cambridge) were notified on Monday, November 27, 2017.

## 2.7 CONSULTATION AND ENGAGEMENT WITH INDIGENOUS COMMUNITIES

Indigenous communities were contacted by the Project Team during Preliminary Design and at key milestones throughout the Detail Design phase. The Indigenous communities contacted by MTO during this Detail Design study included Six Nations of the Grand River Territory, Haudenosaunee Confederacy Chiefs Council, and Mississaugas of the Credit First Nation. The following correspondence from the Ministry was sent to inform the communities of the project and the PIC, as well as provide an opportunity for input:

- Notice of Study Update - June 12, 2017
- Notice of PIC – Monday, October 30, 2017
- Notice of DCR for Phase I – Wednesday, November 29, 2017
- Notice of DCR for Phase II – Tuesday, February 26, 2019

## 2.8 CONSULTATION WITH EXTERNAL AGENCIES

The government agencies, ministries, and stakeholders that were contacted during this study are outlined in Table 1.

Table 1: Study Contact List

Federal / Provincial Government	
• Ministry of Natural Resources and Forestry, Guelph District Office	• Cambridge MP
• Ministry of Tourism, Culture, and Sport	• Cambridge MPP
• Ministry of Environment Conservation and Parks	• Ministry of Agriculture, Food, and Rural Affairs
• Wellington-Halton Hills MP	• Ministry of Municipal Affairs and Housing, Ontario Growth Secretariat
• Wellington-Halton Hills MPP	• Ontario Provincial Police, Cambridge & Wellington Detachments
• Kitchener South – Hespeler MP	• Infrastructure Ontario
• Kitchener South – Hespeler MPP	• Canadian National Railway

### Municipal Government

- |  |   |
|--|---|
| • City of Cambridge                    | • Township of Puslinch                          |
| • Regional Municipality of Waterloo    | • County of Wellington                          |
| • Guelph-Wellington Paramedic Service  | • Township of Puslinch Fire and Rescue Services |
| • Waterloo Regional Police Service/OPP | • Cambridge Environmental Advisory Committee    |

### Stakeholders / Interest Groups

- |                                       |  |
|---------------------------------------|--|
| • Grand River Transit                 | • Student Transportation Services of Waterloo Region |
| • Waterloo Region Heritage Foundation | • Greater Kitchener-Waterloo Chamber of Commerce     |
| • Cambridge Chamber of Commerce       | • Ontario Cyclist Association                        |
| • Ontario Heritage Trust              | • Grand River Conservation Authority                 |

### Utilities

- |                                   |                                     |
|-----------------------------------|-------------------------------------|
| • Energy Plus                     | • Union Gas                         |
| • Rogers Cable Communication Inc. | • Bell Canada                       |
| • Hydro One Networks Inc.         | • Regional Municipality of Waterloo |

## 2.8.1 Municipal Meetings

**Meeting No. 1** - The first municipal meeting was held on Monday, June 23, 2014 with representatives from the City of Cambridge and the Regional Municipality of Waterloo to introduce the Detail Design study and obtain comments related to the design and construction of the project. The meeting was attended by representatives from:

- City of Cambridge - Engineering Services
- City of Cambridge - Transportation Engineering
- City of Cambridge - Fire Department
- Regional Municipality of Waterloo - Transportation Engineering & Planning
- Regional Municipality of Waterloo - Transportation Operations

The Regional Municipality (RMW) and the City requested that MTO reconsider the options of integrating active transportation facilities on Hespeler Road. Options may include a sidewalk on the approaches to and across the structure or way-finding to other Highway 401 crossings (i.e., a separate bridge such as Speedsville Road or Franklin Boulevard). It was noted that the east side

of Hespeler Road is the preliminary preferred side for accessibility options. RMW will consider undertaking a Feasibility Study to address the issue.

Construction staging and traffic management during construction was reviewed. Both RMW and the City confirmed that there were no serious emergency service concerns relating to the closure of Franklin Boulevard during construction.

**Meeting No. 2** – The second municipal meeting was held on Tuesday, August 15, 2017 with representatives from the City of Cambridge, Township of Puslinch and RMW to provide an update of the project, including traffic staging plans and potential impacts during construction. The meeting was attended by representatives from:

- City of Cambridge – City Engineer
- City of Cambridge – Manager Development Engineering
- City of Cambridge – Manager of Road Operations
- Regional Municipality of Waterloo – Head Transportation
- Regional Municipality of Waterloo – Acting Manager, Transportation Planning
- Regional Municipality of Waterloo – Grand River Transit

RMW indicated it has no preference on the barrier wall design for the multiuse pathway, but to consider a less robust design to allow for aesthetic treatments since the interior wall is crash tested. RMW suggested that a form liner be added to the inside of the barrier wall.

RMW requested that the W-S ramp (eastbound off-ramp) remain open as much as feasible during construction and that a review of how the multi-use trail aligns to cross the ramp.

**Meeting No. 3** – The third municipal meeting was held on December 17, 2018 with representatives from the County of Wellington, and RMW to provide an update of the project, including traffic staging plans and potential impacts during construction. The City of Cambridge and Township of Puslinch were also invited to attend the meeting but were unable to send representatives. The meeting was attended by representatives from:

- County of Wellington – Operations Manager
- County of Wellington – Roads Superintendent
- Regional Municipality of Waterloo – Director, Transportation

- Regional Municipality of Waterloo – Manager, Transportation
- Regional Municipality of Waterloo – Head, Rehab Design and Construction
- Regional Municipality of Waterloo – Supervisor, Corridor Management
- Regional Municipality of Waterloo – Acting Manager, Transportation Planning
- Regional Municipality of Waterloo – Senior Transportation Engineer

Stantec presented the proposed improvements, highlights from a recent Value Engineering (VE) Study, and staging (including ramp and lane closures). RMW noted its concern about impact on the local economy regarding the ramp closures, but understood the quality, schedule, safety and cost benefits of proceeding with this staging. RMW requested a copy of the staging plans to review the ramp closures. RMW requested details about the design of the multi-use trail direct ramp crossings. The County of Wellington did not express concerns with the overnight closures of the Wellington Road 32 underpass for repairs given the available detour route at Townline Road.

### 2.8.2 Summary of External Agency Feedback

There was limited formal correspondence received from external agencies as a result of the study commencement, study update, PIC and DCR Phase I notification. Comments provided by the Ministry of Tourism, Culture and Sport (MTCS), Heritage Planner (June 27, 2014) requested confirmation of how the recommendations in the TESR are being addressed during the Detail Design component of the project. On November 17, 2014, MTCS was advised that a Stage 1 Archaeological Assessment was undertaken during Preliminary Design and the TESR did not recommend site specific mitigation measures because it is not anticipated that improvements will adversely affect the identified cultural heritage resources in this study area. A copy of the Stage 1 acceptance letter from MTCS was included with the correspondence, as well as the MTO *Heritage Bridge Committee Decision Forms* for Hespeler Road Underpass and Franklin Boulevard Underpass to confirm that structures have been deemed not worthy of further consideration for listing on the Ontario Heritage Bridge List.

Grand River Transit (GRT) provided comments on June 19, 2017 indicating that three bus routes are in operation within the study area, on Hespeler Road and on Franklin Boulevard. The full closure of Franklin Boulevard Bridge in 2018 will impact one route. It was also noted that Hespeler Road is a major transit corridor with frequent transit service, and the Region of Waterloo/GRT are currently working with MTO on future opportunities for transit priority measures in the study area. The GRT requests that the transit only left turn from Hespeler Road towards the Smart Centre be maintained throughout the duration of construction.

## 2.9 PUBLIC INFORMATION CENTRE

A Public Information Centre (PIC) was held on Tuesday, November 14, 2017 at the Hespeler Memorial Arena – Gallery Room, in the City of Cambridge, from 4:00 PM to 8:00 PM. The objectives of the PIC were to present and receive input on the design details of the Approved Plan, including information about the construction staging, traffic management plan, detours and associated road improvements in advance of the construction activities anticipated to begin in the Spring 2018.

The PIC was a drop-in format. The text panels and displays were available for review. Project team representatives from MTO and Stantec were present to answer questions and discuss the study on a one-to-one basis. The Class EA document and the *Environmental Reference for Highway Design* (2013) were made available for reference purposes.

The following information was displayed at the PIC:

- Welcome
- About the Project
- About the Process
- Environmental Management
- Highway 401: Before and After
- Phase 1: Franklin Boulevard (2018 project)
- Phase 2: Mainline (2019 project)
- Construction Staging
- Thank You for Attending

A copy of the information presented on the PIC display boards is provided in **Appendix B**. The display boards were uploaded to the project website following the PIC.

### 2.9.1 PIC Attendance

A total of 83 people signed the visitor register at the PIC including seven representatives from external agencies. Attendees included representatives from the Township of Puslinch, Region of Waterloo, City of Cambridge, City of Guelph, and other members of the public who were interested in the project. All names and addresses from the visitor register and submitted comment forms were added to the project mailing list.

### 2.9.2 Summary of PIC Participant Feedback

Participants were encouraged to fill in and submit a comment form to provide feedback on the project, or to complete an online comment form on iPads provided at the PIC. Comment forms could be returned in the comment box at the meeting or by mail, fax, or email.

Verbal comments received at the PIC were related to the timing of construction and a request to close the west to south ramp (Highway 401 eastbound off-ramp) on Franklin Boulevard south of the bridge due to the difficulty of changing lanes near the roundabout. A few residents in attendance brought up their concerns with an increase in noise at Lake Road, in the Puslinch Lake area and questioned the lack of a noise barrier in this area. A potential drainage concern was also identified south of Highway 401 between Franklin Boulevard and Townline Road. Finally, construction timing, impacts of potential work on the Wellington Road 32 structure within the Township of Puslinch, and coordination with the Morriston Bypass project, were also discussed.

An overview of comments received from the public/agencies and responses provided are included in Table 2 and Table 3.

Table 2: Summary of Public Input Received and Responses

Comment	Response and/or Action Taken
<b>Construction Area</b>	
<ul style="list-style-type: none"> <li>Request to have improved lighting and reflective line paint during construction to improve visibility for drivers; especially for people who have difficulty seeing in the dark.</li> <li>Request to add photo radar to construction area to reduce speed of drivers</li> </ul>	<ul style="list-style-type: none"> <li>Your request for improved visibility within the construction zone has been noted and forwarded to the Ministry of Transportation for their consideration. Standard line painting will be used rather than orange markings, sometimes found in construction zones.</li> <li>Your interest in photo radar to monitor traffic has been noted. Driving speeds and safety on provincial highways are enforced by the Ontario Provincial Police (OPP) and reduced speed limits will be posted within the construction zones. Photo Radar is not currently or is not anticipated to be used during construction.</li> </ul>
<b>Detours</b>	
<ul style="list-style-type: none"> <li>Concern westbound Highway 401 traffic will use Townline Rd and residential neighborhood to avoid construction</li> </ul>	<ul style="list-style-type: none"> <li>Your concern with Highway 401 traffic detouring through your neighbourhood has been noted and the preferred detour route during full closures of Highway 401 has been developed in consultation with the Region of Waterloo and City of Cambridge. The signed detour route will be implemented as required, including one overnight closure for the removal</li> </ul>

Comment	Response and/or Action Taken
<ul style="list-style-type: none"> <li>▪ Question asking if the possible traffic impacts of the detour have been assessed</li> <li>▪ Concern having detour traffic with roundabout. Request to have police traffic enforcement at the Franklin and Pinebush roundabout</li> </ul>	<p>of the existing Franklin Road bridge over Highway 401, and two overnight closures for girder placement for the new bridge. Police support will be utilized at key intersections to assist in the use of the signed detour route.</p> <ul style="list-style-type: none"> <li>▪ The detouring of Highway 401 traffic is required to complete bridge demolition and girder placement as this cannot be done over live traffic. Full closures of Highway 401 will be completed during periods when traffic volumes are lowest. The Ministry is working with the Region of Waterloo and City of Cambridge to assess possible operational improvements along the detour route during these closures, such as signal timing adjustments. Police support will be utilized at key intersections to assist in the use of the signed detour.</li> <li>▪ Your concerns regarding detours through the roundabout has been noted. Unfortunately, there are no reasonable alternatives that do not involve a roundabout. Police support will be provided at critical intersections during closure.</li> </ul>

#### Multi-Use Trail

- |   |   |
|---|---|
| <ul style="list-style-type: none"> <li>▪ Happy you have considered pedestrians and cyclists. Having access to two multi-use trails will make it easier to feel like Hespeler is a part of greater Cambridge</li> <li>▪ City of Cambridge and Waterloo Region should invest in pedestrian and cycling access to the new bridges crossing Highway 401.</li> <li>▪ Multi-use trails at Hespeler Rd and Franklin Blvd are good. Request to close southbound ramp on west side of Franklin Blvd and have trail on west side of bridge</li> </ul> | <ul style="list-style-type: none"> <li>▪ Your support for the multi-use trail has been noted.</li> <li>▪ Thank you for your comment regarding the pedestrian and cycling access to the new bridge crossings. This has been forwarded to the Region of Waterloo and the City of Cambridge.</li> <li>▪ Your request to close the southbound ramp on Franklin Boulevard has been noted and your comment is under consideration by the</li> </ul> |
|---|---|

Comment	Response and/or Action Taken
<ul style="list-style-type: none"> <li>▪ Request for a pedestrian bridge over the roundabout at Franklin Blvd and Pinebush Road <b>(x2)</b></li>   <li>▪ Concern for user’s safety of the proposed multi-use trail crossing of 401 at Franklin Blvd. Users will be at grade and need to beware of and give way to the on-ramp traffic is simply not an acceptable design solution</li>   <li>▪ Concern for user’s safety of the proposed multi-use trail crossings and a suggestion to create a pedestrian only bridge from the Smart Centre Plaza across the 401 to the Zehrs parking lot.</li> </ul>	<p>Ministry of Transportation, the Region of Waterloo, and the City of Cambridge.</p> <ul style="list-style-type: none"> <li>▪ Thank you for your comment regarding the pedestrian bridge over the Franklin Boulevard and Pinebush Road roundabout. This has been forwarded to the Region of Waterloo and the City of Cambridge.</li>   <li>▪ Your concern about safety in relation to the multi-use trail crossing Franklin Boulevard have been noted. A separation barrier will be included between northbound traffic and the multi-use trail. The purpose of this barrier is to separate and protect pedestrian and cyclists as they cross the bridge. A grade separation of the multi-use trail and the ramp is not being considered at this time.</li>   <li>▪ Your concern about safety in relation to the multi-use trail crossing Franklin Blvd have been noted. A separation barrier will be included between northbound traffic and the multi-use trail. The purpose of this barrier is to separate and protect pedestrian and cyclists as they cross the bridge.</li> </ul>
<b>Noise</b>	
<ul style="list-style-type: none"> <li>▪ Concerns regarding an increase in noise due to an increase in lanes. Request for a sound barrier east of Townline Road <b>(x25)</b></li> </ul>	<ul style="list-style-type: none"> <li>▪ Your concern with the increased noise due to traffic has been noted. A noise assessment was undertaken during the Preliminary Design study to assess the potential impacts from the Highway 401 improvements. Two noise receptors (OLA 05 and 06) in the area of concern were included in the assessment; however, future build and no-build results showed noise levels less than 65 dBA and changes in decibels less than 5.</li>   <li>▪ Under the requirements of the MTO Environmental Guide for Noise, noise mitigation should be investigated when the proposed project results in changes in sound</li> </ul>

Comment	Response and/or Action Taken
	<p>exposures are greater than 5 decibels or noise levels are above 65 dBA. As a result, this area was not identified as requiring mitigation in the Transportation Environmental Study Report approved in November 2013.</p> <ul style="list-style-type: none"> <li>▪ Based on the number of comments received, the Ministry is reviewing the previously completed noise assessment. Since this area is located east of widening of Highway 401 being completed as part of this project, any recommendations from the updated assessment will be considered for future expansion work east of Townline Road.</li> </ul>
<b>Franklin Bridge</b>	
<ul style="list-style-type: none"> <li>▪ Request for all 4 directions on/off ramp at Franklin Blvd (x2)</li> <li>▪ Request for an eastbound on-ramp to the 401 at Franklin Boulevard</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your request for a full interchange at Franklin Boulevard has been noted. This comment was received during the Environmental Assessment and Preliminary Design stages. The provision of access/egress from the east to/from Franklin Boulevard was reviewed during the Preliminary Design stage. Due to the major property impacts and the proximity of the Hespeler Road and Townline Road interchanges, this alternative was not carried forward. The preferred alternative was to have Franklin Boulevard remain as a partial interchange.</li> <li>▪ Your interest in an eastbound on-ramp to Highway 401 has been noted. The provision of access/egress from the east to/from Franklin Boulevard was reviewed during the Preliminary Design stage. Due to the major property impacts and the proximity of the Hespeler Road and Townline Road interchanges, this alternative was not carried forward. The preferred alternative was to have Franklin Boulevard remain as a partial interchange.</li> </ul>

Comment	Response and/or Action Taken
<ul style="list-style-type: none"> <li>▪ Request to remove the off-ramp from the 401 at Franklin Boulevard or add traffic lights at top of ramp</li> <li>▪ Concern with speed of traffic exiting 401 and weaving distance between off-ramp at Franklin Blvd to the roundabout at Pinebush Road. It is very difficult for traffic, including buses travelling south on Franklin Blvd to make lane changes to turn right onto Pinebush Road due to the 3 lanes at the roundabout.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your request to remove the off-ramp on Franklin Boulevard has been noted and your comment is under consideration by the Ministry of Transportation, the Region of Waterloo and the City of Cambridge.</li> <li>▪ Your comment regarding the speed of traffic exiting Highway 401 eastbound at Franklin Boulevard and the difficulty in changing lanes before the roundabout at Pinebush Road have been noted and is under consideration by the Ministry of Transportation, the Region of Waterloo and the City of Cambridge.</li> </ul>
<b>Drainage</b>	
<ul style="list-style-type: none"> <li>▪ Request for drainage plan for property at 420 Pinebush Road.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your concern about drainage from Highway 401 onto your property has been noted. The project team is evaluating the drainage patterns in this area based on the concern raised.</li> </ul>
<b>Carpool Lot</b>	
<ul style="list-style-type: none"> <li>▪ Request for provisions for a GO Transit and Grand River Terminal at the proposed expanded carpool lot.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your interest in a GO Transit and Grand River Bus terminal at the expanded carpool lot has been forwarded on to Grand River Transit.</li> </ul>
<b>Other Concerns</b>	
<ul style="list-style-type: none"> <li>• Concern that MTO is behind on planning for these projects. These projects should be started before traffic conditions get to the levels that are observed</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your comment has been noted. MTO is mandated to follow the Preliminary Design, Environmental Assessment, and Detail Design process in terms of planning for future needs. MTO currently utilizes the Northern and Southern Highways Program, which is an annually published five-year investment plan, that identifies and prioritizes projects across the province. Improving Ontario's transportation network is part of the government's plan to strengthen the economy.</li> </ul>

Comment	Response and/or Action Taken
<ul style="list-style-type: none"> <li>▪ Request for MTO to complete the project faster</li> </ul>	<ul style="list-style-type: none"> <li>▪ Your comment has been noted. MTO is strategically implementing accelerated construction methods in this project to complete critical portions of the project more expeditiously. Phase I will commence in the Spring of 2018 and will include the replacement of the Franklin Boulevard bridge over Highway 401 along with some interchange work, replacement of guiderail and municipal illumination on Franklin Boulevard. Phase II is planned to begin in 2019 and will include widening of Highway 401 to ten lanes, including managed lanes; reconstruction of the Highway 401 interchange at Hespeler Road; reconstruction of ramp tie-ins at Franklin Boulevard and Townline Road interchanges; replacement of the Hespeler Road bridge; and retaining walls as needed. In addition, the expansion of the commuter parking lot at the Townline Road interchange; drainage improvements; illumination; overhead signs; and, traffic signals will be completed as part of this phase.</li> </ul>
<ul style="list-style-type: none"> <li>▪ Request by bus operator for a bus only turn lane on southbound Hespeler road into the Smart Centre be expanded to accommodate two buses.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Thank you for your comment regarding the bus-only turn lane southbound on Hespeler Road into the Smart Centre. The project team will review the turning movements and consider the comment in the design.</li> </ul>
<ul style="list-style-type: none"> <li>▪ Request for a left turn lane into Melran Drive along Townline Road. Concern due to resident being hit on the right while turning right into their driveway due to people thinking they were turning left and sneaking around on the shoulder.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Thank you for your comment regarding left turn lane from Townline Road onto Melran Drive. This is outside of the scope of this project.</li> </ul>
<ul style="list-style-type: none"> <li>▪ Question about what is being done currently on the 401 in this corridor as the next phase doesn't start until 2018.</li> </ul>	<ul style="list-style-type: none"> <li>▪ The project to the west of Hespeler Road on Highway 401 is currently under construction and appropriate separation is required.</li> </ul>

Note: The symbol 'x#' denotes the number of comments

Table 3: External Agency Input Received and Responses/Action Taken

External Agency	Comment	Response and/or Action Taken
<b>Grand River Transit</b>	<ul style="list-style-type: none"> <li>Request by the scheduler to get a detailed estimate on when Franklin Boulevard will be completely closed and an estimate on when it will be available for traffic again for extra Bus Operators and printing detour schedules for their customers.</li> </ul> <p>The winter schedule lasts until April 29, 2018 and they can handle lane restrictions through April but requested that the full closure not start until May 2018.</p>	<ul style="list-style-type: none"> <li>Grand River Transit has been advised of the anticipated start and closure times. The contractor will be required to provide notice of construction schedule as it becomes available.</li> </ul>
<b>Waterloo Region District School Board</b>	<ul style="list-style-type: none"> <li>Request by principal planner for Waterloo Region District School Board (WRDSB) to promote active transportation for students south of the 401 by providing suitable multi-use paths within the proposed bridge design.</li> <li>Request to continue to be involved as the planning progresses</li> </ul>	<ul style="list-style-type: none"> <li>Multi-use paths south of the highway is outside of the scope of this project.</li> <li>WRDSB has been advised of the anticipated start and closure times. The contractor will be required to provide notice of construction schedule as it becomes available.</li> </ul>

## 2.10 STUDY COMPLETION AND NOTICE OF DCR FOR PHASE II

A Notice of Submission of the Design and Construction Report (Phase II) was published in the following newspapers:

- Turtle Island News on Wednesday, March 6, 2019;
- Two Row Times on Wednesday, March 6, 2019;
- Cambridge Times on Wednesday, March 6, 2019; and
- Waterloo Record on Saturday, March 9, 2019.

The notice announced the commencement of the 30-day public and regulatory agency review period for the DCR, the locations where the DCR was available for viewing, and the closing date for comments to be submitted. The Notice requested public involvement and provided names and contact information for representatives from the MTO and Stantec.

Notification letters, with a copy of the OGN, were sent to external agencies/stakeholders/property owners on the project mailing list on Tuesday, February 26, 2019. Members of Parliament and Members of Provincial Parliament were notified on Friday, February 19, 2019.

## 3.0 Major Features of the Proposed Work

The project has been developed to the implementation level of detail. The Approved Plan was carried forward from Preliminary Design and refined during the Detail Design phase. The PIC displays in **Appendix B** provide an overview of the information detailed in the sections below.

Design drawings with details of the proposed improvements to be constructed are shown on the Detail Design Plan in **Appendix C**.

### 3.1 STRUCTURES

#### 3.1.1 Hespeler Road Underpass

This project includes construction of a replacement structure for upgrading the Hespeler Road Underpass interchange, accommodating a new 10 lane configuration of Highway 401 including managed lanes plus ramps and a wide, separated multi-use trail on the bridge. The existing interchange is a partial Parclo A-4 and will remain; i.e., no additional ramps will be constructed. The new bridges will span an ultimate 10 lane cross-section where two lanes are designated for managed lane travel, between ramps where the S-W ramp will pass under the north span and the N-E ramp will pass under the south span.

The bridge design for the new Hespeler Road Underpass consists of a two-span slab-on-girder integral abutment structure with 1600 mm deep steel box girders, utilizing partial-depth precast concrete deck panels. The structure will be on a 4.0° skew with two equal spans of 40.0 m. The bridge will be constructed in stages over two construction seasons.

The new single structure will have a larger footprint than the two existing structures with an approximately 15 m longer span and, with the addition of the multi-use trail, will be slightly wider.

### 3.1.2 Franklin Boulevard Bridge

The Franklin Boulevard underpass was replaced in advance of the reconstruction and widening of Highway 401 under GWP 3125-07-00 (Contract 2017-3016). The remaining structural work required under this contract is to construct the toe walls for the underpass.

### 3.1.3 Wellington County Road 32 Underpass

The Wellington County Road 32 Underpass requires rehabilitation of the portion of the deck soffit over Highway 401. Rehabilitation work will consist of the removal and patching of deteriorated concrete. Overnight Highway 401 lane closures and full closures of Wellington Road 32 will be required to carry out the soffit repairs.

### 3.1.4 Culverts

This project includes the following improvements to structural culverts within the project limits:

#### Station 10+663 (Culvert C8)

- North Side of Highway 401 - The existing 4 m long CSP portion (i.e., existing extension on the end of the box culvert) is in poor condition (rusting) and will be replaced with a CSP culvert of the same length. Ditching will occur within 30 m of the culvert.
- South Side of Highway 401 - Ditching within 30 m of the culvert.

#### Station 11+400 (Culvert C9 – Irish Creek)

- North Side of Highway 401 - There is no work required at the culvert outlet or within 30 m of the culvert outlet.
- South Side of Highway 401 - Ditching within 30 m. A concrete header and gabion basket wing walls will be added to the top of the south end of the culvert. Rip-rap will be placed along the bottom of the wing walls. The wing walls and rip-rap will be located beside the culvert and above the high-water level of Irish Creek. Sand bag barriers will be used to isolate the work areas from the creek flow during construction. The barriers will be on the creek bed (i.e., inside the culvert) and on the creek banks (i.e., to approximately 10 m downstream of the culvert).

## 3.2 HIGHWAY 401 WIDENING

Based on the analysis and evaluation of alternatives and the review and integration of comments received through the consultation process, a preferred plan for the widening of Highway 401 has been developed.

The widening of Highway 401, as part of this project, includes improvements to vertical curves to accommodate 120 km/h design standards, reconstruction of deteriorated pavement along existing Highway 401, and improvements to the vertical alignments of crossing roads. The widening will be as follows:

- From 1.0 km west of Hespeler Road to 2.1 km east of Townline Road (including transitions to match the existing 6 lane cross-section), widen Highway 401 from 6 general purpose lanes to 10 lanes consisting of:
  - 8 general purpose lanes; and
  - 2 managed lanes.

The transitions to the existing 6-lane cross-section occur near the Townline Road interchange. The Highway 401 eastbound managed lane becomes a general purpose lane approximately 500 m west of Townline Road, and a general purpose lane becomes a right lane must exit to the Townline Road off-ramp (W-N/S). The additional general purpose lane is dropped east of the successive on-ramps, approx. 1.7 km east of Townline Road. The Highway 401 westbound managed lane is fully generated approx. 100m east of Townline Road, and the fourth general purpose lane is created by the Townline Road inner loop on-ramp (S-W).

Highway 401 improvements will also include replacement of the median tall wall and storm sewer, and installation of new high mast illumination to 1.75 km east of Townline Road.

## 3.3 INTERCHANGE IMPROVEMENTS

### 3.3.1 Hespeler Road Interchange

The Hespeler Road Interchange at Highway 401 will be reconstructed, including replacement of the bridges over Highway 401, and the realignment of the south ramp terminal to provide increased separation from the underpass and increase the radius of the eastbound inner loop on-ramp (N-E). Existing ramps will be resurfaced, except the eastbound off-ramp (W-N/S) and westbound on-ramp (N-W) which were recently constructed under Contract 2014-3014. The ramp alignments will be modified to match the widened cross section of Highway 401, except for the eastbound off-ramp and westbound on-ramp. The N-E ramp will be shifted slightly south, and radius increased to improve operations of the ramp. Overhead signs will be used to identify the exit points for the ramps. A realignment of a portion of the eastbound off-ramp, and mall entrance will be completed to match the new south ramp terminal intersection location.

Hespeler Road will be reconstructed since the profile will be raised by up to 1.5m due to the depth of the new underpass to span the new 10-lane cross section of Highway 401. The majority of the grade raise occurs on the south approach, where the grade is increased to 3.5%. This was completed to avoid clearance issues with the Hydro transmission lines which cross above the south ramp terminal intersection. The north approach to the bridge requires only a slight grade raise.

A new multi-use trail (MUT) will be constructed on the east side of Hespeler Road and will include a separation barrier on the bridge over Highway 401. The MUT will vary in width from 3.0 m to 4.0 m, with the widest section occurring on the bridge over Highway 401. Enhanced signing and pavement markings will be provided for the at-grade crossing of the direct ramp (S-W). Other crossing locations of ramps and the mall entrance will occur at signalized intersections.

### 3.3.2 Franklin Boulevard Interchange

As previously noted, the Franklin Boulevard underpass was replaced in advance of the reconstruction and widening of Highway 401 under GWP 3125-07-00 (Contract 2017-3016). This project also included the realignment of the Franklin Blvd S-W ramp. The remaining work required under this contract is to reconstruct the ramp tie-ins to the widened section of Highway 401.

### 3.3.3 Townline Road Interchange

The Townline Road interchange at Highway 401 will be resurfaced as part of this contract. In addition, the ramp tie-ins will be realigned to match the widened section of Highway 401. Drainage concerns were previously identified by MTO staff on the Townline Road approaches to the underpass, and temporary asphalt curb was constructed. The asphalt curb will be replaced with concrete curb and gutter, and new catchbasins to control the surface water from the Townline Road pavement.

## 3.4 COMMUTER PARKING LOT EXPANSION

An existing MTO commuter parking lot is located in the southeast quadrant of the Highway 401/Townline Road interchange, with access off of Wellington Road 32. Based on the needs for the widening of Highway 401, the preferred plan includes the expansion of the existing carpool parking lot to accommodate additional parking spaces. High mast lights for the Townline Road interchange will provide full illumination of the commuter parking lot.

## 3.5 ACTIVE TRANSPORTATION

Pedestrian and cyclist facilities across Highway 401 on Hespeler Road bridge are recommended in accordance with RMW and City of Cambridge goals for active transportation. Several alternative designs have been developed based on local and provincial design standards and expectations. The evaluation was completed based on several criteria, most of which concerned user comfort and safety. The preferred alternative includes the following:

- 4.0 m wide MUT on the east side of Hespeler Road bridge;
- 3.0 to 3.5 m wide MUT approaches along the east boulevard;
- 1.5 to 2.0 m wide splash strip/buffer, where feasible; and
- a separation barrier between the MUT and the motor vehicle traffic lanes on the bridge.

Design treatments to mitigate potential conflicts at free-flow ramp crossings are consistent with MTO guidelines for accommodating active transportation at highway interchanges.

### 3.6 DRAINAGE

Storm sewers and highway ditches provide drainage for Highway 401 within the study area. Most of the existing highway storm sewers are located within the centre median with smaller storm sewer networks providing drainage for interchange ramps. Modifications to the existing highway drainage system are necessary to accommodate the widening of Highway 401. The proposed modifications include the replacement of median storm sewers, flat-bottom grassed swales and standard ditches.

The Stormwater Management (SWM) strategy was designed to minimize/mitigate environmental impacts due to the proposed highway improvements. Under the proposed design Highway 401 will be widened resulting in additional impervious surface (paved area). The proposed stormwater management system consists of grass swales and stormwater management facilities at the Hespeler Road and Townline Road Interchanges to provide water quantity and water quality controls.

### 3.7 ADVANCED TRAFFIC MANAGEMENT SYSTEMS

Advanced Traffic Management Systems (ATMS) will be included in this project, through the implementation of travel time sensors, closed circuit television (CCTV) cameras, and a permanent dynamic message sign (DMS). The DMS will be located along the Highway 401 westbound lanes near Wellington Road 35 to advise motorists of incidents and travel time information ahead. Additional CCTV cameras will be installed along the Highway 401 eastbound lanes near Franklin Boulevard and Townline Road and will be used to monitor road and traffic conditions, as part of the Ministry's network of traffic management cameras. Travel time sensors will be installed along the Highway 401 westbound lanes between Fountain Street North and Highway 8. The ATMS systems will be managed from the West Region Traffic Operations Centre in London, Ontario.

### 3.8 ILLUMINATION

In accordance with MTO *Policy for Highway Illumination PLNG-B-05*, an illumination warrant analysis for continuous illumination of Highway 401 within the study area was undertaken for the preferred design. It was concluded that high mast lighting is warranted throughout the study area. Therefore, median high mast illumination will be provided for the length of the project,

extending to approximately 1.75 km east of Townline Road. Additional high mast lights at Hespeler Road, Franklin Boulevard and Townline Road interchanges will be included to provide full illumination for the ramps. Replacement of some existing interchange high mast illumination at Hespeler Road is also required to accommodate the interchange design changes. New high mast lighting will be provided at the Franklin Boulevard and Townline Road interchanges to provide full illumination. Newly installed high mast lighting will be LED, rather than High Pressure Sodium (HPS).

### 3.9 TRAFFIC SIGNALS

Traffic Signals are currently located at two locations within the project limits and the existing traffic signals will be replaced meeting design standards. During construction, temporary signals will be required to maintain normal function of the following intersections:

- Highway 401 W-N/S ramp and Mall Entrance/Exit at Hespeler Road
- Highway 401 E-N/S ramp at Hespeler Road

### 3.10 UTILITIES

There are no pipelines present. The following utilities are present within the project limits:

- Energy+ aerial hydro is located along the west side of Franklin Boulevard and the west side of Townline Road. An aerial line also crosses Hespeler Road to the north of Highway 401. There are no conflicts with the aerial hydro.
- Rogers aerial cable is located on the Energy+ poles along the west side of Franklin Boulevard and on Hespeler Road to the north of Highway 401. Rogers aerial line is also located on Hydro One poles to the west of Townline Road. There are no conflicts with the aerial lines. There is also a buried fibre optic line to the west of Franklin Boulevard.
- Hydro One aerial hydro is located along the west side of Townline Road but is not in conflict with the proposed work.
- A buried Bell line is located along the east side of Hespeler Road that is also attached to the structure. Relocation will occur prior to construction. A buried Bell line located to the east of Townline Road will be relocated prior to construction.
- A buried Bell line is located along the west side of Franklin Boulevard but is not in conflict with the proposed work. There is also a buried Bell line located at the Townline Commuter Parking Lot and is not in conflict with the proposed work.
- A Union Gas gas main is located along the west side of Franklin Boulevard but is not in conflict with the proposed work. There is also a buried gas main located at the Townline Commuter Parking lot and is not in conflict with the proposed work.
- Hydro One Transmission towers cross Hespeler Road and the W-N/S ramp to the south of Highway 401 but are not in conflict with the proposed work.

- A RMW watermain is located east of Hespeler Road and west of Townline Road but is not in conflict with the proposed work.
- A RMW watermain is located east of Franklin Boulevard, and does not have a casing under the expanded Highway 401 footprint. The proposed work includes installation of a new casing for future use by RMW for a watermain relocation following this project. The contract will include RMW emergency contacts should an issue occur with the existing watermain.

### 3.11 CONSTRUCTION STAGING AND TRAFFIC MANAGEMENT

To maintain traffic safety and allow for efficient construction, road and ramp closures will be required during construction. The Construction Staging and Traffic Management Plan for Phase II improvements has been updated since the PIC, as summarized below.

- Year 1 (partial season only)
  - Advanced patching and median shoulder strengthening on Highway 401
  - Expansion of the commuter parking lot at Townline Road interchange
  - Rehabilitation of the Wellington Road 32 underpass
- Year 2
  - Replacement of the northbound Hespeler Road Bridge over Highway 401
  - Widening and reconstruction of Highway 401 from Hespeler Road to the horizontal curve east of Franklin Blvd, including completion of the horizontal curve realignment from approx. Sta 21+000 to Sta 21+700
  - Construction of some median storm sewer, tall wall and high mast lights
  - Realignment and paving of ramps at Hespeler Road interchange
  - Installation of the noise barrier at Franklin Boulevard W-S ramp
  - Three lanes of traffic in each direction on Highway 401 will be maintained, with reduced lane and shoulder widths
  - Hespeler Road traffic will be reduced to two lanes in each direction
- Year 3
  - Replacement of the southbound Hespeler Road Bridge and completion of approaches on Hespeler Road
  - Complete widening at reconstruction of Highway 401 from Hespeler Road to the horizontal curve east of Franklin Blvd, approx. Sta 21+000

- Construction of some median storm sewer, tall wall, high mast lights and overhead signs
- Complete ramp realignments to Highway 401 at Hespeler Road
- Three lanes of traffic in each direction on Highway 401 will be maintained, with reduced lane and shoulder widths
- Hespeler Road traffic will be reduced to two lanes in each direction
- Year 4
  - Completion of widening and reconstruction on Highway 401 from approx. Sta 21+700, east of Franklin Blvd, to east limit
  - Remove temporary overbuilding on Highway 401 used to stage traffic
  - Completion of median storm sewers, tall wall, high mast lights and overhead signs within the entire project limits
  - Completion of all I surface course paving and permanent signs and pavement markings
  - Implementation of Managed Lanes from Highway 8 to Townline Road
  - Three lanes of traffic in each direction on Highway 401 will be maintained, with reduced lane and shoulder widths
  - Townline Road will have reduced lanes during structural repairs, curb installation and resurfacing operations.

### 3.11.1 Anticipated Closures

The following provides an overview of the anticipated closures:

#### Full Highway and Hespeler Road Closures

- Year 2 – for Hespeler Road NBL structure demolition, and girder placement
- Year 3 – for Hespeler Road SBL structure demolition, and girder placement
- Two full closures during a non-holiday weekend from 21:00 Saturday to 10:00 Sunday for bridge demolition
- Four full closures during a non-holiday weekday from 23:00 to 05:00 for girder placement
- OPP and Waterloo Regional Police are to be engaged for full highway closures and control at the following key intersections:
  - Hespeler Road North Ramp Terminal
  - Hespeler Road South Ramp Terminal
  - Hespeler Road and Pinebush Road

- Franklin Boulevard and Holiday Inn Drive/Jamieson Parkway
- Pinebush Road and Townline Road
- One roaming officer from each of the OPP and Waterloo Regional Police

#### Ramp Closures

- Hespeler Road Interchange
  - W-N ramp closed in Year 1 for approx. 2 weeks
  - Mall entrance and exit closed in Year 1 for approx. 2 weeks
  - Required for constructing the new south ramp terminal intersection, including grade raise
  - Mall exit reduced to one lane in Year 2 and Year 3 during Hespeler Road bridge replacements
- Franklin Boulevard Interchange
  - S-W and W-S ramps closed in Year 2 for approx. 4 months. This is required to complete widening of Highway 401 outside lanes and maintain Hespeler Road ramps
- Townline Road Interchange
  - N-W, W-N/S, N-E and S-W ramps closed in Year 3 for approx. 6 weeks
  - Required to complete widening of Highway 401 outside lanes, repaving of the Townline Road ramps, and repairs to the Townline Road underpass
  - E-N/S and S-E ramps closed in Year 3 for approx. 1 week for pavement rehabilitation
- Other periodic short duration ramp closures will be required throughout construction

#### Wellington Road 32 Underpass

- Overnight closures of Wellington Road 32 at Highway 401 will be required for structural soffit repairs for approximately 8 weeks
- Local traffic can use Townline Road during these overnight closures via Wellington Road 34 (north of Highway 401) or Wellington Road 32 (south of Highway 401).

### 3.11.2 Traffic Management

The traffic management for this project will focus on advance notification of the users of the study area roadways and area residents of changes to the available ramps and lanes at various stages of construction. Drivers will be able to select alternate routes when their preferred route

through the Highway 401 is not available or is constrained during construction. Formal detour routes will only be used for closure of Highway 401.

## 4.0 Environmental Issues and Commitments

This section focuses on the direct and indirect environmental effects associated with the construction of the project. In general, impacts to the natural, social, and cultural environment were minimized during planning and design by following these three principles:

- Avoidance;
- Identification of roadway design elements at key locations that can minimize environmental impacts; and
- Development of generic environmental protection plan guidelines for consideration during and following construction.

The design, construction, and operation/maintenance phases of this project involve typical activities for which potential environmental impacts are predictable and known environmental protection measures are applied. All commitments made during the Preliminary Design have been reviewed and addressed. Details on how environmental impacts will be mitigated, either through the use of environmental design or through environmental constraints contained in the contract package are included in the following sections.

Design changes made during Detail Design do not compromise or contradict the intent of the Preliminary Design mitigation or the EA commitments in the TESR.

### 4.1 NATURAL ENVIRONMENT

The existing environmental features as described in the TESR (2012) were taken into consideration during this study and updated based on supplementary field investigations and background studies undertaken in Spring 2014, Summer 2017 and Fall 2018.

#### 4.1.1 Terrestrial Resources

Field investigations were undertaken by Stantec in Spring 2014, Summer 2017 and Fall 2018 to confirm existing conditions as documented in the Terrestrial Ecosystems Report (Ecoplans, 2013) for the Preliminary Design study.

The following features were identified in the right-of-way:

- Puslinch Lake Irish Creek PSW Complex; including provincially rare Winterberry Organic Deciduous Thicket Swamp;

- occurrence of gray-headed prairie coneflower (S3), a provincially significant plant species was observed on the south side of the eastbound ramp to Townline Road; and
- occurrence of Phragmites identified within the Highway 401 right-of-way along drainage features, in ditches and extended into the right-of-way from the adjacent PSW.

Category 2 habitat for Blanding's Turtle was identified by the MNRF in the wetlands in the Study Area. This includes the PSW between Townline Road and Wellington Road 32 (Puslinch Lake/Irish Creek Wetland Complex), and shallow water wetlands between Franklin Boulevard and Townline Road (Shallow Aquatics features).

#### 4.1.1.1 Potential Impacts

##### Vegetation

Vegetation clearing for Phase II construction will be completed by MTO through an advanced contract prior to April 1, 2019. Proposed works involving structural replacements will require temporary and short-term disturbance to vegetation cover and terrestrial habitat during construction. The vegetation removals associated with the Highway 401 improvements are considered minor due to the abundance, tolerance, and generally disturbed nature of these features.

The proposed improvements will require the clearing of approximately 2.67 ha of shrub and tree cover. Grading activities will also result in the loss of cultural meadow habitat in the right-of-way.

##### Environmentally Sensitive Areas

Planned construction work areas overlay the Puslinch Lake Irish Creek PSW Complex east of Townline Road. Approximately 0.26 ha of Provincially Significant Wetland (PSW) is proposed for removal to accommodate the Phase II works. The PSW will be identified as an Environmentally Sensitive Area in the contract. Erosion and sediment control measures will be implemented throughout the work area.

##### Habitat for Species of Conservation Concern

Habitat for species of conservation concern (SOCC) includes habitat for those species that are not covered under the Ontario *Endangered Species Act, 2007* (ESA). SOCC with the potential to occur in the Study Area were identified through MNRF consultation, the NHIC database search and the results of field investigations. Potential impacts identified include the following.

- **Snapping Turtle** – Although there were no direct observations of the species, and potential overwintering habitat is located outside of the ROW, there is a possibility that construction activities may interact with turtle movement. Turtles may be particularly vulnerable during peak activity periods, including movement between wintering and nesting sites. Nesting

activity typically occurs during the summer months (approximately June through September).

- **Eastern Ribbonsnake** – Construction activity may result in direct mortality to snakes. Snakes may be vulnerable during hibernaculum emergence, re-entrance and basking periods and may preferentially seek out construction materials to bask under. Although Eastern Ribbonsnake individuals were not observed in the Study Area, potential habitat is present in wetlands and swamps east of Townline. Peak activity for Eastern Ribbonsnake is typically between late April and late June (MNR 2012). Candidate hibernacula occurred as fenced cobble slopes adjacent to culverts, to aid in stabilization of embankments.
- **Milksnake** - Construction activity may result in direct mortality to snakes. Snakes may be vulnerable during hibernaculum emergence, re-entrance and basking periods, and may preferentially seek out construction materials to bask under. Although there were no natural hibernacula features or old building foundations identified in the ROW that have the potential to provide snake hibernacula; suitable habitat occurs throughout the Study Area, especially near open aquatic features. Peak activity for Milksnake is typically between late April and late June (MNR 2012). Candidate hibernacula occurred as fenced cobble slopes adjacent to culverts, to aid in stabilization of embankments.
- **Gray-headed Prairie Coneflower** – Approximately 82m<sup>2</sup> of this species will be impacted by grading activities, including an offset of 1.0 m for tree protection barrier. The impact for the permanent grading is 48m<sup>2</sup>. The species is likely planted and not native, therefore replanting is not required.
- **Monarch** – habitat for this species was observed in the cultural meadow of the ROW. Temporary disturbance to the ROW will occur during construction; however, the ROW will be restored back to meadow habitat post-construction.
- **Western Chorus Frog** - Construction activity may result in direct mortality to frogs. Western Chorus Frog may be vulnerable during dispersal to and from breeding wetlands, and breeding wetlands may be vulnerable to sedimentation during construction. Peak activity for Western Chorus Frog is typically between mid-March to April and late May (Environment Canada 2008).

## Threatened or Endangered Species

Habitat assessments were completed to determine if suitable habitat for threatened or endangered species may be present in the Study Area. Potential impacts were identified as follows:

- **Blanding's Turtle** – Although there were no direct observations, Blanding's Turtle may occur in the Study Area and construction activity could result in direct mortality. Potential overwintering habitat for Blanding's Turtle was present adjacent to the Study Area. The PSW east of Townline Road may also provide Category 2 habitat for Blanding's Turtle. Turtles may be particularly vulnerable during peak activity periods, including movement between wintering and nesting sites. Nesting activity typically occurs during the summer months (approximately June through September).

- **Little Brown Myotis and Tri-colored Bat** – Within the Study Area, potential bat maternity roost trees may be present in the FODM5 communities between Hespeler Road and Franklin Boulevard. Vegetation removal of 0.028 ha is proposed for the FODM5 community on the north side of the highway. Due to the small amount of removal of potential bat habitat, long-term impacts to this species are not anticipated, especially if tree removals occur outside of the breeding window for bats (May 1 to September 30, conservatively).

## Migratory Birds

Although field investigations did not identify the nests of any protected species, the vegetation communities in the Study Area are likely to support nesting migratory birds on an annual basis. All work near active migratory bird nests has the potential to disturb nesting behavior or damage/destroy the nests. There were no nests present on culverts or bridge structures in 2014, 2017 and 2018 indicating a low likelihood for future use; however, isolated nests may still be constructed in subsequent avian breeding seasons particularly if conditions in proximity to the culverts are changed (e.g. vegetation removals from the culvert openings). Pre-tarping for Phase II construction will be completed by MTO through an advanced contract prior to April 1, 2019.

### 4.1.1.2 Mitigation Measures

#### Erosion and Sediment Control

Mitigation measures for sedimentation, erosion, and dust control should be implemented to prevent sediment and dust from entering sensitive natural features (i.e., downstream effects to the Puslinch Lake Irish Creek PSW Complex). See **Section 4.2**.

#### Vegetation Protection

During construction adjacent to vegetated areas, heavy equipment could damage peripheral vegetation from contact, excavation and/or soil compaction. In areas where sensitive natural features occur adjacent to construction activities, barriers for tree protection (e.g. snow fencing) should be erected to protect vegetation that is to be retained. Barrier fencing should be established at, or beyond, the treed drip-line, as appropriate. Areas to be cleared of existing vegetation should be clearly marked to prevent unnecessary clearing. Barrier for tree protection may be coincident with silt fencing used to control erosion and sediment transport at the site in areas of grading.

#### Invasive Phragmites Control

Phragmites is an invasive species and was identified throughout the Study Area, typically in low-lying areas. MTO is striving to control Phragmites found in highway corridors (including ditches and some medians) within West Region by using an Integrated Pest Management Plan, which

includes chemical control (i.e. pesticide application) and mechanical control (i.e. mowing). Pesticide application will be focused only on Phragmites plants within the MTO ROW.

A Special Provision will be included in the Contract to ensure that all vehicles and equipment exposed to Phragmites is cleaned prior to leaving the construction site to control the spread of the plant.

### PSW Protection

The Puslinch Lake Irish Creek PSW Complex overlaps with the Study Area and a small portion (0.26 ha) is to be removed to facilitate construction. It is recommended that the work area and grading near the PSW be limited to reduce potential impacts. Other recommended site-specific protection for the PSW includes demarcating the extent of the construction at the wetland, and installation of paige wire supported sediment and erosion control fencing.

### Protection of Migratory Birds

The core nesting period for migratory bird species is identified as April 1 to August 31 (Environment and Climate Change Canada 2014) although nesting could occur outside of this period. MTO – West Region has identified April 1 to August 31 as the restricted period of vegetation clearing; however, activity must avoid active nests at any time.

Although no nests were observed on structures, it is possible that nests could be established in subsequent years. Pre-tarpping for Phase II construction will be completed by MTO through an advanced contract prior to April 1, 2019. These measures are to be maintained for the duration of the migratory bird nesting season or until the work on structures is complete.

### Myotis Bat Species Protection Measures

Individuals of Little Brown Myotis, Northern Myotis and their habitats are (i.e., maternity roost trees) are protected from harm or harassment under general habitat regulations of the ESA. Trees >25 cm diameter breast height with suitable cavities and/or peeling bark may provide maternity roost habitat within the Highway 401 ROW. Because there is a possibility that bat maternity roosts may be present in the Study Area, a timing restriction for vegetation clearing is recommended to avoid the core maternity season (April 1 to August 15) and reduce the potential construction phase impacts to bats. Vegetation clearing for Phase II construction will be completed by MTO through an advanced contract prior to April 1, 2019. If any additional vegetation removal is proposed, then these areas should be surveyed by a qualified biologist prior to removal.

### Wildlife Protection

Standard environmental protection measures for erosion and sediment control (Section 4.2) will also serve as a wildlife barrier where structure replacements encroach into areas of natural

vegetation. Exclusionary fencing has been recommended when an aquatic feature is within the ROW for Phase II works.

Meadow habitats within the Study Area have the potential to be used by Monarch. Because this type of meadow habitat is readily available, site specific mitigation has not been considered for this project.

#### 4.1.2 Fish and Fish Habitat

Field investigations were undertaken by Ecoplans in Fall 2009 and Spring 2010 to document existing conditions as documented in the Fish and Fish Habitat Impact Assessment Report (Ecoplans, 2013) for the Preliminary Design study. A fisheries habitat assessment was later conducted in October 2018 to confirm existing conditions as documented in the Ecoplans 2013 report.

The 2018 updates are site-specific to three non-structural culverts and one structural culvert that cross Highway 401 between Townline Road and Wellington Road 32, within the limits of the Study Area that were extended to Wellington Road 32 Underpass. Based on the information available from the Preliminary Design phase of the project (Ecoplans 2013) and additional background data sources reviewed for potential updates, no fish and fish habitat constraints are present in the originally defined project limits between Hespeler Road and Townline Road.

Based on the fisheries habitat assessment conducted on October 4, 2018, fish habitat was associated with two culverts within the study area including Culvert C8 (Station 10+663) (south side of Highway 401 only), and Culvert C9 – Irish Creek (Station 11+400).

##### 4.1.2.1 Mitigation Measures

Works adjacent to aquatic resources that provide fish habitat, or have the potential to support fish habitat, are often restricted to certain periods to avoid construction-related impacts to fish species during their most sensitive/vulnerable life cycles (i.e. during reproduction and early development stages of off-spring). Therefore, construction activities are often not permitted close to or within fish habitats during these periods. The MNRF provided an in-water timing window of May 16 to March 14 (no work allowed from March 15 to May 15, inclusive) (MNRF 2018a).

Activities associated with ditching will follow the MTO BMP for *Ditch Maintenance Within 30 Metres of a Waterbody* as per the *Best Management Practices Manual for Fisheries* (MTO 2016b). The following operational constraints and protection measures are applicable to ditch maintenance (MTO 2016b):

- *Ditch maintenance activities shall not be conducted within the receiving waterbody or within a wetland and shall only be conducted when/if the ditch is dry*

- *Ditch maintenance activities shall be conducted during dry weather and when heavy or persistent precipitation is not expected to occur*
- *An Erosion and Sediment Control Plan shall be developed and implemented for the site that reduces the risk of sedimentation of the waterbody during the project. A response plan shall also be developed that is to be implemented in the event of a sediment release*

Activities associated with vegetation removal (if required) will follow the BMP for *Maintenance of Riparian Vegetation in Existing Right-of-Way* (MTO 2016b). Operational conditions, constraints and protection measures for vegetation removal are provided in the BMP.

The MTO BMP for Ditch Maintenance is applicable to work on the south side of Highway 401 at Station 10+663 (Step 3 of the Protocol) and the Project Notification form can be completed at Step 5. The BMP should also be applied to ditching on the north side of Highway 401, and the culvert extension should be replaced when there is no flowing water at this location, or during the in-water work timing window (no work allowed from March 15 to May 15, inclusive).

The MTO BMP for Ditch Maintenance is applicable to ditch maintenance activities on the south side of Highway 401 at Station 11+400; however, a fisheries assessment was completed at Step 4 of the Protocol for construction of the wing walls at the culvert inlet. The assessment determined that serious harm to fish is unlikely and the Project Notification Form can be completed (Step 5 of the Protocol).

### 4.1.3 Surface Water Protection

Irish Creek is located within the Phase II study area (Station 11+400) and utilizes Culvert C9. According to the *Fish and Fish Habitat Impact Assessment Report* prepared during the Preliminary Design (2013), the watercourse is permanent and originates approximately 650 m south (upstream) of the highway in Puslinch Lake. From the lake, the watercourse flows as a defined wetland channel through the Irish Creek/Puslinch Lake Wetland Complex on both sides of the highway. South of the highway, the watercourse crosses under County Road 32 via twin cell culverts. North of the highway, the channel flows to the northwest through the PSW and then forest and agricultural fields, eventually outletting to the Speed River approximately 4.4 km further downstream.

The drainage design was undertaken in a manner that minimized any impacts to hydrologic characteristics of adjacent areas. Relevant standard construction mitigation measures will ensure that surface water quality is protected during construction. These measures are outlined in **Section 4.2**.

## 4.2 EROSION AND SEDIMENT CONTROL

Erosion and sediment control measures will be implemented during all phases of construction to prevent sediment-laden runoff from entering drainage ditches, culverts, and to the adjacent woodlot areas during excavation and grading activities associated with construction. Erosion and sediment protection will be provided during and after construction in accordance with applicable Best Management Practices (BMPs) outlined in MTO's *Environmental Guide for Erosion and Sediment Control During Construction of Highway Projects* (2015).

Erosion Control BMPs are intended for application to exposed soil where there is high potential for erosion and are intended to prevent erosion at the source to reduce the amount of sediment that needs to be managed by downstream sediment control measures. Cover is the most effective erosion control practice to protect soil from erosion by wind, rain splash, and overland flow. Sediment Control BMPs are intended for application to flowing water where there is a need to retain mobilized sediment and prevent water with excessive sediment from leaving the construction site. These measures should be installed close to the sediment source to reduce the quantity of water that must be managed and reduce the consequences of a failure. Sediment control can be accomplished by filtering or settling sediment-laden run-off water.

Various mitigation techniques will be employed during construction to reduce the risk of impacts to natural environment features. The primary principles associated with sedimentation and erosion protection measures are to: (1) minimize the duration of soil exposure; (2) retain existing vegetation, where feasible; (3) encourage re-vegetation; (4) divert runoff away from exposed soils; (5) keep runoff velocities low; and to (6) trap sediment as close to the source as possible. To address these principles, the following mitigation measures are proposed:

- Silt fencing will be used when exposed soil slopes are at risk of eroding;
- No equipment will be permitted to enter any natural areas beyond tree protection fencing;
- All materials requiring stockpiling (fill, topsoil, etc.) will be stabilized and kept at a safe distance (> 15 m) from drainage features, and the top of steep slopes;
- If there is insufficient time remaining in the growing season, sites shall be stabilized with temporary erosion control measures such as erosion control blankets and seeded the following spring. Erosion and sediment control measures shall be maintained until vegetation has been established in disturbed areas;
- Refueling of equipment will be carried out away from natural features with the largest buffer possible (minimum 30 m) to avoid potential impacts, in the event that an accidental spill occurs;

- In addition to any specified requirements, additional silt fence should be available on-site, prior to grading operations, to provide a contingency supply in the event of an emergency;
- All sediment and erosion controls should be monitored regularly and properly maintained, as required. Controls are to be removed only after the soils of the construction area have been stabilized and adequately protected until cover is re-established;
- The limits of construction adjacent to natural features to be retained will be delineated prior to construction, and monitored during construction (along with sediment and erosion control measures) to ensure the limits are maintained with respect to vehicular traffic and soil or equipment stockpiling; and
- The Contractor is required to restore any disturbed natural areas to pre-construction conditions.

### 4.3 MANAGEMENT OF EXCESS MATERIALS

Excess material will be generated during construction and may include old pavement, concrete, and structural components through the removal of existing features throughout the study area. These excess materials and debris should be removed and disposed of at an approved facility, in accordance with Ontario Provincial Standard Specification (OPSS)180. Standard mitigation measures will be used for dust control during construction.

Should any other suspected designated substances, such as asbestos, silica, or lead-based paint be encountered during the construction of the project, the Contractor should assess and manage the designated substances prior to any disturbances, in accordance with SSP 101 F21 – Occupational Health and Safety Act - List of Designated Substances and with Ontario Regulation 490/09 and 278/05 (Ministry of Labour, 2010) under the *Occupational Health and Safety Act*.

### 4.4 HYDROGEOLOGIC AND HYDROLOGIC ASSESSMENT

Hydrogeological site conditions were documented in the 2012 TESR and a Hydrogeologic and Hydrologic Assessment was completed in December 2018 in support of the construction dewatering procedure required for the proposed works.

#### 4.4.1 Groundwater Dewatering Requirements

Groundwater dewatering is anticipated for construction of the central caisson at the center pier for the Hespeler Road Bridge, a gabion retaining wall at the Puslinch Lake Creek Culvert (Site 35-592/C) to accommodate lane widening on the south side of Highway 401, and other dewatering works as necessary.

The replacement of the Hespeler Road Underpass and the gabion retaining wall will require a maximum of up to 120 separate days of groundwater dewatering over an approximately 2-year construction period. The proposed method for the control of groundwater seepage into the excavation area is dewatering on an as-required basis using submersible pumps, trash pumps, or equivalent. It is the Contractor's responsibility to determine the location and extent of the groundwater dewatering.

During any dewatering, pumping rates will be monitored and recorded by the Contractor based on pump size/ rating curves, field measurements of flow rate as possible, duration of pumping each day, and/or an equivalent calculation.

The following assumptions were made when performing the average day dewatering calculations for the Hespeler Road Interchange:

- The groundwater level was estimated as 287.7 m based on the observed groundwater level in BH 14-2 (Stantec, 2018).
- The hydraulic conductivity was estimated at approximately  $2 \times 10^{-4}$  m/s for sand and gravel till.
- Dewatering to 284.0 m, the depth of bedrock and foundation for caisson at BH 14-5.
- The base of the aquifer was estimated at 284.0 m based on the termination of the sand and gravel layer to limestone bedrock at BH 14-5.

The following assumptions were made when performing the average day dewatering calculations for the Puslinch Lake Creek gabion retaining wall:

- The groundwater level was estimated as 304 m based on the water levels in Puslinch Lake Creek.
- The hydraulic conductivity was estimated at approximately  $1 \times 10^{-6}$  m/s for sand and gravel till.
- Dewatering to 302.5 m, approximately 1 m below the depth of the foundation.
- The base of the aquifer was estimated at 292.0 m based on the termination of the borehole in BH 1.

#### 4.4.1.1 Stormwater Takings

Stormwater may need to be managed during construction, especially after heavy rain or snow melt events. A review of surface water flow within Puslinch Lake Creek indicates a drainage area

of approximately 12 km<sup>2</sup> and a mean annual flow of 0.13 m<sup>3</sup>/s (11,232,000 L/day) based on MNRF's *Ontario Flow Assessment Tool*. Temporary construction dewatering of up to 6,000,000 L/day may be required to manage groundwater and surface water seepage. In the event of higher surface water levels following a large rain event, additional surface water seepage would also be expected. This seepage is difficult to quantify as it is dependent on Contractors diversion set-up. The predicted pumping rates are summarized in Table 4.

Table 4: PTTW Taking Estimates

	Average Groundwater Pumping Rate		Maximum Groundwater Pumping Rate		Maximum Pumping Rate (Surface Water and Groundwater)	
	L/day	L/min	L/day	L/min	L/day	L/min
<b>Hespeler Road Underpass</b>	144,000	100	1,225,000	850	1,225,000	850
<b>Puslinch Lake Creek Culvert Gabion Retaining Wall</b>	14,000	10	1,535,000	1065	6,000,000	4,170
<b>Total</b>	158,000	110	2,760,000	1,915	7,225,000	5,020

#### 4.4.2 Management of Discharge

##### 4.4.2.1 Groundwater Quality

The Contamination Overview Study (COS) (Ecoplans, 2013) identified several potential sources of groundwater contamination in the vicinity of the study area, including activities such as road de-icing, a historic dump, a historic rail line, and industrial facilities. The COS recommend that groundwater discharge be tested for polycyclic aromatic hydrocarbons (PAH), petroleum hydrocarbons (PHC), metals, salt related parameters (Sodium Adsorption Ratio, Sodium, Chloride, Electrical Conductivity), and total and dissolved metal concentrations at the start of dewatering to confirm concentrations. It is the Contractor's responsibility to ensure that water quality meets applicable criteria.

##### 4.4.2.2 Surface Water

The Hespeler Road Underpass replacement will not require any diversion of surface water. It is considered unlikely that the construction dewatering activities will result in negative impacts to surface water quality.

The Puslinch Lake Creek gabion retaining wall is located adjacent to Puslinch lake Creek. A flow barrier is anticipated between the watercourse and the gabion retaining wall with passive diversion of surface water flows during construction.

Erosion control / energy dissipation measures will be implemented at the proposed discharge location to disperse flows across a broad area minimizing the potential for scour and erosion, and for sediment transport and deposition within the watercourse. In general, it is the Contractor's responsibility to ensure that both groundwater and surface water quality criteria are met.

#### 4.4.3 Potential Impacts

##### 4.4.3.1 Private Water Supply

A review of MECP Water Well Records (WWR) (2018) indicated 40 water well records located within 500 m of the bridge replacement and gabion retaining wall sites. Based on MECP WWR there are four wells located within 300 m (the expected radius of influence during the maximum pumping rate) of the bridge replacement (WWR 7238968, 7251530, and 7274525) which are summarized as follows:

- Well 7137703 exists to the northeast of the Site and extends to a depth of 10.6 m BGS. The well is installed primarily in a sand and gravel material and is used for monitoring purposes
- Well 7274525 exists to the northwest of the site and extends to a depth of approximately 16.7 m. The well was installed in a primarily sand-gravel material and is used for monitoring purposes
- Two wells (7238968 and 7251530) exists to the southwest of the site
  - Well 7238968 was bored in sand and gravel material to a depth of 10.6 m and issued for monitoring purposes
  - Well 7251530 is abandoned

As these MECP water wells are all used for monitoring purposes or abandoned, it was deemed that dewatering at the Hespeler Road Underpass will have negligible impacts on groundwater supply in the vicinity of the site.

There are no wells within 110 m (the approximate expected radius of influence during the maximum pumping rate) of the Puslinch Lake Creek gabion retaining wall (Site 35-592/C). Based on the distance and depth of the private wells and the limited extent of pumping influence, well impacts are not anticipated due to the proposed dewatering.

##### 4.4.3.2 Municipal Water Supply

Review of the MECP WWR database indicates there are no municipal wells located within 500 m of the project area. According to the Grand River Conservation Authority, the site is located

within a Well Head Protection Area (WHPA). The proposed construction activities for the bridge replacement project are not anticipated to result in adverse interference to municipal water supply systems.

#### 4.4.4 Mitigation Measures

MTO is implementing mitigation measures to limit unacceptable impacts to the surrounding environment during groundwater dewatering. Regular visual inspections of the discharge location and work area will be completed to document compliance with the MTO's mitigation measures.

Standard construction mitigation measures will be implemented throughout construction including, but will not be limited to the following:

- Erosion and Sediment Control (ESC) measures (e.g. silt fences, fibre tubes) will be implemented, as required, prior to, and maintained during the construction phases, to prevent erosion and sedimentation
- The ESC measures may need to be upgraded and/or amended as site conditions change to minimize sediment laden runoff from leaving the work areas
- Additional ESC supplies are to be kept on site and be used as necessary
- Disturbed areas will be minimized to the extent possible, and temporarily or permanently stabilized or restored as the work progresses
- No equipment will be permitted to enter any natural areas beyond the silt fencing or tree protection fencing (site boundaries) during construction
- All materials requiring stockpiling (fill, topsoil, etc.) are to be stabilized and kept a safe distance from watercourses and natural areas
- All disturbed areas will be stabilized and revegetated as required upon completion of work and restored to a pre-disturbed state or better

During the completion of construction work, the potential wastes and spills shall be managed to ensure that there is minimal impact to the natural environment from the project. The following mitigation measures are to be followed:

- Activities, including equipment maintenance, will be controlled to prevent the entry of petroleum products, debris, rubble, concrete or other deleterious substances into the surface water
- Refueling and maintenance of vehicles will be conducted a minimum of 30 m from water bodies
- The Contractor shall ensure that all waste materials from construction activities shall be safely disposed of through an appropriate waste management facility and in compliance with the MECP regulations

In the event of a spill of fuel or other hazardous material during construction, remedial actions must be undertaken immediately. The Contractor shall have spill containment kits on the project site. Spills with the potential to create an impact to the environment should be reported to the MECP as required by the provincial spills legislation.

## 4.5 SOCIO-ECONOMIC ENVIRONMENT

### 4.5.1 Adjacent Land Use / Property

The land surrounding the highway within the study area has varying designations, but is primarily comprised of agricultural lands, with some residential, commercial and industrial areas. Residents, commuter traffic, local businesses and property owners may experience temporary disruption during construction due to detours. However, access to private properties within the study area will be maintained at all times during construction.

There is an existing SmartCentres mall located on the southeast side of the Highway 401/Hespeler Road interchange. Work at the interchange will require the temporary full closure of the entrance to the mall at the south ramp terminal. The Contractor shall provide SmartCentres with two weeks' notice prior to commencing work at the existing mall located on the southeast side of the Highway 401/Hespeler Road interchange.

### 4.5.2 Noise

There are several noise sensitive receivers within the study area. The noise assessment undertaken during the Preliminary Design study concluded that there are anticipated increases in noise levels at the receiver locations as a result of mainline (Phase II) construction including the widening of Highway 401 (McCormick Rankin, 2012). The noise assessment recommended a 270 m long noise barrier be constructed along the south side of Highway 401, west of Franklin Boulevard.

Following the Public Information Centre (PIC), a significant number of comments were received related to noise concerns. Several comments came from the area southeast of the Townline Road interchange, and residential areas north of Highway 401 east of Franklin Boulevard. Based on the number of comments received, the Ministry is reviewing the previously completed noise assessment. Since this area is located east of widening of Highway 401 being completed as part of this project, any revised recommendations resulting from the updated assessment will be considered for future expansion work east of Townline Road.

The City of Cambridge Noise By-Law 32-04 prohibits any unreasonable or unnecessary noise resulting from excavation or construction operations between 6:00 PM and 7:00 AM without written permission from the City.

#### 4.5.2.1 Potential Impacts and Mitigation

The nature of night work will primarily include setup and take down of traffic control measures for detours, removal of asphalt pavement, and paving operations. This includes setup of temporary concrete barriers, temporary signs, and pavement markings. In addition, night work is required for the reconstruction of the Highway 401 interchange at Hespeler Road and the reconstruction of ramp tie-ins at Franklin Boulevard and Townline Road interchanges. Overnight full closures of Highway 401 will also occur in order to accommodate the demolition of the existing Hespeler Road bridges and to install girders for the new structures. Specific timing and details of these overnight full closures are to be confirmed by the Contractor.

During night-time construction, the primary sources of noise will include generators to provide night-time lighting and operation of dump trucks and diesel-powered equipment for loading/unloading, demolishing, etc. During the overnight closures for the demolition of the Hespeler Road bridges and placing the girders for the new structures, there will be significant noise related to removal equipment such as hoe rams and cranes.

In order to accommodate the required night work, a formal request has been made to the City of Cambridge to approve an exemption from Noise By-Law 32-04 pertaining to prohibited hours for construction. The Contract Administrator will monitor and investigate any complaints regarding construction noise according to the provisions of the MTO's Environmental Guide for Noise.

#### 4.5.3 Air Quality

Air quality concerns are related primarily to total suspended particulate (TSP) and dust fall impacts caused by construction activities. Other dust emissions may result from bridge demolition activities, the movements of construction vehicles other than grading equipment, and wind erosion of stockpiles and exposed areas.

Every measure should be taken to minimize emissions and prohibit visible emissions from escaping beyond the contract limits of the construction site. A dust control plan for the construction phase will be developed in accordance with standard MTO procedure.

Dust and air quality impacts to properties adjacent to the study area during construction will be minimized by General Conditions in the contract. These include: minimizing operation and idling of gas-powered equipment vehicles, minimizing vehicular traffic on exposed soils and stabilizing high traffic areas with suitable cover material, restoring disturbed areas as soon as possible to minimize the duration of soil exposure, and controlling dust emissions by the application of suitable MECP approved dust suppressant materials.

## 4.6 CULTURAL ENVIRONMENT

### 4.6.1 Archaeological Resources

An archaeological assessment was undertaken for lands associated with the Approved Plan during the Preliminary Design study. During construction or future maintenance activities, there is always the chance of encountering deeply buried archaeological material. If this occurs, the MTCS should be informed and the work in the vicinity of the discovery must be suspended immediately. In addition, if unmarked human remains are encountered the provisions of the *Ontario Cemeteries Act* apply. Notification must be made to the OPP, or local police, who will conduct a site investigation and contact the District Coroner. Notification to the MTCS and the Registrar of Cemeteries shall also be undertaken.

Should other cultural heritage values (archaeological or historical materials or features) be identified during construction operations, all activity in the vicinity of the discovery will be suspended and the MTCS archaeologist shall be contacted.

## 4.7 UTILITIES

As noted in Section 3.10, existing utilities have been reviewed. Necessary utility relocations have been coordinated with the utility owners.

An existing Bell line is attached to the east side of the Hespeler Road structure which will be demolished in this project therefore was identified as a conflict. The relocation will move the line off the structure and will go under Highway 401 to the east of Hespeler Road. The depth of the line will clear the proposed highway grading.

An existing Bell buried line is located to the east of Townline Road. The depth of the existing line was identified as a conflict with the proposed grading. The relocated line will be at a depth that will clear the proposed highway grading and the grading required at the Townline Commuter Parking Lot.

All utility relocations will be completed prior to the spring of 2019.

## 4.8 CONSTRUCTION STAGING AND TRAFFIC MANAGEMENT

Construction staging and traffic management will be required for the construction of the proposed improvements.

Motorists and commercial haulers traveling through the study area may experience temporary delay or disruption during construction. Advanced signing of construction and detour routes will alert drivers of the potential for a slowdown. During construction, traffic conditions will be

monitored to make sure that unreasonable delays and back-ups are not occurring. Corrective action will be taken as required to remedy any potentially unsafe situations.

The following measures will be included in the Contract to keep traffic delays to the traveling public and commercial truck traffic to a minimum while maintaining safety through the study area:

- Provision of an emergency contact person available 24/7 to assist with emergencies;
- The Contractor shall provide SmartCentres with two weeks' notice prior to commencing work at the existing mall located on the southeast side of the Highway 401/Hespeler Road interchange. Provision of enhanced signing for SmartCentres during closure of the mall access;
- During full highway closures, ensure additional police officers are available to assist with traffic movements at key intersections;
- Use of PVMS to notify the public of upcoming closures;
- The City of Cambridge, Grand River Transit, local student transportation services; local industrial/commercial operations and emergency response agencies (EMS) will be provided advance notice of the construction schedule; and
- The Contractor shall invite all local EMS representatives from RMW to monthly meetings to confirm any and all work operations that may impact the temporary EMS ramps at Hespeler Road.
- Advanced notice will be provided to transportation companies, giving the location and dates that a detour/construction zone will be in effect.

In addition to the above measures, the project website ([highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)) will provide a link to the Ontario 511 - Traveller Information Services website which gives easy 24/7/365 access to MTO road information on provincially maintained highways.

## 4.9 ENVIRONMENTAL EXEMPTIONS AND PERMITS

An exemption from the City of Cambridge Noise By-Law 32-04 is being sought prior to construction.

Ontario Regulation (O. Reg.) 64/16 and O. Reg. 63/16, the MECP requires a Permit to Take Water (PTTW) for dewatering in excess of 400,000 L/day. A PTTW application has been submitted to MECP.

## 4.10 SUMMARY OF ENVIRONMENTAL PROTECTION/MITIGATION REQUIREMENTS DURING CONSTRUCTION

The project can be completed without significant adverse impacts to the existing natural, socio-economic and cultural environment. The commitments made herein will be documented in the contract package as part of the drawings, tender or referenced attachments. Standard ministry special provision and applicable non-standard special provisions will be included as warranted.

Environmental protection/mitigation and environmental monitoring are integral components of the Detail Design for this project. To the extent possible, adverse impacts can be avoided or mitigated by the measures and provisions included in the contract. A summary of environmental effects, proposed mitigation and commitments, as identified at the end of this study is provided in Table 5.

### Legend

MTO: Ministry of Transportation  
MNRF: Ministry of Natural Resources and Forestry  
MECP: Ministry of Environment, Conservation and Parks  
MTCS: Ministry of Tourism, Culture and Sport  
GRCA: Grand River Conservation Authority  
OPP: Ontario Provincial Police  
EMS: Emergency Services  
RES/BUS: Local residents and/or businesses  
CITY: City of Cambridge  
REGION: Region of Waterloo  
PUS: Township of Puslinch  
UTIL: Utilities

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Table 5: Summary of Environmental Concerns and Commitments

ID	Issues/Concerns/ Potential Effects	Concerned Agencies	Mitigation/Protection/Monitoring
<b>1.0 Erosion and Sediment Control</b>			
1.1	<p>Potential for sediment-laden runoff during construction.</p> <p>Minimize the potential for erosion of newly exposed cut/filled slopes.</p>	MTO MNRF	<ul style="list-style-type: none"> <li>• Erosion and sediment protection measures included on contract drawings</li> <li>• Silt fencing will be used when exposed soil slopes are at risk of eroding, as per the contract drawings</li> <li>• No equipment will be permitted to enter any natural areas beyond tree protection fencing</li> <li>• All materials requiring stockpiling (fill, topsoil, etc.) will be stabilized and kept a safe distance from drainage features, and the top of steep slopes</li> <li>• If there is insufficient time remaining in the growing season, sites shall be stabilized with temporary erosion control measures such as erosion control blankets and seeded the following spring</li> <li>• Erosion and sediment control measures shall be maintained until vegetation has been established in disturbed areas</li> <li>• Refueling of equipment will be carried out away from natural areas with the largest buffer possible (minimum 30 m) to avoid potential impacts, in the event that an accidental spill occurs</li> <li>• In addition to any specified requirements, additional silt fence should be available on-site, prior to grading operations, to provide a contingency supply in the event of an emergency</li> <li>• All erosion and sediment controls should be monitored regularly and properly maintained, as required. Controls are to be removed only after the soils of the construction area have been stabilized and adequately protected until cover is re-established</li> <li>• The limits of construction adjacent to natural features to be retained will be delineated prior to construction, and monitored during construction (along with sediment and erosion control measures) to ensure the limits are maintained with respect to vehicular traffic and soil or equipment stockpiling</li> <li>• The Contractor is required to restore any disturbed natural areas to pre-construction conditions</li> <li>• Minimize clearing and grubbing</li> </ul>
<b>2.0 Management of Excess Materials</b>			
2.1	Excess materials may be encountered during construction and require proper management and/or disposal.	MECP MNRF	<ul style="list-style-type: none"> <li>• Contract documentation to include OPSS 180 – General Specification for the Management of Excess Material (i.e., disposal, re-use, and stockpiling)</li> <li>• The Contractor should assess and manage the designated substances prior to disturbances, in accordance with SSP 101 F21 – Occupational Health and Safety Act – List of Designated Substances and with Ontario Regulation 490/09 and 278/05 under the <i>Occupational Health and Safety Act</i></li> </ul>
<b>3.0 Surface Water Protection</b>			
3.1	<p>Surface water quality in local receiving waters retains its ecological values.</p> <p>Protection of a designated wellhead protection area and gravel deposits with high permeability. Groundwater may be impacted due to increased infiltration and mobilization of surface contaminants through the gravel formation.</p>	GRCA MNRF	<ul style="list-style-type: none"> <li>• In order to prevent groundwater/surface water contamination during construction, measures will be incorporated into the Contract Package to state that care should be taken to avoid accidental spillage or discharge of chemical contaminants (e.g. gasoline, oils, and lubricants):                         <ul style="list-style-type: none"> <li>◦ Refueling of equipment is to be completed away from natural areas, whenever possible, to minimize potential impacts to groundwater quality in the event that an accidental spill occurs</li> <li>◦ To minimize the impact of potential contaminant spills, the Contractor shall implement best management protocols such as secondary containment of temporary fuel storage, and the use of a spill response plan</li> <li>◦ For non-mobile equipment, refueling should be carried out in a controlled manner so as to prevent fuel spillage, and drip pans should be located under the equipment at all times</li> </ul> </li> </ul>

ID	Issues/Concerns/ Potential Effects	Concerned Agencies	Mitigation/Protection/Monitoring
<b>4.0 Terrestrial Ecosystems</b>			
4.1	Vegetation protection and restoration.	MNRF REGION CITY	<ul style="list-style-type: none"> <li>Tree clearing work will be completed during the construction timing window of September 1 through March 31</li> <li>If construction activities have the potential to damage or destroy existing nests, the nest should be removed prior to commencement of the activity and prior to April 1. No nests shall be disturbed between April 1 – August 31</li> <li>In the event that nesting is initiated on the structure during construction, activity must stop until the nest is no longer active to avoid contravening the <i>Migratory Birds Convention Act</i> (MBCA)</li> </ul>
4.2	Potential for species-at-risk habitat within or adjacent to the Study Area.	MNRF REGION CITY	<ul style="list-style-type: none"> <li>The Contractor will ensure that all SAR sighted or encountered within the project limits are protected and avoided if under immediate threat from construction activities. Observations must be reported to the MNRF Guelph District within 48 hours of the observation.</li> <li>Operational Constraint – Protection of Species at Risk</li> <li>The Contractor should conduct a thorough visual search of the work areas and construction equipment and machinery at all culvert sites before work commences each day to avoid interaction with reptiles. In the event reptiles are encountered during construction, work at that location will stop until the reptiles are no longer present</li> <li>Reptile exclusion fencing will be installed before any construction activity is initiated to reduce the likelihood of turtles or snakes entering the work area. Fencing materials with plastic mesh will not be used due to the risk of entanglement of reptiles. Installation will occur before June 1 or after September 1 to define Work Zones and restrict the movement of reptiles into the working area.</li> </ul>
<b>5.0 Fish and Fish Habitat</b>			
5.1	Habitat protection and restoration. Permanent impacts resulting from the proposed replacement are expected to be minimal.	MNRF REGION CITY	<ul style="list-style-type: none"> <li>Construction activities that may impact fish or fish habitats will not occur outside of the MNRF approved in-water timing window. No work allowed from March 15-May 15, inclusive</li> </ul>
5.2	Use, storage, or handling of equipment near the watercourses may result in the release of fuel, lubricants, paint, and other deleterious substances that could impair water quality.	MTO MNRF DFO MECP	<ul style="list-style-type: none"> <li>All equipment used for the work in and around waterbodies or on waterbody banks shall at all times be free of excess or leaking fuel, lubricants, coolant, and any other deleterious substances that could enter the waterbody</li> <li>Equipment refueling, and maintenance shall take place at locations as far away as practicable from a waterbody and in a manner that prevents sediment and other deleterious substances from entering into a waterbody</li> <li>OPSS 182 – Environmental Protection for Construction in Waterbodies and on Waterbody Banks</li> </ul>
5.3	Uncontrolled runoff during construction could result in contamination of surface water and groundwater resources in the Study Area through construction and facility maintenance.	MTO MNRF DFO MECP	<ul style="list-style-type: none"> <li>Surface water and groundwater protection will include the application of proper fuel or other containment spills management during construction, proper sediment control, and proper facility management during operation and maintenance to avoid potential impacts to surface water and groundwater</li> <li>Exposed soil areas within 200 m of the watercourse will be stabilized and revegetated, through the placement of seed and bonded fibre matrix within 15 days of soil exposure (45 days in all other areas) but no later than October 30</li> </ul>
<b>6.0 Air Quality/Noise During Construction</b>			

ID	Issues/Concerns/ Potential Effects	Concerned Agencies	Mitigation/Protection/Monitoring
6.1	<p>Dust emissions from construction operations.</p> <p>Construction noise impacts are temporary in nature and may be noticeable at times at residential Noise Sensitive Areas (NSAs).</p>	<p>MTO MECP CITY REGION BUSINESSES</p>	<ul style="list-style-type: none"> <li>• General Conditions in the contract, include: minimizing operation and idling of gas-powered equipment vehicles, minimizing vehicular traffic on exposed soils and stabilizing high traffic areas with suitable cover material, restoring disturbed areas as soon as possible to minimize the duration of soil exposure, and controlling dust emissions by the application of MECP approved dust suppressant materials</li> <li>• Maintain equipment, particularly mufflers in good working order</li> <li>• Enforce construction codes of practice and local municipal by-law for noise to provide means of limiting excessively noisy operations and equipment</li> <li>• A Noise By-law exemption was requested from the City of Cambridge to accommodate night time work</li> <li>• Complaints will be dealt with in accordance with the MTO Noise Guide</li> </ul>
<b>7.0 Archaeological Resources</b>			
7.1	<p>Limited potential for previously unknown/deeply buried artifacts/remains to be uncovered during construction.</p>	<p>MTCS CITY REGION MTO</p>	<ul style="list-style-type: none"> <li>• MTCS archaeological clearance provided for the Approved Plan</li> <li>• Any new property requirements to be assessed for archaeological potential</li> <li>• Should other archaeological material be found during construction activities, all activity in the vicinity will be suspended and the MTCS archaeologist shall be contacted</li> <li>• In the event that any human remains are encountered during construction, the proponent must immediately suspend work in the vicinity and contact the MTO, OPP, or local police, the MTCS and the Registrar of Cemeteries</li> </ul>
<b>8.0 Notifications</b>			
8.1	<p>Potential for temporary change to emergency access.</p> <p>Potential delays and disruptions during construction.</p>	<p>MTO OPP EMS PROP/BUS REGION CITY PUS</p>	<ul style="list-style-type: none"> <li>• Notify OPP and emergency services of construction staging, start of construction, etc. to minimize delay in emergency response times during and after construction</li> <li>• Access to business entrances within the construction area will be maintained at all times and additional signage will indicate that businesses at the SmartCentres property are open during construction</li> <li>• The project website highway401-hespeler-townline.ca will provide a link to the Ontario 511 - Traveller Information Services website which gives easy 24/7/365 access to MTO road information on provincially maintained highways</li> <li>• Contractor to monitor traffic conditions to make sure unreasonable delays and back-ups are not occurring</li> <li>• Provision of an emergency contact person available 24/7 to assist with emergencies</li> <li>• During full highway closures, ensure additional police officers are available to assist with traffic movements at key intersections</li> <li>• Use of PVMS to notify the public of upcoming closures</li> <li>• The City of Cambridge, Grand River Transit, local student transportation services; local industrial/commercial operations and emergency response agencies (EMS) will be provided advance notice of the construction schedule</li> <li>• The Contractor shall invite all local EMS representatives from RMW to monthly meetings to confirm any and all work operations that may impact the temporary EMS ramps at Hespeler Road</li> <li>• Advanced notice will be provided to transportation companies, giving the location and dates that a detour/construction zone will be in effect</li> </ul>

## 5.0 Environmental Monitoring

### 5.1 PROJECT SPECIFIC TECHNICAL MONITORING

An Environmental Synopsis will be developed to ensure that the Contract Administrator and the Contractor are made aware of, and are prepared to deal with, environmental issues that may arise during construction. Specific environmental controls based on these detailed mitigation measures will be included in the contract documents and drawings to address specific environmental concerns during the construction phase.

Monitoring will be conducted by on-site construction supervisory staff to make sure that environmental protection measures, as outlined in this report and in the contract package, are being implemented and are effective. During construction, environmental protection activities outlined in the Contract will be implemented and integrated with the rest of the construction process. Monitoring will be conducted by the Contractor to make sure that environmental protection measures, as outlined in this report and in the contract package, are being implemented and are effective. This includes making sure that the implementation of mitigating measures and key design features is consistent with commitments made to external agencies prior to construction.

In the event that protective measures do not address concerns identified or if major problems develop, the appropriate agency will be contacted to provide additional input.

In the event that the impacts of construction are different than anticipated, or that the method of construction is such that there are greater than anticipated impacts, the Contractor's method of operation will be modified to reduce those impacts.

### 5.2 POST CONSTRUCTION

Following the completion of the project, it is anticipated that monitoring will be conducted by maintenance staff to ensure erosion control measures and stabilization measures for new or modified earthworks are effective. In the event that problems are identified, remedial action will be initiated as appropriate.

The post-construction monitoring and environmental inspection program will ensure, to the extent possible, that lands disturbed as a result of construction activities will be restored to their original use and condition as soon as possible after construction.

# Notification Materials





Stantec Consulting Ltd.  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

May 29, 2014  
File: 165000897

**Attention:** «First\_Name» «Last\_Name», «Position»  
«Organization»  
«Address\_1»  
«Address\_2»  
«City» «Prov» «PostC»

Dear «Title» «Last\_Name»:

**EXTERNAL AGENCY LETTER**

**Reference: NOTICE OF STUDY COMMENCEMENT  
Highway 401 Interchange Improvements from Hespeler Road to Townline  
Road, City of Cambridge (GWP 3060-11-00)**

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake the Detailed Design and Class Environmental Assessment (Class EA) Study for improvements on Highway 401, between Hespeler Road and Townline Road, as outlined in the enclosed *Ontario Government Notice (OGN)*.

*The Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR) which received Environmental Clearance in November 2013.

The purpose of this letter is to inform you that Phase 1 of the Preliminary Design Study is proceeding to Detailed Design and contract preparation. The enclosed Reply Form provides an opportunity to confirm your agency's interest in the project; submit new information and/or identify approvals or permits that may be required.

A Public Information Centre (PIC) will be held in 2015 to present the Recommended Plan for improvements, as well as the proposed construction staging and traffic management plan. A separate External Agency and municipal session will also be scheduled to coincide with the PIC.

We respectfully request that you complete and return the enclosed form by **June 27, 2014** so that your comments may be considered early in the design process. Comments may also be submitted via the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

If you have any questions regarding the project, please contact either the undersigned or one of the individuals named in the enclosed material.

Yours truly,

**STANTEC CONSULTING LTD.**

Donna Madden, CPT  
Senior Environmental Planner  
Tel: (905) 381-3217  
Fax: (905) 385-3534  
[Donna.madden@stantec.com](mailto:Donna.madden@stantec.com)

Encl: OGN; Reply Form

- c. R. Ward, Senior Project Manager, MTO West Region
- K. Cooper, Environmental Planner, MTO West Region
- A. Barg, Consultant Project Manager, Stantec Consulting Ltd.

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange Improvements from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3060-11-00)**

**REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014**

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**Date:** \_\_\_\_\_

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<b>Contact Information on Study Mailing List</b> «Title» «First_Name» «Last_Name», «Position» «Organization» «Address_1» «Address_2» «City» «Prov» «PostC» Phone: «Telephone» Fax: «Fax»	<i>If this information is incorrect please provide updated information or identify the key project contact below.</i>
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**Please return the completed form to:**

Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

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**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:

---



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

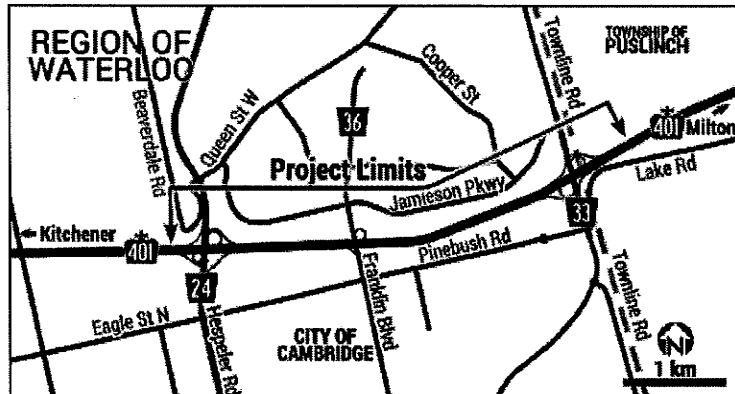


# NOTICE OF STUDY COMMENCEMENT

## Detailed Design and Class Environmental Assessment Study Highway 401 Interchange Improvements from Hespeler Road to Townline Road, City of Cambridge (G.W.P. 3060-11-00)

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to undertake the Detailed Design and Class Environmental Assessment (Class EA) Study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo as shown on the key plan.

Subject to the outcome of the study, the project includes the reconstruction of the Highway 401 interchanges at Hespeler Road and Franklin Boulevard, including replacement of the bridges. To facilitate these improvements, the project will include reconstruction of Highway 401. The project also includes the rehabilitation of the Townline Road bridge, drainage improvements, illumination, overhead signs, traffic signals and retaining walls. A new noise wall is proposed along the south side of Highway 401 just west of Franklin Boulevard. The study will include the provision for pedestrian/cyclist travel across Highway 401 on Franklin Boulevard in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge.



All of the proposed improvements will take into consideration the future 10-lane cross-section of Highway 401. The construction staging/traffic management strategy during the replacement of the Franklin Boulevard bridge will include a temporary full closure of this interchange with no access across Highway 401 during construction.

**PROCESS**

This study is following the approved planning process for Group 'B' projects under the MTO *Class Environmental Assessment for Provincial Transportation Facilities* (2000). The Preliminary Design of the project was included in the Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary study, which concluded with the publication of a Transportation Environmental Study Report (TESR). The TESR received Environmental Assessment (EA) clearance in November 2013.

A Public Information Centre (PIC) will be held in 2015 to present the Recommended Plan for improvements as well as the proposed construction staging and traffic management plan. The date, time and location of the PIC will be advertised in local newspapers.

A Design and Construction Report (DCR) documenting this study will be made available for a 30-day public review period with public notice advising of the start of the review period. The DCR is not subject to a Part II Order ('bump-up') request under the provisions of the *Environmental Assessment Act*.

**COMMENTS**

We are interested in hearing your comments regarding the project. If you would like to have your name added to the mailing list or to provide comments, please contact one of the following individuals:

**Mr. Adam Barg, P.Eng.**  
Consultant Project Manager  
Stantec Consulting Ltd.  
200-835 Paramount Drive  
Stoney Creek, ON L8J 0B4  
tel: 905-381-3216 (+0 collect)  
fax: 905-385-3534  
e-mail: adam.barg@stantec.com

**Roger Ward, LEL**  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3rd Floor  
London, ON N6E 1L3  
tel: 519-873-4547  
toll-free: 1-800-265-6072 ext. 519-873-4547  
fax: 519-873-4600  
e-mail: roger.a.ward@ontario.ca

For up-to-date project information and to submit comments to the Project Team at any time during the study, please visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

S/Sgt. John Mraud Detachment Commander Ontario Provincial Police, Cambridge Detachment 500 Beaverdale Road Box 21060 Cambridge ON N3C 4B1	Ms. Carol Neumann Rural Planner Ministry of Agriculture, Food and Rural Affairs, Environmental and Land Use Policy Elora Resource Centre Unit 10, 6484 Wellington Rd 7 Elora ON N0B 1S0	Mr. Louis Bitonti Senior Planner Ministry of Municipal Affairs and Housing, Community Planning and Development 2nd Floor, 777 Bay Street Toronto ON M5G 2E5
Insp. Scott Lawson Detachment Commander Ontario Provincial Police, Wellington County Detachment 5145 Wellington Rd 27 Rockwood ON N0B 2K0	Mr. David Marriot District Planner Ministry of Natural Resources - Guelph District Office 1 Stone Road West Guelph ON N1G 4Y2	Ms. Barbara Slattery EA/Planning Coordinator Ministry of the Environment, West Central Region 119 King Street West 12th Floor Hamilton ON L8P 4Y7
Mr. Art Timmerman Fish and Wildlife Biologist Ministry of Natural Resources - Guelph District Office 1 Stone Road West Guelph ON N1G 4Y2	Ms. Maya Harris Manager, Growth Planning and Analysis Ontario Growth Secretariat Mowat Block 900 Bay Street, 5th Floor Toronto ON M7A 1C2	Mr. Michael Di Lullo City Clerk The City of Cambridge 50 Dickson Street PO Box 669 Cambridge ON N1R 5W8
Mr. Joseph Muller Heritage Planner Ministry of Tourism, Culture and Sport, Programs and Services Branch 401 Bay Street Suite 1700 Toronto ON M7A 0A7	Mr. George Elliot Commissioner of Transportation and Public Works The City of Cambridge 1310 Bishop Street Cambridge ON N1R 6V5	Ms. Cathy Robertson, P.Eng Director of Engineering Services The City of Cambridge 1310 Bishop Street Cambridge ON N1R 6V5
Mr. George Barnes Director of Operations The City of Cambridge 50 Dickson Street PO Box 669 Cambridge ON N1R 5W8	Mr. Kent McVittie Commissioner of Community Services The City of Cambridge 50 Dickson Street PO Box 669 Cambridge ON N1R 5W8	Mr. Bill Chesney Fire Chief The City of Cambridge - Fire Department 1625 Bishop Street North Cambridge ON N1R 7J4
Ms. Shannon Noonan Manager of Transportation Engineering The City of Cambridge 1310 Bishop Street Cambridge ON N1R 6V5	Ms. Valerie Spring Heritage Planner Cambridge Municipal Heritage Advisory Committee 50 Dickson Street PO Box 669 Cambridge ON N1R 8S1	Ms. Karen Landry CAO/Clerk Township of Puslinch 7404 Wellington Road 34 Guelph ON N1H 6H9
Ms. April Souwand Senior Environmental Planner Cambridge Environmental Advisory Committee 3rd Floor, 50 Dickson Street PO Box 669 Cambridge ON N1R 5W8	R. Gordon Fire Chief Township of Puslinch Fire and Rescue Services 7404 Wellington Road 34 Guelph ON N1H 6H9	Ms. Jessica Rudy Regional Clerk Regional Municipality of Waterloo 150 Frederick Street 1st Floor Kitchener ON N2G 4J3
Mr. Don Creed Director of Public Works and Parks Township of Puslinch 7404 Wellington Road 34 Guelph ON N1H 6H9	Mr. Chris Gosselin Manager, Environmental Planning Regional Municipality of Waterloo 150 Frederick Street 7th Floor Kitchener ON N2G 4J3	Mr. Bob Henderson Manager of Transportation Engineering Regional Municipality of Waterloo 150 Frederick Street 7th Floor Kitchener ON N2G 4J3
Mr. John Cicuttin Director, Transportation (Acting) Regional Municipality of Waterloo 150 Frederick Street Kitchener ON N2G 4J3	Ms. Donna Bryce County Clerk County of Wellington 74 Woolwich Street Guelph ON N1H 3T9	Mr. Paul Johnson Operations Manager County of Wellington, Engineering Services 74 Woolwich Street Guelph ON N1H 3T9
Chief Stephen VanValkenburg Chief of EMS EMS, Regional Municipality of Waterloo 120 Maple Grove Road Cambridge ON N3H 4R6		

Chief Stephen Dewar EMS Chief Guelph-Wellington EMS 160 Clair Road West Guelph ON N1L 1G1	Chief Matthew Torigian Waterloo Regional Police Service 200 Maple Grove Road PO Box 3070 Cambridge ON N3H 5M1	Mr. Blair Allen Supervisor - Transit Development Grand River Transit City Hall - Planning Department, 200 King Street West P.O. Box 1118, 8th Floor Kitchener ON N2G 4G7
Mr. Mike Grivicic Administrative Services Waterloo Region Heritage Foundation 150 Frederick Street Regional Administration Building, 2nd Floor Kitchener ON N2G 4J3	Mr. Fred Natolochny Supervisor of Resource Planning Grand River Conservation Authority 400 Clyde Road PO Box 729 Cambridge ON N1R 5W6	Mr. Greg Durocher President/CEO Cambridge Chamber of Commerce 750 Hespeler Road Cambridge ON N3H 5L8
Mr. Ian McLean President/CEO Greater Kitchener-Waterloo Chamber of Commerce PO Box 2367 80 Queen Street N Kitchener ON N2H 6C4	Mr. John MacTaggart, P.Eng. Senior Engineering Services Officer Canadian National Railway - Regional Engineering/Engineering Services 1st Floor, 1 Administration Road Box 1000 Concord ON L4K 1B9	Mr. Burkhard Mausberg CEO Friends of the Greenbelt Foundation 661 Yonge Street Suite 500 Toronto ON M4Y 1Z9
Ms. Lisa Myslicki Environmental Coordinator Infrastructure Ontario 1 Dundas Street West Suite 2000 Toronto ON M5G 2L5	Mr. Norman Ragetlie President Speed River Land Trust 1 Trent Lane Guelph ON N1Q 2W1	Mr. Beniot Bourgault General Manager Student Transportation Services of Waterloo Region 130 – 4275 King St. East Kitchener ON N2P 2E9
Mr. Chris Baskys Ontario Cyclist Association 3 Concorde Gate Suite 212 Toronto ON M3C 3N7	Ms. Beth Hanna Executive Director Ontario Heritage Trust 10 Adelaide Street East Toronto ON M5C 1J3	Mr. Todd Kramp Construction Coordinator Atria Networks 3100 Steeles Ave., E 10th Floor, Suite 1000 Markham, ON L3R 8T3
Mr. Wayne Corrigan Regional Manager Bell Canada P.O. Box 920 Station A Toronto ON M5W 1G5	Mr. John Sabiston P.Eng. Manager, Transmission Planning Hydro One Networks Inc., Ontario Power Authority 45 Sarjent Drive P.O. Box 6700 Barrie ON L4M 5N5	Mr. Kevin Schimus Mapping Team Lead Union Gas Ltd. 603 Kumpf Drive Waterloo ON N2V 1K3
Cambridge and North Dumfries Hydro 1500 Bishop Street North PO Box 1060 Cambridge ON N1R 5X6		

**From:** Madden, Donna  
**To:**  
**Cc:** [Ward, Roger \(MTO\)](#); [Barg, Adam](#); [Mochrie, Adele \(MTO\) \(Adele.Mochrie@ontario.ca\)](#); "[Coxon, Chad \(MTO\)](#)"  
**Subject:** Project Reference: GWP 3060-11-00, Highway 401 Interchange Improvements from Hespeler Road to Townline Road - Response to Comments  
**Date:** Monday, November 17, 2014 12:41:00 PM

---

Dear

Thank you for your email comments via the Highway 401 Hespeler Road project website.

The Ontario Ministry of Transportation (MTO) completed a Preliminary Design Study for the proposed improvements as part of a larger study that included Highway 401 from 1.0 km west of Hespeler Road easterly to the Wellington County/Halton Region Boundary, 25.8 km. The study was documented in a Transportation Environmental Study Report (TESR) that received environmental clearance in November 2013. The TESR is available for review on the project website.

A noise assessment was undertaken during the Preliminary Design study to assess the potential noise impacts from the Highway 401 improvements. Sound levels were predicted for two situations, one with no highway expansion, and the other assuming the planned improvements were in place. Absolute sound levels greater than 65 dBA were predicted at some adjacent noise sensitive locations. Noise mitigation at these locations was investigated, in accordance with the MTO Environmental Guide for Noise requirements.

In areas where 5 dB of attenuation is achievable with noise barriers, mitigation is considered Technically Feasible. North of Highway 401, there are a number of residences backing onto Jamieson Parkway that include backyard noise barriers. These barriers have the potential to reduce noise impacts from Highway 401. Additional noise mitigation measures were not determined to be technically or economically feasible in this area and are not recommended. South of Highway 401 at Wayne Avenue, mitigation has been found to be Technically and Economically Feasible on the southwest corner of the Franklin Boulevard underpass. A 270 m barrier is recommended in this area.

The project team will review the design further during this detailed design phase. In addition, we will be confirming construction noise mitigation requirements and the need for a municipal Noise By-law exemption if construction work is required outside of normal working hours and/or on weekends.

A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time. You will receive a notice directly as we have added your email address to the project mailing list. I trust this information is helpful. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Sincerely,

**Donna M. Madden, CPT**  
Senior Environmental Planner  
Stantec  
200 - 835 Paramount Drive Stoney Creek ON L8J 0B4  
Phone: 905-381-3217  
Cell: 289-237-9268  
Fax: 905-385-3534  
[donna.madden@stantec.com](mailto:donna.madden@stantec.com)

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**From:** Madden, Donna  
**To:**  
**Cc:** [Ward, Roger \(MTO\)](#); [Barg, Adam](#); [Mochrie, Adele \(MTO\) \(Adele.Mochrie@ontario.ca\)](#); "[Coxon, Chad \(MTO\)](#)"  
**Subject:** Project Reference: GWP 3060-11-00, Highway 401 Interchange Improvements from Hespeler Road to Townline Road - Response to Comments  
**Date:** Monday, November 17, 2014 12:40:00 PM

---

Dear

Thank you for your email as a result of the notice of study commencement for the Highway 401 Hespeler Road project.

The Ontario Ministry of Transportation (MTO) completed a Preliminary Design Study for the proposed improvements as part of a larger study that included Highway 401 from 1.0 km west of Hespeler Road easterly to the Wellington County/Halton Region Boundary, 25.8 km. The study was documented in a Transportation Environmental Study Report (TESR) that received environmental clearance in November 2013. The TESR is available for review on the project website noted below.

A noise assessment was undertaken during the Preliminary Design study to assess the potential noise impacts from the Highway 401 improvements. Sound levels were predicted for two situations, one with no highway expansion, and the other assuming the planned improvements were in place. Absolute sound levels greater than 65 dBA were predicted at some adjacent noise sensitive locations. Noise mitigation at these locations was investigated, in accordance with the MTO Environmental Guide for Noise requirements.

In areas where 5 dB of attenuation is achievable with noise barriers, mitigation is considered Technically Feasible. North of Highway 401, there are a number of residences backing onto Jamieson Parkway that include backyard noise barriers. These barriers have the potential to reduce noise impacts from Highway 401. Additional noise mitigation measures were not determined to be technically or economically feasible in this area and are not recommended. South of Highway 401 at Wayne Avenue, mitigation has been found to be Technically and Economically Feasible on the southwest corner of the Franklin Boulevard underpass. A 270 m barrier is recommended in this area.

The project team will review the design further during this detailed design phase. In addition, we will be confirming construction noise mitigation requirements and the need for a municipal Noise By-law exemption if construction work is required outside of normal working hours and/or on weekends. Visual screening with coniferous and deciduous tree plantings along the corridor will be considered in the final design and incorporated into the landscape composition plan, where appropriate. A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time. You will be notified directly at study milestones.

Thank you again for your interest. I trust this information is helpful. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Sincerely,

**Donna M. Madden, CPT**

Senior Environmental Planner

Stantec

200 - 835 Paramount Drive Stoney Creek ON L8J 0B4

Phone: 905-381-3217

Cell: 289-237-9268

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[donna.madden@stantec.com](mailto:donna.madden@stantec.com)

**From:** Madden, Donna  
**To:**  
**Cc:** [Ward, Roger \(MTO\)](#); [Barg, Adam](#); [Mochrie, Adele \(MTO\) \(Adele.Mochrie@ontario.ca\)](#); "[Coxon, Chad \(MTO\)](#)"  
**Subject:** Project Reference: GWP 3060-11-00, Highway 401 Interchange Improvements from Hespeler Road to Townline Road - Response to Comments  
**Date:** Monday, November 17, 2014 12:42:00 PM

---

ATT:

Dear

Thank you for your email as a result of the Notice of Study Commencement for the above-noted project. The Ontario Ministry of Transportation (MTO) completed a Preliminary Design Study for the proposed improvements as part of a larger study that included Highway 401 from 1.0 km west of Hespeler Road easterly to the Wellington County/Halton Region Boundary, 25.8 km. The study was documented in a Transportation Environmental Study Report (TESR) that received environmental clearance in November 2013. The TESR is available for review on the project website.

A noise assessment was undertaken during the Preliminary Design study to assess the potential noise impacts from the Highway 401 improvements. Sound levels were predicted for two situations, one with no highway expansion, and the other assuming the planned improvements were in place. Absolute sound levels greater than 65 dBA were predicted at some adjacent noise sensitive locations. Noise mitigation at these locations was investigated, in accordance with the MTO Environmental Guide for Noise requirements. In areas where 5 dB of attenuation is achievable with noise barriers, mitigation is considered Technically Feasible. North of Highway 401, there are a number of residences backing onto Jamieson Parkway that include backyard noise barriers. These barriers have the potential to reduce noise impacts from Highway 401. Additional noise mitigation measures were not determined to be technically or economically feasible in this area and are not recommended. South of Highway 401 at Wayne Avenue, mitigation has been found to be Technically and Economically Feasible on the southwest corner of the Franklin Boulevard underpass. A 270 m barrier is recommended in this area.

The project team will review the design further during this detailed design phase. In addition, we will be confirming construction noise mitigation requirements and the need for a municipal Noise By-law exemption if construction work is required outside of normal working hours and/or on weekends. The study will include the provision for appropriate pedestrian/cyclist travel across Highway 401 on Franklin Boulevard in accordance with the active transportation goals of MTO, the Region of Waterloo and the City of Cambridge. The Franklin Boulevard structure will be reconstructed to accommodate the widened highway. The provision for a full interchange at Franklin Boulevard was reviewed during preliminary design, however the alternative was not carried forward due to the major property impacts and the proximity of the Hespeler Road and Townline Road interchanges. Franklin Boulevard will remain as a partial interchange.

A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time. Your contact information on the project mailing list has been revised as requested and you will be contacted directly in advance of study milestones. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Thank you again for your interest. I trust this information is helpful.

Sincerely,

**Donna M. Madden, CPT**  
Senior Environmental Planner

Stantec  
200 - 835 Paramount Drive Stoney Creek ON L8J 0B4  
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**Stantec**

**Stantec Consulting Ltd.**

200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

November 17, 2014

File: 165000897

**Attention: Mr. Michael McGann, Manager**  
Design and Approvals (Engineering Services)  
City of Cambridge  
50 Dickson Street, 3<sup>rd</sup> Floor  
PO Box 669  
Cambridge ON N1R 5W8

Dear Mr. McGann,

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

Thank you for submitting comments in response to the Notice of Study Commencement for the Highway 401 Interchange Improvements project.

Stantec confirms that this project is the initial phase of the Preliminary Design Study (G.W.P. 8-00-00) documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in November 2013. The TESR is available for review on the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

The project team is completing field investigations to confirm and update the findings of the Preliminary Design study. It is anticipated that the preferred Highway 401 widening alternative, to widen outward to an ultimate ten-lane cross-section along with the bridge replacements and interchange reconstruction will result in impacts to municipal infrastructure. Impacts to existing utilities and municipal infrastructure will be confirmed by the engineering team during detailed design. Alternatives to eliminate or minimize conflicts will be considered. The widening phase of this project has not been scheduled or funded at this time.

The Project Team will maintain contact with the City of Cambridge throughout the study process. As the design progresses a meeting with the City will be held to discuss the design details and to coordinate relocations as necessary.

A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time.



November 17, 2014  
Mr. Michael McGann, Manager  
Page 2 of 2

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

I trust this information is helpful. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Regards,

**STANTEC CONSULTING LTD.**

*D. M. Madden*

Donna M. Madden, CPT  
Senior Environmental Planner  
Phone: 905-381-3217  
Fax: 905-385-3534  
donna.madden@stantec.com

c. R. Ward, Senior Project Manager, MTO West Region  
A. Mochrie, Senior Environmental Planner, MTO West Region  
A. Barg, Project Manager, Stantec Consulting Ltd.



**Stantec Consulting Ltd.**  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

November 17, 2014  
File: 165000897  
GRCA File: W.86.358

**Attention: Mr. John Brum, Resource Planner**  
Grand River Conservation Authority  
400 Clyde Road, PO Box 729  
Cambridge ON N1R 5W6

Dear Mr. Brum,

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

Thank you for your July 3, 2014 letter in response to the Notice of Study Commencement for the Highway 401 Interchange Improvements project. We understand that the Grand River Conservation Authority's (GRCA) primary interests relate to natural heritage, natural hazards and property impacts.

This project is the initial phase of the *Preliminary Design Study – Highway 401 from west of Hespeler Road Easterly to the Wellington County/Halton Region Boundary (MTO Project G.W.P. 8-00-00)* that was documented in a Transportation Environmental Study Report (TESR). The TESR is available for review on the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

The project team is completing environmental field investigations to confirm and update the findings of the Preliminary Design study. The mitigation measures outlined in the TESR will be refined as the Detailed Design progresses.

The following information is provided in response to your comments:

1. There are no anticipated direct impacts to the Provincially Significant Puslinch Lake/Irish Creek Wetland Complex and other wetland areas adjacent to the study area.
2. A permit pursuant to Ontario Regulation 156/06 from the GRCA is not required for provincial infrastructure subject to the activity being approved through a satisfactory Environmental Assessment process (i.e., *Ministry of Transportation Class Environmental Assessment for Provincial Transportation Facilities, 2000*).
3. The GRCA property east of Townline Road, south of Highway 401 will not be affected by the preferred Highway 401 widening alternative or the rehabilitation of the Townline Road bridge.
4. No wildlife species of concern were recorded during previous field surveys, with the exception of the Monarch butterfly (i.e., due to presence of Common Milkweed).



November 17, 2014  
Mr. John Brum, Resource Planner  
Page 2 of 2

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

5. Species of conservation concern that has been designated federally or provincially has been reviewed since the completion of Preliminary Design. No new designations have been identified.
6. None of the water crossings in this study area support direct fish use. One culvert (C4) appears to convey roadside drainage to a large pond on the north side of Highway 401 which was identified to support direct fish use.

The project includes hydrology and drainage design of surface and subsurface drainage systems including erosion control measures within the project limits. Stantec will address modifications to the highway drainage system including replacement of culverts, storm sewers, and provisions of features such as grass swales for stormwater management. This will involve consultation with the Ministry of the Environment and Climate Change, Ministry of Natural Resources and Forestry and the GRCA.

We will continue to consult with GRCA through the EA process. A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time.

Thank you for taking time to respond to our request for comments. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Regards,

**STANTEC CONSULTING LTD.**

*Donna M. Madden*

Donna M. Madden, CPT  
Senior Environmental Planner  
Phone: 905-381-3217  
Fax: 905-385-3534  
donna.madden@stantec.com

- c. R. Ward, Senior Project Manager, MTO West Region
- A. Mochrie, Senior Environmental Planner, MTO West Region
- A. Barg, Project Manager, Stantec Consulting Ltd.
- J. Innes, Surface Water Engineer, Stantec Consulting Ltd.



**Stantec Consulting Ltd.**  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

November 17, 2014  
File: 165000897

**Attention: Ms. Anne Marie Laurence, A/District Planner**  
Ministry of Natural Resources and Forestry  
Guelph District  
1 Stone Road West  
Guelph ON N1G 4Y2

Dear Ms. Laurence,

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

Thank you for your July 11, 2014 letter outlining the Ministry of Natural Resources and Forestry (MNR) primary interests relating to natural heritage features within this study area.

This project is the initial phase of the *Preliminary Design Study – Highway 401 from west of Hespeler Road Easterly to the Wellington County/Halton Region Boundary (MTO Project G.W.P. 8-00-00)* that was documented in a Transportation Environmental Study Report (TESR). The TESR is available for review on the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

The project team is completing environmental field investigations to confirm and update the findings of the Preliminary Design study. The mitigation measures outlined in the TESR will be refined as the Detailed Design progresses. The following information is provided in response to your comments regarding Natural Heritage features within the study area:

#### **Wetlands**

The Provincially Significant Puslinch Lake/Irish Creek Wetland Complex lies east of the study limits at Townline Road. Direct impacts to the PSW as a result of the proposed improvements are not anticipated. Indirect impacts will be identified and addressed through avoidance and/or mitigation during this detailed design phase.

#### **Species at Risk**

##### Habitat Inventory

Baseline conditions of the *Terrestrial Ecosystems Impact Assessment (Ecoplans 2013)* report prepared during the previous study were reviewed during Detailed Design field investigations conducted on July 9 - 10, 2014. Vegetation and wildlife habitat assessments were conducted between Hespeler Road and Townline Road interchanges. All ELC communities were confirmed



November 17, 2014  
Ms. Anne Marie Laurence, A/District Planner  
Page 2 of 2

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP 3060-11-00)**

and additional flora/fauna findings were identified. These will be considered as the detailed design progresses.

Potential Species at Risk Within the Study Area/Species at Risk Surveys

The July 2014 surveys were undertaken to verify ELC communities identified during the Preliminary Design phase; as well as to identify migratory bird nests, reptile habitat, and any additional Species at Risk (SAR) occurring within the study area. No wildlife species of concern were recorded during previous field surveys, with the exception of the Monarch butterfly (i.e., due to presence of Common Milkweed). Species of conservation concern that has been designated federally or provincially has been reviewed since the completion of Preliminary Design. No new designations have been identified.

Once the scope of the improvements has been confirmed, appropriate protection/mitigation measures for potential Species at Risk will be developed in consultation with the MNRF during this detailed design phase. The widening phase of this project has not been scheduled or funded at this time.

We will continue to consult with MNRF through the EA process. A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time.

Thank you for taking time to respond to our request for comments. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Regards,

**STANTEC CONSULTING LTD.**

*Donna M. Madden*

Donna M. Madden, CPT  
Senior Environmental Planner  
Phone: 905-381-3217  
Fax: 905-385-3534  
donna.madden@stantec.com

- c. R. Ward, Senior Project Manager, MTO West Region
- A. Mochrie, Senior Environmental Planner, MTO West Region
- A. Barg, Project Manager, Stantec Consulting Ltd.



**Stantec Consulting Ltd.**  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

November 17, 2014  
File: 165000897  
MTCS File # 0001519

**Attention: Mr. Joseph Muller, Heritage Planner**  
Ministry of Tourism, Culture and Sport  
Culture Services Unit, Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON L8J 0B4  
E: joseph.muller@ontario.ca

Dear Mr. Muller,

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP. 3060-11-00)**

Thank you for your June 27, 2014 letter in response to the Notice of Study Commencement for the Highway 401 Interchange Improvements project. Stantec confirms that this project is the initial phase of the Preliminary Design Study (G.W.P. 8-00-00) documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in November 2013. The TESR is available for review on the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

The Stage 1 archaeological assessment concluded that the Highway 401 study area (i.e., from west of Hespeler Road to the Wellington County/Halton Regional Boundary) is disturbed. A copy of the original MTC acceptance letter, dated June 2011 is attached. Stantec will undertake Stage 2 investigations during this phase of the project for any new property required for development outside the boundaries of the existing corridor. The Stage 2 report will be submitted to MTCS during this detailed design study.

The cultural heritage resource assessment carried out during preliminary design identified the following resources that may be indirectly affected by this project:

- A farmhouse and barn complex located at 2089 Townline Road, Lot 1 Concession 2, Township of Puslinch, Wellington County
- The Highway 401 corridor throughout the study area; and
- Waterloo Regional Road 24

The TESR did not recommend site specific mitigation measures because it is not anticipated that the road improvements will adversely affect these cultural heritage resources. This will be confirmed during detailed design.

A Cultural Heritage Evaluation was undertaken in 2010 for Waterloo Regional Road #24 (Hespeler Road) Underpass S.B.L., Site No. 33-150-S and Waterloo Regional Road #36 (Franklin Boulevard)



November 17, 2014  
Mr. Joseph Muller, Heritage Planner  
Page 2 of 2

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP. 3060-11-00)**

Underpass, Site No. 33-151. The evaluations concluded that the structures are not worthy of consideration for listing on the Ontario Heritage Bridge List. The Cultural Heritage Evaluation Report (CHER) serves as the documentation record of their evaluation. The MTO Heritage Bridge Committee Forms are attached for your reference.

I trust this information is helpful. We will continue to circulate MTCS on this project through the EA process. A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time.

Thank you for taking time to respond to our request for comments. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Regards,

**STANTEC CONSULTING LTD.**

*Donna M. Madden*

Donna M. Madden, CPT  
Senior Environmental Planner  
Phone: 905-381-3217  
Fax: 905-385-3534  
donna.madden@stantec.com

Attachment: MTCS Acceptance Letter  
MTO Heritage Bridge Committee Forms

- c. R. Ward, Senior Project Manager, MTO West Region
- A. Mochrie, Senior Environmental Planner, MTO West Region
- A. Barg, Project Manager, Stantec Consulting Ltd.

Ministry of Tourism and Culture  
Culture Programs Unit  
Programs & Services Br.  
900 Highbury Avenue  
London, ON N5Y 1A4  
Tel: 519-675-6898  
Fax: 519-675-7777  
e-mail: [shari.prowse@ontario.ca](mailto:shari.prowse@ontario.ca)

Ministre du Tourisme et de la Culture  
Unité des programmes culturels  
Direction des programmes et des services  
900, av. Highbury  
London, ON N5Y 1A4  
Tél: 519-675-6898  
Télé: 519-675-7777  
e-mail: [shari.prowse@ontario.ca](mailto:shari.prowse@ontario.ca)



October 3, 2011

Mr. Philip Woodley  
New Directions Archaeology Ltd.  
55 Valmont Street  
Ancaster, Ontario, L9G 4Z4

**RE: Review and Acceptance into the Provincial Register of Reports: Archaeological Assessment Report Entitled, "Stage 1 Archaeological Assessment of Highway 401, From West of Hespeler Road to the Halton Region Boundary" January 2011, Received March 30, 2011, Licence/PIF # P018-272-2009, G.W.P 8-00-00, MTC File 401H114**

Dear Mr. Woodley:

This office has reviewed the above-mentioned report which has been submitted to this Ministry as a condition of licensing in accordance with Part VI of the Ontario Heritage Act, R.S.O. 1990, c 0.18. This review is to ensure that the licensed professional consultant archaeologist has met the terms and conditions of their archaeological licence, that archaeological sites have been identified and documented according to the 1993 technical guidelines set by the Ministry and that the archaeological fieldwork and report recommendations ensure the conservation, protection and preservation of the cultural heritage of Ontario.\*

As the result of our review, this Ministry will accept the above titled report into the Ontario Public Register of Archaeological Reports. The report indicates that portions of the subject property as detailed in Figures 5-8 have archaeological potential and, consequently, should be subject to a Stage 2 archaeological assessment. This Ministry concurs with this recommendation.

I trust this information is of assistance. Should you require any further information regarding this matter, please feel free to contact me.

Sincerely,

A handwritten signature in black ink, appearing to read "Shari Prowse".

Shari Prowse  
Archaeology Review Office

cc. Archaeology Licence Office  
Ecoplans Ltd.

\* In no way will the Ministry be liable for any harm, damages, costs, expenses, losses, claims or actions that may result: (a) if the Report(s) or its recommendations are discovered to be inaccurate, incomplete, misleading or fraudulent; or (b) from the issuance of this letter. Further measures may need to be taken in the event that additional artifacts or archaeological sites are identified or the Report(s) is otherwise found to be inaccurate, incomplete, misleading or fraudulent.



MTO Heritage Bridge Committee  
Decision Form

<b>Structure Name: Waterloo RR #24 (SBL) Underpass</b>	
<b>Structure ID: 33-150S</b>	<b>Highway: 401</b>
<b>Structure Type: Continuous Deck Slab (Rectangular Voids)</b>	<b>Year of Construction: 1960</b>
<b>Municipality: Puslinch, Wellington County</b>	<b>CHER Date: Feb 2010</b>
<b>CHER Author: Unterman McPhail</b>	
<b>CHER Recommendation:</b> Is not an Ontario Heritage Bridge List candidate	
<b>The Heritage Bridge Committee (HBC) decided that this structure:</b> Is not an Ontario Heritage Bridge List candidate	
<b>Rationale for Decision:</b>	
<b>(1) Design/Physical Value:</b>	
<b>(2) Contextual Value:</b>	
<b>(3) Historic/ Associative Value:</b>	
<b>Final Statements:</b> The HBC is in general agreement with the evaluation and the conclusion that the bridge does not have Provincial Heritage Significance.	



MTO Heritage Bridge Committee  
Decision Form

**HBC Scoring Matrix**

Category	Criterion	Maximum Score	CHER Score
Design / Physical Value	Functional Design	20	20
	Visual Appeal	20	4
	Materials	10	0
Contextual Value	Landmark	15	9
	Character Contribution	10	6
Historical/ Associative Value	Designer / Construction Firm	15	9
	Association with a historical theme, person or event	10	6
<b>Total Score</b>		<b>100</b>	<b>54</b>

**Heritage Bridge Committee Members:**

Chair  
Dino Bagnariol, P.Eng.  
Head Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
St. Catharines ON  
(905) 704-2399

Walter Kenedi, P.Eng.  
Sr. Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
St. Catharines ON  
(905) 704-2399

Leslie Currie  
Regional Archaeologist  
Provincial Highways Management  
Central Region Planning & Environmental  
1201 Wilson Ave. Building D, 3<sup>rd</sup> Floor  
Downsview ON  
(416) 235-5541

Martin Haalstra, P.Eng.  
Sr. Structural Engineer  
Provincial Highways Management  
West Region Engineering  
659 Exeter Rd., 3rd Floor  
London ON  
(519) 873-4348

<b>Structure Name: Waterloo RR #36 Underpass</b>	
<b>Structure ID: 33-151</b>	<b>Highway: 401</b>
<b>Structure Type: Continuous Deck Slab (Rectangular Voids)</b>	<b>Year of Construction: 1960</b>
<b>Municipality: Puslinch, Wellington County</b>	<b>CHER Date: Feb 2010</b>
<b>CHER Author: Unterman McPhail</b>	
<b>CHER Recommendation:</b> Is not an Ontario Heritage Bridge List candidate	
<b>The Heritage Bridge Committee (HBC) decided that this structure:</b> Is not an Ontario Heritage Bridge List candidate	
<b>Rationale for Decision:</b>	
<b>(1) Design/Physical Value:</b>	
<b>(2) Contextual Value:</b>	
<b>(3) Historic/ Associative Value:</b>	
<b>Final Statements:</b> The HBC is in general agreement with the evaluation and the conclusion that the bridge does not have Provincial Heritage Significance.	



MTO Heritage Bridge Committee  
Decision Form

**HBC Scoring Matrix:**

Category	Criterion	Maximum Score	CHER Score
Design / Physical Value	Functional Design	20	16
	Visual Appeal	20	4
	Materials	10	0
Contextual Value	Landmark	15	3
	Character Contribution	10	6
Historical/ Associative Value	Designer / Construction Firm	15	9
	Association with a historical theme, person or event	10	0
<b>Total Score</b>		<b>100</b>	<b>38</b>

**Heritage Bridge Committee Members:**

Chair  
Dino Bagnariol, P.Eng.  
Head Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
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Martin Haalstra, P.Eng.  
Sr. Structural Engineer  
Provincial Highways Management  
West Region Engineering  
659 Exeter Rd., 3rd Floor  
London ON  
(519) 873-4348

DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3060-11-00)

REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014

Date: June 25, 2014

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

Please consult with the Engineering Services Division at the City of Cambridge during the detailed design to confirm if any City-owned infrastructure (ie. storm sewer, sanitary sewer, watermain) will be impacted by the proposed works.

Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Mr. Michael Di Lullo, City Clerk  
The City of Cambridge  
50 Dickson Street  
PO Box 669  
Cambridge ON N1R 5W8  
Tel: (519) 623-1540 ext. 4585

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

**Key Project Contact:** Michael McGann

Job Title: Manager of Design and Approvals (Engineering Services)

Name of Group/Agency: City of Cambridge

Mailing Address: 50 Dickson St. 3rd Floor, P.O. Box 669, Cambridge, ON, N1R 5W8

Tel: 519-621-0740

Fax: 519-740-7729 E-mail: mcgannm@cambridge.ca

ext. 4478



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.



400 Clyde Road, P.O. Box 729, Cambridge, ON N1R 5W6  
Phone: 519-621-2761 Toll free: 866-900-4722 www.grandriver.ca

July 3, 2014

Via Email & Regular Mail

Ms. Donna Madden, CPT  
Senior Environmental Planner  
Stantec Consulting Ltd.  
200-835 Paramount Drive  
Stoney Creek, ON L8J 0B4

Dear Ms. Madden:

**Re: Notice of Study Commencement  
Detailed Design and Class Environmental Assessment Study  
Group 'B' Project (G.W.P. 3060-11-00)  
Proposed Highway 401 Interchange Improvements  
Hespeler Road to Townline Road  
City of Cambridge  
GRCA File: W.86.358**

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The Grand River Conservation Authority (GRCA) has received the Notice of Study Commencement and advises that the project study limits are located within an Area of Interest. As such, we offer the following comments for your review and consideration.

Information currently available at this office indicates that the study limits contain portions of the Provincially Significant Puslinch Lake/Irish Creek Wetland Complex and other wetland areas. Consequently, portions within the project study limits are regulated by the GRCA under Ontario Regulation 150/06 (Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation).

Please be further advised that the GRCA also owns property within close proximity of the study limits near Townline Road (refer to enclosed map). Therefore, any impacts on GRCA property will need to be reviewed and discussed with our Property Division.

We would further recommend that you contact the Guelph District Office of the Ontario Ministry of Natural Resources (MNR) with regards to endangered/rare species and fisheries data.

The GRCA's Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation (Ontario Regulation 150/06), prohibits development in or on the following areas:

- a) adjacent or close to the shoreline of the Great Lakes-St. Lawrence River System or to inland lakes that may be affected by flooding, erosion or dynamic beaches, and within the 15 metre allowance,

- b) within 15 metres of a river or stream valleys that have depressional features associated with a river or stream, whether or not they contain a watercourse,
- c) hazardous lands;
- d) wetlands; or
- e) other areas where development could interfere with the hydrologic function of a wetland, including areas within 120 metres of all provincially significant wetlands and wetlands greater than or equal to 2.0 hectares in size, and areas within 30 metres of wetlands less than 2.0 hectares in size, but not including those where development has been approved pursuant to an application made under the Planning Act or other public planning or regulatory process;

And prohibits alteration to:

- f) straighten, change, divert or interfere in any way with the existing channel of a river, creek, stream or watercourse or change or interfere in any way with a wetland prior to receiving written consent of the GRCA.

Any future development within the regulated areas on the subject property will require the prior issuance of a Permit pursuant to Ontario Regulation 150/06 from the GRCA. The Permit process involves the submission of a Permit Application to this office, the review of the application by Authority staff and the subsequent approval/refusal of the Permit Application by the GRCA.

Based on the foregoing, the GRCA would be interested in receiving additional information on this study and participating in the study process.

We trust you will find the above of assistance on this matter. Should you have any further questions, please do not hesitate to contact the undersigned at ext. 2233.

Yours truly,



John Brum  
Resource Planner  
Grand River Conservation Authority

JB/

Encl.

cc: Roger Ward, MTO (via email)  
Adam Barg, Stantec Consulting Ltd. (via email)  
Samantha Lawson, GRCA (via email)

**Hwy 401-Hesp-Townline Rd**

**LEGEND**

- WATERSHED BOUNDARY (GRCA)
- UTILITY LINE (NRVIS)
- ROADS-ADDRESSED (MNR)
- RAILWAY (NRVIS)
- MUNICIPAL BOUNDARY (GRCA)
- DRAINAGE-NETWORK (GRCA)
- FLOODPLAIN (GRCA)
- ENGINEERED
- APPROXIMATE
- ESTIMATED
- WETLAND (GRCA)
- SLOPE VALLEY (GRCA)
- STEEP
- OVERSTEEP
- SLOPE EROSION (GRCA)
- STEEP
- OVERSTEEP
- TOE
- PROVINCIALY SIGNIFICANT
- LOCALLY SIGNIFICANT
- UNEVALUATED
- PARKS (GRCA)
- PROPERTY-OTHER (GRCA)
- MNR PROPERTY-GRCA MANAGED
- EASEMENT
- PROPERTY (GRCA)
- REGULATION LIMIT 2014 (GRCA)
- DRAINAGE-POLY (NRVIS)

**GRCA Disclaimer**

This map is for illustrative purposes only. Information contained herein is not a substitute for professional review or a site survey and is subject to change without notice. The Grand River Conservation Authority takes no responsibility for, nor guarantees, the accuracy of the information contained on this map. Any interpretations or conclusions drawn from this map are the sole responsibility of the user.

The source for each data layer is shown in parentheses in the map legend. For a complete listing of sources and citations go to:

<http://grims.grandriver.ca/docs/Sources/Citations.L.Mm>



NAD 1983, UTM Zone 17 Scale 1:26,726



THIS MAP IS NOT TO BE USED FOR NAVIGATION

**Hwy 401-Hesp-Townline Rd**

**LEGEND**

- WATERSHED BOUNDARY (GRCA)
- UTILITY LINE (NRVIS)
- ROADS-ADDRESSED (NMR)
- RAILWAY (NRVIS)
- MUNICIPAL BOUNDARY (GRCA)
- DRAINAGE-NETWORK (GRCA)
- FLOODPLAIN (GRCA)
- ENGINEERED
- APPROXIMATE
- ESTIMATED
- WETLAND (GRCA)
- SLOPE VALLEY (GRCA)
- SLOPE EROSION (GRCA)
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- PROVINCIALLY SIGNIFICANT
- LOCALLY SIGNIFICANT
- UNEVALUATED PARKS (GRCA)
- PROPERTY-OTHER (GRCA)
- NMR PROPERTY-GRCA MANAGED
- EASEMENT
- PROPERTY (GRCA)
- REGULATION LIMIT 2014 (GRCA)
- DRAINAGE-POLY (NRVIS)

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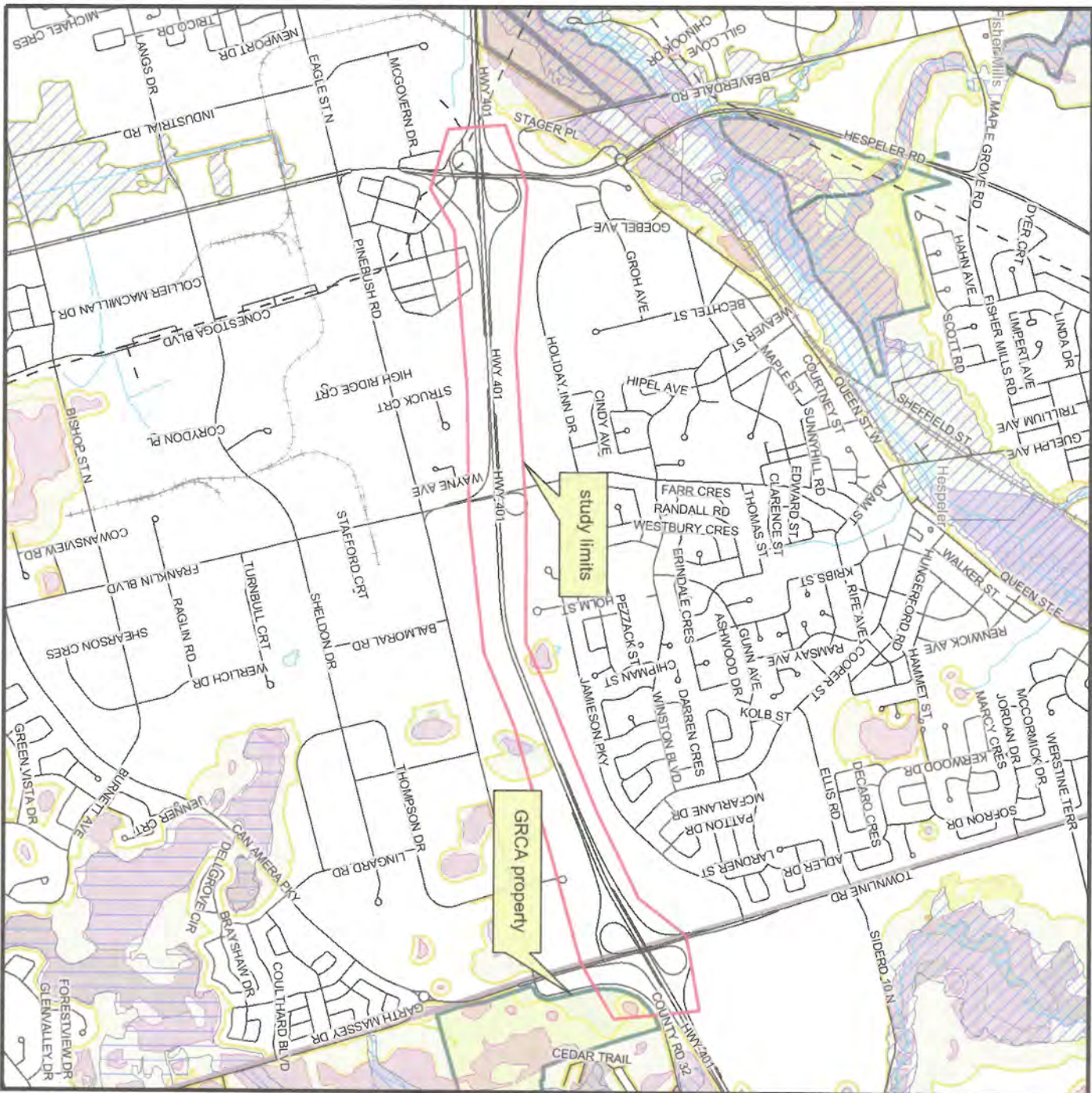
The source for each data layer is shown in parentheses in the map legend. For a complete listing of sources and citations go to:

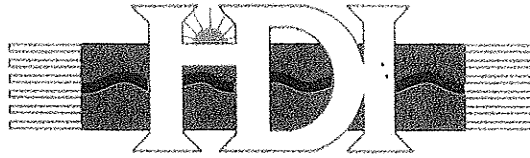
<http://grms.grandriver.ca/docs/Sources/Citations1.htm>

NAD 1983, UTM Zone 17

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THIS MAP IS NOT TO BE USED FOR NAVIGATION





## Haudenosaunee Development Institute

June 11, 2014

Adam Barg, P.Eng  
Consultant Project Manager  
Stantec Consulting Ltd.  
200-835 Paramount Dr.  
Stoney Creek, On  
L8J 0B4

Roger Ward, LEL  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Rd. 3<sup>rd</sup> floor  
London, On  
N6E 1L3

Gentlemen:

**Re: Highway 401 Interchange Improvements from Hespeler Road to  
Townline Rd. City of Cambridge (G.W.P. 3060-11-00) ('Project')  
Our File No.: 030-200-020**

We wish to advise that the Haudenosaunee hold rights and interest in the area contemplated by your Project including treaty rights recognized by the 1701 Treaty of Albany. We wish to advise that the proposed expansion project will have a negative impact upon those rights.

We are hereby requiring the Ministry of Transportation to commence a meaningful engagement process to uphold the honor of the Crown in relation to the proposed Project.

The process that is currently being initiated by the Ministry of Transportation and Stantec further to the *Environmental Assessment Act* is not capable of upholding the honor of the Crown, and in particular, does not provide the Ministry of Transportation with the opportunity to:

- Make an assessment of the rights and interests of the Haudenosaunee;
- Provide the Haudenosaunee with the opportunity to set out and clarify rights and interests such that the Ministry of Transportation can provide a complete and fulsome assessment of the rights and interests;
- Frame the nature and scope of the engagement obligations by way of reference to the assessment of the rights and interests;

OUR LAND, OUR LAW, OUR PEOPLE, OUR FUTURE

- Advise what if any procedural aspects of engagement can be delegated to Stantec.

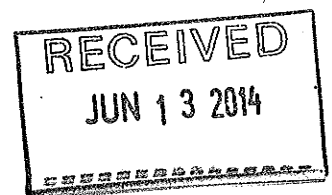
We trust that the Ministry of Transportation will withhold any approval of the Environmental Assessment until such time as the honor of the Crown and our treaty relationship is considered and fulfilled.

Yours truly,



Hazel E. Hill  
Director

cc. Hohahes Leroy Hill, Secretary, Haudenosaunee Confederacy Chiefs Council  
The Honourable Glen Murray, Minister of Transportation



**Ministry of Tourism,  
Culture and Sport**

Culture Services Unit  
Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON M7A 0A7  
Tel: 416 314 7145  
Fax: 416 212 1802

**Ministère du Tourisme,  
de la Culture et du Sport**

Unité des services culturels  
Direction des programmes et des services  
401, rue Bay, Bureau 1700  
Toronto ON M7A 0A7  
Tél: 416 314 7145  
Télééc: 416 212 1802



June 27, 2014 (EMAIL ONLY)

Donna Madden  
Stantec Consulting Ltd.  
200-835 Paramount Drive  
Stoney Creek, ON L8J 0B4  
E: Donna.Madden@stantec.com

**MTCS file #: 0001519**  
**Proponent: Ministry of Transportation**  
**Subject: Notice of Commencement, Highway 401 Interchange Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3060-11-00)**  
**Location: City of Cambridge, Ontario**

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Dear Donna Madden:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the Notice of Commencement for your project. MTCS's interest in this EA project relates to its mandate of protecting, conserving and preserving Ontario's culture heritage, which includes:

- Archaeological resources, including land-based and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources. Please advise MTCS whether an archaeological assessment and/or a heritage impact assessment will be completed for your EA project, and provide them to MTCS before issuing a Notice of Completion.

Please note that the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#) (S&G), prepared pursuant to Section 25.2 of the *Ontario Heritage Act (OHA)*, came into effect on July 1, 2010. All Ontario government ministries and public bodies that are prescribed under Ontario Regulation 157/10 must comply with the S&Gs. They apply to property that is owned or controlled by the Crown in right of Ontario or by a prescribed public body.

Aboriginal communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Aboriginal communities includes a discussion about known or potential cultural heritage resources that are of value to these communities.

I understand that this project may in whole or in part fall within the scope of the Preliminary Design Study GWP 8-00-00. Please confirm if this is the case, and if so how recommendations made in the TESR were followed up for this Detail Design component of the project.

**Archaeological Resources**

Your EA project may impact archaeological resources and you may screen the project with the MTCS [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MTCS archaeological sites data are available at [archaeologicalsites@ontario.ca](mailto:archaeologicalsites@ontario.ca). If your EA project area exhibits archaeological potential, then an archaeological assessment (AA) by an *Ontario Heritage Act*

(OHA) licensed archaeologist, who is responsible for submitting the report directly to MTCS for review, is recommended.

### **Built Heritage and Cultural Heritage Landscapes**

The attached MTCS checklist *Screening for Impacts to Built Heritage and Cultural Heritage Landscapes* helps determine whether your EA project may impact cultural heritage resources. The clerks for the City of Cambridge and Region of Waterloo can provide information on property registered or designated under the *Ontario Heritage Act*.

The draft [MTO Ontario Heritage Bridge Guidelines for Provincially Owned Bridges](#) screening criteria have also been established for cultural heritage evaluation of bridges under the Class EA for Provincial Transportation Facilities.

A Cultural Heritage Evaluation Report is used to determine the cultural heritage value or interest of a potential Provincial Heritage Property. If your EA project will impact heritage resources, MTCS recommends that a Heritage Impact Assessment (HIA) be prepared by a qualified consultant. Our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. Please send HIAs to MTCS for review, and make it available to local organizations or individuals who have expressed their interest in heritage.

### **Environmental Assessment Reporting**

HIA and AA reports and their recommendations are to be addressed and incorporated into EA projects. If your screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file. MTCS is in no way liable if the information in the completed checklists is found to be inaccurate or incomplete.

Thank-you for circulating MTCS on this project: please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Joseph Muller, RPP/MCIP  
Heritage Planner  
Joseph.Muller@Ontario.ca

Copied to: Adam Barg, Consultant Project Manager, Stantec Consulting Ltd.  
Roger Ward, Senior Project Manager, Ministry of Transportation

Please notify MTCS if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out a determination of their nature and significance.

If human remains are encountered, all activities must cease immediately and the local police be contacted as well as the Cemeteries Regulation Unit of the Ministry of Consumer Services must be contacted. In situations where human remains are associated with archaeological resources, MTCS should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.

### Screening for Impacts to Built Heritage and Cultural Heritage Landscapes

This checklist is intended to help proponents determine whether their project could affect known or potential cultural heritage resources. The completed checklist should be returned to the appropriate Heritage Planner or Heritage Advisor at the Ministry of Tourism and Culture.

Step 1 – Screening for Recognized Cultural Heritage Value			
YES	NO	Unknown	
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	1. Is the subject property designated or adjacent* to a property designated under the <i>Ontario Heritage Act</i> ?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	2. Is the subject property listed on the municipal heritage register or a provincial register/list? (e.g. Ontario Heritage Bridge List)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	3. Is the subject property within or adjacent to a Heritage Conservation District?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4. Does the subject property have an Ontario Heritage Trust easement or is it adjacent to such a property?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5. Is there a provincial or federal plaque on or near the subject property?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6. Is the subject property a National Historic Site?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	7. Is the subject property recognized or valued by an Aboriginal community?
Step 2 – Screening Potential Resources			
YES	NO	Unknown	<b>Built heritage resources</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	1. Does the subject property or an adjacent property contain any buildings or structures <b>over forty years old<sup>†</sup></b> that are:
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Residential structures (e.g. house, apartment building, shanty or trap line shelter)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Farm buildings (e.g. barns, outbuildings, silos, windmills)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Industrial, commercial or institutional buildings (e.g. a factory, school, etc.)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Engineering works (e.g. bridges, water or communications towers, roads, water/sewer systems, dams, earthworks, etc.)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Monuments or Landmark Features (e.g. cairns, statues, obelisks, fountains, reflecting pools, retaining walls, boundary or claim markers, etc.)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	2. Is the subject property or an adjacent property associated with a known architect or builder?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	3. Is the subject property or an adjacent property associated with a person or event of historic interest?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	4. When the municipal heritage planner was contacted regarding potential cultural heritage value of the subject property, did they express interest or concern?
YES	NO	Unknown	<b>Cultural heritage landscapes</b>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	5. Does the subject property contain landscape features such as:
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Burial sites and/or cemeteries
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Parks or gardens
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Quarries, mining, industrial or farming operations
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Canals
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Prominent natural features that could have special value to people (such as waterfalls, rocky outcrops, large specimen trees, caves, etc.)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	▪ Evidence of other human-made alterations to the natural landscape (such as trails, boundary or way-finding markers, mounds, earthworks, cultivation, non-native species, etc.)
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	6. Is the subject property within a Canadian Heritage River watershed?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	7. Is the subject property near the Rideau Canal Corridor UNESCO World Heritage Site?
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	8. Is there any evidence from documentary sources (e.g., local histories, a local recognition program, research studies, previous heritage impact assessment reports, etc.) or local knowledge or Aboriginal oral history, associating the subject property/ area with historic events, activities or persons?

**Note:**

If the answer is "yes" to any question in Step 1, proceed to Step 3.

The following resources can assist in answering questions in Step 1:

**Municipal Clerk or Planning Department** – Information on properties designated under the Ontario Heritage Act (individual properties or Heritage Conservation Districts) and properties listed on a Municipal Heritage register.

**Ontario Heritage Trust** – Contact the OHT directly regarding easement properties. A list of OHT plaques can be found on the website: [Ontario Heritage Trust](#)

**Parks Canada** – A list of National Historic Sites can be found on the website: [Parks Canada](#)

**Ministry of Tourism and Culture** – The Ontario Heritage Properties Database includes close to 8000 identified heritage properties. Note while this database is a valuable resource, it has not been updated since 2005, and therefore is not comprehensive or exhaustive. [Ontario Heritage Properties Database](#)

**Local or Provincial archives**

**Local heritage organizations**, such as the municipal heritage committee, historical society, local branch of the Architectural Conservancy of Ontario, etc.

Consideration should also be given to obtaining oral evidence of CHR. For example, in many Aboriginal communities, an important means of maintaining knowledge of cultural heritage resources is through oral tradition.

If the answer is "yes" to any question in Step 2, an evaluation of cultural heritage value is required. If cultural heritage resources are identified, proceed to Step 3.

If the answer to any question in Step 1 or to questions 2-4, 6-8 in Step 2, is "unknown", further research is required.

If the answer is "yes" to any of the questions in Step 3, a heritage impact assessment is required.

If uncertainty exists at any point, the services of a qualified person should be retained to assist in completing this checklist. All cultural heritage evaluation reports and heritage impact assessment reports **must** be prepared by a qualified person. Qualified persons means individuals (professional engineers, architects, archaeologists, etc.) having relevant, recent experience in the identification and conservation of cultural heritage resources. Appropriate evaluation involves gathering and recording information about the property sufficient to understand and substantiate its heritage value; determining cultural heritage value or interest based on the advice of qualified persons and with appropriate community input. If the property meets the criteria in Ontario Regulation 9/06 under the Ontario Heritage Act, it is a cultural heritage resource.

<sup>†</sup> The 40 year old threshold is an indicator of potential when conducting a preliminary survey for identification of cultural heritage resources. While the presence of a built feature that is 40 or more years old does not automatically signify cultural heritage value, it does make it more likely that the property could have cultural heritage value or interest. Similarly, if all the built features on a property are less than 40 years old, this does not automatically mean the property has no cultural heritage value. Note that age is not a criterion for designation under the *Ontario Heritage Act*.

Step 3 – Screening for Potential Impacts		
YES	NO	Will the proposed undertaking/project involve or result in any of the following potential impacts to the subject property or an adjacent* property?
<input type="checkbox"/>	<input type="checkbox"/>	<b>Destruction, removal or relocation</b> of any, or part of any, heritage attribute or feature.
<input type="checkbox"/>	<input type="checkbox"/>	<b>Alteration</b> (which means a change in any manner and includes restoration, renovation, repair or disturbance).
<input type="checkbox"/>	<input type="checkbox"/>	<b>Shadows</b> created that alter the appearance of a heritage attribute or change the exposure or visibility of a natural feature or plantings, such as a garden.
<input type="checkbox"/>	<input type="checkbox"/>	<b>Isolation</b> of a heritage attribute from its surrounding environment, context or a significant relationship.
<input type="checkbox"/>	<input type="checkbox"/>	<b>Direct or indirect obstruction</b> of significant views or vistas from, within, or to a built or natural heritage feature.
<input type="checkbox"/>	<input type="checkbox"/>	<b>A change in land use</b> such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces.
<input type="checkbox"/>	<input type="checkbox"/>	<b>Soil disturbance</b> such as a change in grade, or an alteration of the drainage pattern, or excavation, etc.

\* For the purposes of evaluating potential impacts of development and site alteration "adjacent" means: contiguous properties as well as properties that are separated from a heritage property by narrow strip of land used as a public or private road, highway, street, lane, trail, right-of way, walkway, green space, park, and/or easement or as otherwise defined in the municipal official plan.

## Madden, Donna

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**From:** Litner, Matthew (IO) <Matthew.Litner@infrastructureontario.ca>  
**Sent:** Thursday, June 19, 2014 3:59 PM  
**To:** Madden, Donna  
**Subject:** Highway 401 Interchange Imp from Hespeler Rd to Townline Rd  
**Attachments:** IO EA Notice Letter.pdf; Highway 401 Interchange Imp from Hespeler Rd to Townline Rd Notice.pdf

Hi Donna,

On behalf of Lisa Myslicki, please refer to attached PDF that highlights next steps **only if** your project impacts lands owned/managed by Infrastructure Ontario. If your project does not impact any lands owner/managed by IO, please remove IO from the mailing list.

Thank you,

**Matthew Litner** BES, DipEA  
Summer Associate - Environmental Management  
Ontario Infrastructure and Lands Corporation  
✉ [Matthew.Litner@infrastructureontario.ca](mailto:Matthew.Litner@infrastructureontario.ca)

This email, including any attachments, is intended for the personal and confidential use of the recipient(s) named above. If you are not the intended recipient of the email, you are hereby notified that any dissemination or copying of this email and/or any attachment files is strictly prohibited. If you have received this e-mail in error, please immediately notify the sender and arrange for the return of any and all copies and the permanent deletion of this message including any attachments, without reading it or making a copy. Thank you.

June 19, 2014

To whom it may concern,

Thank you for circulating Infrastructure Ontario (IO) on your Notice. Infrastructure Ontario is the strategic manager of the provincial government's real estate with a mandate of maintaining and optimizing value of the portfolio while ensuring real estate decisions reflect public policy objectives of the government.

As you may be aware, *IO is responsible for managing property that is owned by Her Majesty the Queen in Right of Ontario as represented by the Minister of Infrastructure (MOI)*. There is a potential that IO managed lands fall within your study area. As a result, your proposal may impact IO managed properties and/or the activities of tenants present on IO-managed properties. In order to determine if IO property is within your study area, IO requires that the proponent of the project conduct a title search by reviewing parcel register(s) for adjoining lands, to determine the extent of ownership by MOI or its predecessor's ownership (listed below). Please contact IO if any ownership of provincial government lands are known to occur within your study area and are proposed to be impacted. IO managed land can ***include within the title but is not limited to*** variations of the following: Her Majesty the Queen/King, OLC, ORC, Public Works, Hydro One, PIR, MGS, MBS, MOI, MTO, MNR and MEI\*. Please ensure that a copy of your notice is also sent to the ministry/agency on title. As an example, if the study area includes a Provincial Park, then MNR is to also to be circulated notices related to your project.

*IO obligates proponents to complete all due diligence for any realty activity on IO managed lands and this should be incorporated into all project timelines.*

### **Potential Negative Impacts to IO Tenants and Lands**

#### *General Impacts*

Negative environmental impacts associated with the project design and construction, such as the potential for dewatering, dust, noise and vibration impacts, impacts to natural heritage features/habitat and functions, etc should be avoided and/or appropriately mitigated in accordance with applicable regulations best practices as well as Ministry of Natural Resources (MNR) and Ministry of the Environment (MOE) standards. Avoidance and mitigation options that characterize baseline conditions and quantify the potential impacts should be present as part of the EA project file. Details of appropriate mitigation, contingency plans and triggers for implementing contingency plans should also be present.

#### *Impacts to Land holdings*

Negative impacts to land holdings, such as the taking of developable parcels of IO managed land or fragmentation of utility or transportation corridors, should be avoided. If the potential for such impacts is present as part of this undertaking, you should contact the undersigned to discuss these issues at the earliest possible stage of your study.

If takings are suggested as part of any alternative, these should be appropriately mapped and quantified within the EA report documentation. In addition, details of appropriate mitigation and or next steps related to compensation for any required takings should be present. IO requests circulation of the draft EA report prior to finalization if potential impacts to IO-managed lands are present as part of this study.

*Impacts to Cultural Heritage*

Should the proposed activities impact cultural heritage features on IO managed lands, a request to examine cultural heritage features, which can include cultural landscapes, built heritage, and archaeological potential and/or sites, could be required. If the potential for such impacts is present as part of this undertaking, you should contact the undersigned to discuss these issues at the earliest possible stage of your study.

**Potential Triggers Related to MOI's Class EA**

*IO is required to follow the MOI Public Work Class Environmental Assessment Process for (PW Class EA). The PW Class EA applies to a wide range of realty and planning activities including leasing or letting, planning approvals, disposition, granting of easements, demolition and property maintenance/repair. For details on the PW Class EA please visit the Environment and Heritage page of our website found at*

<http://www.infrastructureontario.ca/Templates/Buildings.aspx?id=2147490336&langtype=1033>

*Please note that completion of any EA process does not provide an approval for MOI's Class EA obligations. Class EA processes are developed and in place to assess undertakings associated with different types of projects. For example, assessing the impacts of disposing of land from the public portfolio is significantly different then assessing the best location for a proposed road.*

IO is providing this information so that adequate timelines and project budgets can consider MOI's regulatory requirements associated with a proposed realty activity in support of a project. Some due diligences processes and studies can be streamlined. For example, prior to any disposition of land, at minimum a Phase I Environmental Site Assessment and a Stage I Archaeological Assessment and the MOI Category B Environmental Assessment should be undertaken.. Deficiencies in any of these requirements could result in substantial project delays and increased project costs.

In summary, the purchase of MOI-owned/IO-managed lands or disposal of rights and responsibilities (e.g. easement) for IO-managed lands triggers the application of the MOI Class EA. If any of these realty activities affecting IO-managed lands are being proposed as part of any alternative, please contact the Sales, Easements and Acquisitions Group through IO's main line (Phone: 416-327-3937, Toll Free: 1-877-863-9672), and also contact the undersigned at your earliest convenience to discuss next steps.

**Specific Comments**

Please remove IO from your circulation list, with respect to this project, if MOI owned lands are not anticipated to be impacted. In addition, in the future, please send only **electronic copies of notices** for any projects impacting IO managed lands to:  
Keith.Noronha@infrastructureontario.ca

Thank you for the opportunity to provide initial comments on this undertaking. If you have any questions I can be reached at the contacts below.

Sincerely,

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**Lisa Myslicki**  
Environmental Advisor, Environmental Management  
Infrastructure Ontario  
1 Dundas Street West,  
Suite 2000, Toronto, Ontario  
M5G 2L5  
(416) 212-3768  
lisa.myslicki@infrastructureontario.ca

\* Below are the acronyms for agencies/ministries listed in the above letter

OLC	Ontario Lands Corporation
ORC	Ontario Realty Corporation
PIR	Public Infrastructure and Renewal
MGS	Ministry of Government Services
MBS	Management Board and Secretariat
MOI	Ministry of Infrastructure
MTO	Ministry of Transportation
MNR	Ministry of Natural Resources
MEI	Ministry of Energy and Infrastructure

July 11, 2014

Donna Madden  
Senior Environmental Planner  
Stantec Consulting Ltd.  
200-836 Paramount Drive  
Stoney Creek, ON L8J 0B4

**Re: MNR Comments – Notice of Study Commencement: Highway 401 Interchange Improvements from Hespeler Road to Townline Road, City of Cambridge, Region of Waterloo (GWP 3060-11-00)**

Dear Ms. Madden:

The Ministry of Natural Resources (MNR) Guelph District Office is in receipt of the above-described notice and offers the following comments for consideration:

**Natural Heritage**

*Wetlands*

The MNR notes that the Puslinch Lake Irish Creek Provincially Significant Wetland (PSW) Complex is in close proximity to the proposed undertaking. Please ensure that any potential direct or indirect impacts to the PSW be identified and addressed through the EA process, including avoidance of impacts to the PSW or mitigation.

*Species at Risk*

There are known occurrences in the area for a number of species at risk (SAR): Blanding's Turtle (Threatened), Chimney Swift (Threatened), Barn Swallow (Threatened), Bald Eagle (Special Concern), Snapping Turtle (Special Concern) and Milksnake (Special Concern). Please be advised that because the province has not been surveyed comprehensively for the presence of SAR, the absence of a record is not an appropriate indicator for the absence of SAR from a geographic area. To determine the presence of SAR for a given study area, Guelph District's recommended approach includes the following:

1. **Habitat Inventory**: The District recommends undertaking a comprehensive botanical inventory for the entire study area that may be subject to direct or indirect impacts from the proposed activity. The vegetation communities should be classified as per the "*Ecological Land Classification (ELC) for Southern Ontario*" system, to either "Ecosite" or "Vegetation Type" level. If there are any aquatic habitats in the study area, the District recommends you collect data on the physical characteristics of the waterbodies.

2. Potential Species at Risk Within the Study Area – A list of SAR that have the potential to occur in the area can be produced by cross-referencing the ecosites described during the habitat inventory with the habitat descriptions of SAR known to occur within the Region. The list of SAR known to occur in **Waterloo Region** is attached for your reference. The species-specific Committee on the Status of Endangered Wildlife in Canada (COSEWIC) status reports ([www.cosewic.gc.ca](http://www.cosewic.gc.ca)) provide comprehensive technical information on habitat needs and will be helpful in determining the suitability of the study areas' ecosites for a given species.

Please note that the Species at Risk in Ontario (SARO) list is a living document and is amended from time to time as a result of species assessment and re-assessment conducted by the Committee on the Status of Species at Risk in Ontario (COSSARO). The SARO list can be accessed via the following link:

[http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR\\_SAR\\_CSSR\\_SARO\\_LST\\_EN.html](http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR_SAR_CSSR_SARO_LST_EN.html)

COSSARO also maintains a list of species to be assessed in the future. It is recommended to take COSSARO's list of anticipated assessments into consideration, especially when the proposed start date of the activity is more than six months away, or the project will be undertaken over a period greater than six months. This list can be viewed at:

[http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR\\_SAR\\_CSSR\\_CLNDR\\_CN\\_DDT\\_SP\\_EN.html](http://www.mnr.gov.on.ca/en/Business/Species/2ColumnSubPage/MNR_SAR_CSSR_CLNDR_CN_DDT_SP_EN.html)

3. Species at Risk Surveys – The District is of the opinion that each SAR identified under Step II should be surveyed for, regardless of whether or not the species has been previously recorded for the area. The survey report should describe how each SAR was surveyed for, and provide a rationale for why certain species appearing on the regional municipality list were not afforded a survey. The rationale must be based on evidence demonstrating that suitable habitat for a specific SAR is not present within the study area; or, that the project will not have any impacts (including indirect impacts) on the specific species. Some SAR surveys require an authorization under the *Endangered Species Act 2007* and/or a Scientific Collector's Permit; please contact Graham Buck, Management Biologist at [graham.buck@ontario.ca](mailto:graham.buck@ontario.ca) should you require further direction.

Should you have any questions, please contact the undersigned at 519-826-4912 or at [annemarie.laurence@ontario.ca](mailto:annemarie.laurence@ontario.ca).

Yours truly,



Anne Marie Laurence  
A/District Planner

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange Improvements from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3060-11-00)**

**REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014**

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Date: \_\_\_\_\_

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<b>Contact Information on Study Mailing List</b> Chief <del>Michael</del> , of Police Waterloo Regional Police Service 200 Maple Grove Road PO Box 3070 Cambridge ON N3H 5M1 Tel: (519) 650-8500 ext. 8520	<i>If this information is incorrect please provide updated information or identify the key project contact below.</i>
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**Please return the completed form to:**

Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

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**Key Project Contact:**

Job Title: \_\_\_\_\_

Name of Group/Agency: \_\_\_\_\_

Mailing Address: \_\_\_\_\_

Tel: \_\_\_\_\_

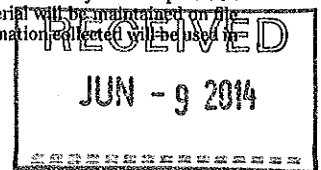
Fax: \_\_\_\_\_

E-mail: \_\_\_\_\_

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Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.



**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3060-11-00)

**REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014**

Date: June 4 / 14

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<b>Contact Information on Study Mailing List</b> Ms. Karen Landry, CAO/Clerk Township of Puslinch 7404 Wellington Road 34 Guelph ON N1H 6H9 Tel: (519) 763-1226 Ext. 214	<i>If this information is incorrect please provide updated information or identify the key project contact below.</i>
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**Please return the completed form to:**

Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

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## **Madden, Donna**

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**From:** Nuala Larwood <NLarwood@puslinch.ca>  
**Sent:** Wednesday, June 11, 2014 1:53 PM  
**To:** Madden, Donna  
**Subject:** Detailed Design and Class Environmental Assessment Study - Highway 401 Interchange Improvements from Hespeler Road to Townline Road - City of Cambridge (GWP 3060-11-00)  
**Attachments:** 1522\_001.pdf  
**Importance:** High

Good Afternoon Donna:

Our CAO/Clerk Karen Landry has asked that I forward the attached form onto you.

Please see the attached form in respect of the Highway 401 Interchange Improvement from Hespeler Road to Townline Road City of Cambridge (GWP 3060-11-000)

The Township of Puslinch does not have any further comments at this time.

Kind Regards,

Nuala Larwood  
Township of Puslinch  
7404 Wellington Rd 34, Guelph, ON N1H 6H9  
P 519 763-1226 ext. 224 F 519 763-5846  
[www.puslinch.ca](http://www.puslinch.ca)

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**From:** [copier@twp.puslinch.on.ca](mailto:copier@twp.puslinch.on.ca) [<mailto:copier@twp.puslinch.on.ca>]  
**Sent:** June-11-14 2:28 PM  
**To:** Nuala Larwood  
**Subject:** Attached Scanned Image

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3060-11-00)**

**REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014**

Date: JUNE 2, 2014

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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10 Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<p><b>Contact Information on Study Mailing List</b>  <del>Ms. April Sowand</del>, Senior Environmental Planner          Cambridge Environmental Advisory Committee          3rd Floor, 50 Dickson Street          PO Box 669          Cambridge ON N1R 5W8          Tel: (519) 740-4650 Ext. 4601</p>	<p><i>If this information is incorrect please provide updated information or identify the key project contact below.</i></p>
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**Please return the completed form to:**  
Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.



DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3060-11-00)

REPLY FORM – PLEASE RETURN BY FRIDAY, JUNE 27, 2014

Date: JUNE 3/14

- Please remove this agency from the study mailing list.  
 Comments have not changed since Preliminary Design.  
 Please consider the following input during the Detailed Design Study.

Removed  
July/14

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<b>Contact Information on Study Mailing List</b> Ms. Valerie Spring, Heritage Planner Cambridge Municipal Heritage Advisory Committee 50 Dickson Street PO Box 669 Cambridge ON N1R 8S1 Tel: (519) 740-4650 ext. 4580	<i>If this information is incorrect please provide updated information or identify the key project contact below.</i>
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**Please return the completed form to:**

Mrs. Donna Madden, Senior Environmental Planner, Stantec Consulting Ltd.  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Tel. (905) 381-3217, Fax (905) 385-3534, Email: donna.madden@stantec.com

**Key Project Contact:**

Job Title:

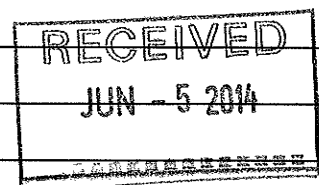
Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

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## Madden, Donna

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**From:** Madden, Donna  
**Sent:** Friday, June 20, 2014 9:48 AM  
**To:** 'Kenneth McArthur'  
**Cc:** Barg, Adam; 'Ward, Roger (MTO)'; Cooper, Kira (MTO) (Kira.Cooper@ontario.ca)  
**Subject:** RE: Noise Wall - Hespeler

Dear Mr. McArthur,

Thank you for taking the time to review the project website and comment on this project. This is to acknowledge receipt of your email. A formal response will be provided once we have had an opportunity to review the issue with the project team.

Sincerely,

Donna M. Madden, CPT  
Senior Environmental Planner  
Stantec  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4  
Phone: 905-381-3217  
Cell: 289-237-9268  
Fax: 905-385-3534  
[donna.madden@stantec.com](mailto:donna.madden@stantec.com)

[stantec.com](http://stantec.com)

The content of this email is the confidential property of Stantec and should not be copied, modified, retransmitted, or used for any purpose except with Stantec's written authorization. If you are not the intended recipient, please delete all copies and notify us immediately.

-----Original Message-----

From: Kenneth McArthur [<mailto:kenneth.r.mcarthur@gmail.com>]  
Sent: Thursday, June 19, 2014 9:28 PM  
To: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
Subject: Noise Wall - Hespeler

From: Kenneth McArthur <[kenneth.r.mcarthur@gmail.com](mailto:kenneth.r.mcarthur@gmail.com)>  
Subject: Noise Wall - Hespeler  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

Hi,

I was surprised to read that a noise wall is being built south of the 401 adjacent to a primarily commercial and industrial area while no noise wall is planned north of the 401 where the land is mostly residential.

I live north of the 401 in Hespeler at 99 Kerwood Drive. Depending on the environmental conditions (i.e. temperature, humidity, etc.), the noise from 401 can be heard clearly in my home. This noise pollution has a negative impact on the quality of life. The addition of more lanes of traffic has me further concerned about noise pollution.

Why was it determined that a noise wall is not required north of the 401?

Thanks,

Ministry of Transportation

Engineering Office  
Environmental Section  
West Region

659 Exeter Road  
London, Ontario N6E 1L3  
Telephone: (519) 873-4560  
Facsimile: (519) 873-4600  
Email: Cathy.Giesbrecht@ontario.ca

Ministère des Transports

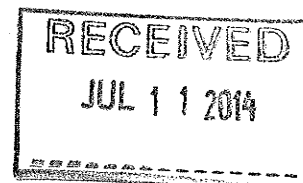
Bureau du génie  
Section de l'environnement  
Région de l'Ouest

659, rue Exeter  
London (Ontario) N6E 1L3  
Téléphone: (519) 873-4560  
Télécopieur: (519) 873-4600



M2014-3524

July 8, 2014



Ms. Hazel E. Hill  
Director, Haudenosaunee Development Institute  
Six Nations of the Grand River Territory  
Suite 407, 16 Sunrise Court  
PO Box 714  
Ohsweken Ontario N0A 1M0

Dear Ms. Hill,

Thank you for your June 11, 2014 letter in response to our notice of commencement for the Detailed Design and Class EA study for Highway 401 improvements from Hespeler Road to Townline Road, in the City of Cambridge. I have been asked to respond.

Letters dated November 17, 2009, November 22, 2011, and November 13, 2012 were previously sent to Leroy Hill, Council Secretary, regarding the Preliminary Design for the larger Highway 401 improvements study from Hespeler Road to the Wellington County/Halton Region Boundary and the Transportation Environmental Study Report (TESR) review period.

Under the approved MTO Class Environmental Assessment (Class EA) process which this study is following, the Ministry consults with the public and with persons who may be affected by the undertaking. In addition, there may be a legal duty to consult with Aboriginal peoples if the project has the potential to adversely impact Aboriginal or treaty rights. The Notice of Study Commencement is part of our consultation process and provides an opportunity to share information and seek feedback and information from First Nation communities about the study area and the potential impacts of the project.

Procedural aspects of consultation are those portions of consultation related to the process of consultation. Depending on the extent of consultation required in the circumstance, the procedural aspects of consultation that the Crown can delegate to a project consultant can include:

- Providing notice about the project to the Aboriginal communities identified by the Crown for consultation;

- Supplying information and adequate time to the Aboriginal communities to comment on the potential adverse impacts of the project on established or asserted Aboriginal or treaty rights;
- Providing information about the project in a manner, with sufficient detail and at a stage in the process that allows the Aboriginal communities to prepare their views on the project;
- Following up, as necessary, with Aboriginal communities to ensure they received project information and are aware of the opportunity to comment and raise questions or concerns;
- Considering and responding to comments and concerns provided by Aboriginal communities; and,
- As appropriate, discussing and potentially implementing changes to the project in response to concerns raised by Aboriginal communities.

Your letter states the Haudenosaunee hold treaty rights recognized by the 1701 Treaty of Albany, which you advise the Highway improvements project will have a negative impact upon. Please provide further information about the scope and nature of activities or interests of the community related to Aboriginal and treaty rights, which could be adversely impacted by this Highway 401 project.

Thank you for responding to our notice of study commencement. Please let me know if you want to meet to discuss the project in more detail. I look forward to your response.

Yours Sincerely,



Cathy Giesbrecht  
Head, Environmental Section  
West Region

- c. Leroy Hill, Secretary, Haudenosaunee Confederacy Chiefs Council  
Adam Barg, Stantec Consulting Ltd.  
Roger Ward, MTO  
Adele Mochrie, MTO

# NOTICE OF STUDY UPDATE DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY

## Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00)

### THE STUDY

The **Ministry of Transportation, Ontario (MTO)** has retained **Stantec Consulting Ltd.** to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo as shown on the key plan below. Since the issuance of the Notice of Study Commencement in May 2014 (formerly GWP 3060-11-00), the project has been divided into two phases and is tentatively scheduled for consecutive construction, subject to completion of the Detailed Design and Class EA Study, funding, and approvals:

- **Phase I** (2018) - Replacement of the Franklin Boulevard bridge including some work at the interchange, rehabilitation of retaining walls at the Townline Road bridge, and an expansion of the commuter parking lot at the Townline Road interchange.
- **Phase II** (Starting in 2019 to 2021) - Widening of Highway 401 to ten lanes, including High Occupancy Vehicle (HOV) lanes; reconstruction of the Highway 401 interchanges at Franklin Boulevard and Hespeler Road; replacement of the Hespeler Road bridge; and retaining walls as needed. Drainage improvements, illumination, overhead signs and traffic signals will be completed as part of this phase.

A section of a new noise wall is proposed along the south side of Highway 401, west of Franklin Boulevard. The study will include the provision for pedestrian/cyclist travel across Highway 401 on Franklin Boulevard and Hespeler Road in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge.

The construction staging/traffic management strategy during the replacement of the Franklin Boulevard Bridge will include a temporary full closure of this interchange with no access across Highway 401 during construction.



### PROCESS

This study is following the approved planning process for Group 'B' projects under the MTO *Class Environmental Assessment (Class EA) for Provincial Transportation Facilities (2000)*. The Preliminary Design of the project was included in the Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary study (GWP 8-00-00) which concluded with the publication of a Transportation Environmental Study Report (TESR). The TESR received Environmental Assessment (EA) clearance in November 2013.

A Public Information Centre (PIC) is planned for the fall 2017 to present the Recommended Plan for improvements for both Phase I and II, as well as the proposed construction staging and traffic management plans for each. The date, time, and location of the PIC will be advertised in local newspapers. A Design and Construction Report (DCR) documenting each phase will be made available for a 30-day public review period with public notice advising of the start of the review period.

### COMMENTS

We are interested in hearing your comments regarding the project. If you would like to have your name added to the mailing list or to provide comments, please contact one of the following individuals:

**Kevin Welker, P.Eng., ENV SP**  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: kevin.welker@stantec.com

**Scott Howard**  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3rd Floor  
London, ON N6E 1L3  
tel: 519-873-4568  
toll-free: 1-800-265-6072, ext. 519-873-4568  
fax: 519-873-4600  
e-mail: scott.howard@ontario.ca

For up-to-date project information and to submit comments to the project team at any time during the study, please visit the project website: [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

External Agency Contact List

Highway 401 Interchange and Highway Improvements  
from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00)

Title	First Name	Last Name	Position	Organization	Address 1	Address 2	City	Prov	PostC	Telephone	Fax	Email	Notes
<b>Provincial Agencies</b>													
Ms.	Kathryn	McGarry	MPP - Cambridge	Constituency Office	498 Eagle Street North		Cambridge	ON	N3H 1C2	(519) 623-5852	(519) 650-3918	kmcgarry.mpp.co@liberal.ola.org	
Mr.	Ted	Arnott	MPP - Wellington-Halton Hills	Constituency Office	181 St. Andrew Street East	2nd Floor	Fergus	ON	N1M 1P9	(519) 787-5247	(519) 787-5249	ted.arnottco@pc.ola.org	
S./Sgt.	John	Mraud	Detachment Commander	Ontario Provincial Police, Cambridge Detachment	500 Beaverdale Road	Box 21060	Cambridge	ON	N3C 4B1	(519) 654-0150			
Insp.	Scott	Lawson	Detachment Commander	Ontario Provincial Police, Wellington County Detachment	5145 Wellington Rd 27		Rockwood	ON	N0B 2K0	(519) 856-1506			
Ms.	Carol	Neumann	Rural Planner	Ministry of Agriculture, Food and Rural Affairs, Environmental and Land Use Policy	1 Stone Road West	Ontario Government Building 3rd Floor	Guelph	ON	N1G 4Y2	(519) 846-3393		carol.neumann@ontario.ca	
Mr.	Louis	Bitonti	Senior Planner	Ministry of Municipal Affairs and Housing, Community Planning and Development	777 Bay Street	College Park 13th Floor	Toronto	ON	M5G 2E5	(416) 585-6910		<a href="mailto:louis.bitonti@ontario.ca">louis.bitonti@ontario.ca</a>	
Mr.	Graham	Buck	Management Biologist	Ministry of Natural Resources and Forestry - Guelph District Office	1 Stone Road West		Guelph	ON	N1G 4Y2	(519) 826-4505		<a href="mailto:graham.buck@ontario.ca">graham.buck@ontario.ca</a>	
Mr.	David	Marriott	District Planner	Ministry of Natural Resources and Forestry - Guelph District Office	1 Stone Road West		Guelph	ON	N1G 4Y2	(519) 826-4926		<a href="mailto:david.marriott@ontario.ca">david.marriott@ontario.ca</a>	
Ms.	Barbara	Slattery	Environmental Resource Planner & EA Coordinator	Ministry of the Environment and Climate Change, West Central Region	119 King Street West	12th Floor	Hamilton	ON	L8P 4Y7	(905) 521-7864		<a href="mailto:barbara.slattery@ontario.ca">barbara.slattery@ontario.ca</a>	
Mr.	Joseph	Muller	Heritage Planner	Ministry of Tourism, Culture and Sport, Programs and Services Branch	401 Bay Street		Toronto	ON	M7A 0A7	(416) 314-7145		<a href="mailto:joseph.muller@ontario.ca">joseph.muller@ontario.ca</a>	
Ms.	Maya	Harris	Manager, Growth Planning and Analysis	Ministry of Municipal Affairs and Housing, Ontario Growth Secretariat	777 Bay Street	College Park 4th Floor	Toronto	ON	M5G 2E5	(416) 325-7327		<a href="mailto:maya.harris@ontario.ca">maya.harris@ontario.ca</a>	
<b>Municipal</b>													
Mr.	Michael	Di Lullo	City Clerk	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 740-4680	(519) 740-3011		
Mr.	Michael	McGann	Manager of Design and Approvals (Engineering Services)	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 621-0740 ext. 4478	(519) 740-7729	<a href="mailto:mcgannm@cambridge.ca">mcgannm@cambridge.ca</a>	
Mr.	George	Barnes	Director, Community Services Operations	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 740-4681	(519) 740-6566		
Mr.	James	Austin	Director, Public Works	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 621-0740			
Sir/Madam			Director of Engineering Services	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 621-0740			
Ms.	Shannon	Noonan	Manager of Transportation	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8				
Mr.	Kent	McVittie	Deputy City Manager, Community Services	The City of Cambridge	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 740-4681 Ext. 4596			
Mr.	Neil	Main	Fire Chief	The City of Cambridge - Fire Department	1625 Bishop Street North		Cambridge	ON	N1R 7J4	(519) 621-6001 ext. 2218			
Ms.	Kathy	Padgett	Senior Environmental Planner	Cambridge Environmental Advisory Committee	50 Dickson Street	PO Box 669	Cambridge	ON	N1R 5W8	(519) 621-0740 Ext. 4826	(519) 622-6184	<a href="mailto:padgett@cambridge.ca">padgett@cambridge.ca</a>	
Ms.	Karen	Landry	CAO/Clerk	Township of Puslinch	7404 Wellington Road 34		Puslinch	ON	N0B 2J0	(519) 763-1226 Ext. 224	(519) 763-5846	<a href="mailto:klandry@puslinch.ca">klandry@puslinch.ca</a>	
Mr.	Don	Creed	Director of Public Works and Parks	Township of Puslinch	7404 Wellington Road 34		Puslinch	ON	N0B 2J0	(519) 763-1470		<a href="mailto:dcreed@puslinch.ca">dcreed@puslinch.ca</a>	
Mr.	Steven	Goode	Fire Chief	Township of Puslinch Fire and Rescue Services	7404 Wellington Road 34		Puslinch	ON	N0B 2J0	(519) 821-3010	(519) 836-6421		
Ms.	Kris	Fletcher	Regional Clerk	Regional Municipality of Waterloo	150 Frederick Street	P.O. Box 9051, Station C	Kitchener	ON	N2G 4J3	(519) 575-4400 ext. 4427			
Mr.	Steve	Van De Keere	Director, Transportation	Regional Municipality of Waterloo	150 Frederick Street	P.O. Box 9051, Station C	Kitchener	ON	N2G 4J3				
Mr.	Chris	Gosselin	Manager, Environmental Planning	Regional Municipality of Waterloo	150 Frederick Street	P.O. Box 9051, Station C	Kitchener	ON	N2G 4J3	(519) 575-4501			
Mr.	Robert	Henderson	Manager, Transportation Engineering	Regional Municipality of Waterloo	150 Frederick Street	P.O. Box 9051, Station C	Kitchener	ON	N2G 4J3	(519) 575-4400 ext. 4515			
Mr.	Stephen	Van Valkenburg	Chief of EMS	Region of Waterloo Paramedic Services	120 Maple Grove Road		Cambridge	ON	N3H 4R6	(519) 650-4400 ext. 8710		<a href="mailto:svanvalkenburg@regionofwaterloo.ca">svanvalkenburg@regionofwaterloo.ca</a>	
Ms.	Donna	Bryce	County Clerk	County of Wellington	74 Woolwich Street		Guelph	ON	N1H 3T9	(519) 837-2600 ext. 2520	(519) 837-1909		
Mr.	Paul	Johnson	Operations Manager	County of Wellington, Engineering Services	74 Woolwich Street		Guelph	ON	N1H 3T9	(519) 837-2600 ext. 2230	(519) 837-1909		
Mr.	Stephen	Dewar	EMS Chief	Guelph-Wellington Paramedic Service	160 Clair Road West		Guelph	ON	N1L 1G1	(519) 822-1260 ext. 2891	(519) 824-5960	<a href="mailto:stephen.dewar@guelph.ca">stephen.dewar@guelph.ca</a>	
Mr.	Bryan	Larkin	Chief of Police	Waterloo Regional Police Service	200 Maple Grove Road	PO Box 3070	Cambridge	ON	N3H 5M1	(519) 650-8500 ext. 8520		<a href="mailto:bryan.larkin@wrps.on.ca">bryan.larkin@wrps.on.ca</a>	
Mr.	Blair	Allen	Supervisor - Transit Development	Grand River Transit	250 Strasburg Road		Kitchener	ON	N2E 3M6				
Mr.	Warren	Stauch	Foundation Chair	Waterloo Regional Heritage Foundation	P.O. Box 9051	Station C	Kitchener	ON	N2G 4J3	(519) 575-4732			
<b>Interest Groups</b>													
Mr.	Fred	Natolochny	Supervisor of Resource Planning	Grand River Conservation Authority	400 Clyde Road	PO Box 729	Cambridge	ON	N1R 5W6	(519) 621-2761 ext. 2229	(519) 621-4945	<a href="mailto:fnatolochny@grandriver.ca">fnatolochny@grandriver.ca</a>	
Mr.	Greg	Durocher	President/CEO	Cambridge Chamber of Commerce	750 Hespeler Road		Cambridge	ON	N3H 5L8	(519) 622-2221	(519) 622-0177	<a href="mailto:cchamber@cambridgechamber.com">cchamber@cambridgechamber.com</a>	
Mr.	Ian	McLean	President/CEO	Greater Kitchener-Waterloo Chamber of Commerce	80 Queen Street North	PO Box 2367	Kitchener	ON	N2H 6L4	(519) 749-6038			
Mr.	John	MacTaggart	Senior Engineering Services Officer	Canadian National Railway - Regional Engineering/Engineering Services	1 Administration Road	PO Box 1000	Concord	ON	L4K 1B9				
Mr.	Burkhard	Mausberg	CEO	Friends of the Greenbelt Foundation	661 Yonge Street	Suite 500	Toronto	ON	M4Y 1Z9	(416) 960-0001	(416) 960-0030		
Ms.	Lisa	Myslicki	Environmental Specialist	Infrastructure Ontario	1 Dundas Street West		Toronto	ON	M5G 1Z3	(416) 212-3768		<a href="mailto:lisa.myslicki@infrastructureontario.ca">lisa.myslicki@infrastructureontario.ca</a>	
Mr.	Norman	Ragetlie	President	Speed River Land Trust	1 Trent Lane		Guelph	ON	N1Q 2W1	(519) 821-5178	(519) 763-5261		
Mr.	Benoit	Bourgault	General Manager	Student Transportation Services of Waterloo Region	130 - 4275 King Street East		Kitchener	ON	N2P 2E9	(519) 650-4934 ext. 222	(519) 650-2979	<a href="mailto:benoit_bourgault@stswr.ca">benoit_bourgault@stswr.ca</a>	
Mr.	Chris	Baskys	Membership Manager	Ontario Cyclist Association	2-2015 Pan Am Boulevard		Milton	ON	L9T 8Y9	(416) 855-1717 ext. 1007		<a href="mailto:info@ontariocycling.org">info@ontariocycling.org</a>	
	c/o Nimet Manji		Executive Assistant	Ontario Heritage Trust	10 Adelaide Street East		Toronto	ON	M5C 1J3	(416) 314-4903	(416) 314-5304	<a href="mailto:nimet.manji@heritagetrust.on.ca">nimet.manji@heritagetrust.on.ca</a>	

## Utilities Contact List

## Highway 401 Interchange and Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00)

Email contacts provided by Adam Renaud June 9, 2017 for Study Update Notice

Utility	Contact Name	Email Address	Comments
Bell Canada	Chris Gill	<a href="mailto:chris.gill@bell.ca">chris.gill@bell.ca</a>	Notice of Study Update sent via email June 13/17
Energy Plus	Ken Redfern	<a href="mailto:kredfern@energyplus.ca">kredfern@energyplus.ca</a>	Notice of Study Update sent via email June 13/17
Rogers	Sue Green	<a href="mailto:Sue.Green@rci.rogers.com">Sue.Green@rci.rogers.com</a>	Notice of Study Update sent via email June 13/17
Hydro One Transmission	Lana Kegel	<a href="mailto:Lana.Kegel@HydroOne.com">Lana.Kegel@HydroOne.com</a>	Notice of Study Update sent via email June 13/17
Hydro One Distribution	Lindsay Lawrence	<a href="mailto:Lindsay.Lawrence@HydroOne.com">Lindsay.Lawrence@HydroOne.com</a>	Notice of Study Update sent via email June 13/17
Union Gas	John Stauffer	<a href="mailto:JStauffer@uniongas.com">JStauffer@uniongas.com</a>	Notice of Study Update sent via email June 13/17
Region of Waterloo	Kevin Dolishny	<a href="mailto:KDolishny@regionofwaterloo.ca">KDolishny@regionofwaterloo.ca</a>	Notice of Study Update sent via email June 13/17
Region of Waterloo	Steve Sieunarine	<a href="mailto:ssieunarine@regionofwaterloo.ca">ssieunarine@regionofwaterloo.ca</a>	Notice of Study Update sent via email June 13/17
City of Cambridge	Sarah Austin	<a href="mailto:AustinS@cambridge.ca">AustinS@cambridge.ca</a>	Notice of Study Update sent via email June 13/17



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

June 8, 2017  
File: 165000897

**Attention: Mr. Ted Arnott, MPP – Wellington-Halton Hills**  
Constituency Office  
2<sup>nd</sup> Floor, 181 St. Andrew Street East  
Fergus ON N1M 1P9

Dear Mr. Arnott,

**Reference: NOTICE OF STUDY UPDATE**  
**Highway 401 Interchange & Highway Improvements from Hespeler Road to**  
**Townline Road, City of Cambridge (GWP 3222-15-00)**

The Ministry of Transportation, Ontario (MTO) in partnership with Stantec Consulting Ltd. is undertaking the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. Since the issuance of the Notice of Study Commencement in May 2014, the project now includes the expansion of Highway 401 to ten lanes, including High Occupancy Vehicle (HOV) lanes. Also, the project has been divided into two phases and is tentatively scheduled for consecutive construction, subject to completion of the Detailed Design and Class EA, funding and approvals. The enclosed *Ontario Government Notice* (OGN) provides a key plan and additional details for your reference.

The *Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR), which received Environmental Assessment (EA) clearance in November 2013.

The project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)) has been updated to provide additional details including an interactive map, ongoing information, and an opportunity for interested stakeholders to submit input at any time during the study.

The OGN will be published in the Turtle Island News and the Two Row Times on Wednesday, June 14, 2017, the Cambridge Times on Thursday, June 15, 2017 and the Waterloo Region Record on Saturday, June 17, 2017.



June 8, 2017

Mr. Ted Arnott, MPP – Wellington-Halton Hills

Page 2 of 2

Reference: **NOTICE OF STUDY UPDATE**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3060-11-00)**

A Public Information Centre (PIC) is planned for the fall 2017 to present the Recommended Plan for improvements, as well as the proposed construction phasing and traffic management plan. You will be notified directly in advance of the PIC.

Yours truly,

**STANTEC CONSULTING LTD.**

Paula Burnard, MScPl, MCIP, RPP

Senior Environmental Planner

Tel: (519) 675-6666

Fax: (519) 645-6575

paula.burnard@stantec.com

Encl: Ontario Government Notice of Study Update

- c. S. Howard, Senior Project Manager, MTO West Region
- G. Mahabir, Area Manager, MTO West Region
- K. Houston, Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

June 8, 2017  
File: 165000897

**Attention: Ms. Kathryn McGarry, MPP – Cambridge**  
Constituency Office  
498 Eagle Street North  
Cambridge ON N3H 1C2

Dear Ms. McGarry,

**Reference: NOTICE OF STUDY UPDATE**  
**Highway 401 Interchange & Highway Improvements from Hespeler Road to**  
**Townline Road, City of Cambridge (GWP 3222-15-00)**

The Ministry of Transportation, Ontario (MTO) in partnership with Stantec Consulting Ltd. is undertaking the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. Since the issuance of the Notice of Study Commencement in May 2014, the project now includes the expansion of Highway 401 to ten lanes, including High Occupancy Vehicle (HOV) lanes. Also, the project has been divided into two phases and is tentatively scheduled for consecutive construction, subject to completion of the Detailed Design and Class EA, funding and approvals. The enclosed *Ontario Government Notice* (OGN) provides a key plan and additional details for your reference.

*The Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR), which received Environmental Assessment (EA) clearance in November 2013.

The project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)) has been updated to provide additional details including an interactive map, ongoing information, and an opportunity for interested stakeholders to submit input at any time during the study.

The OGN will be published in the Turtle Island News and the Two Row Times on Wednesday, June 14, 2017, the Cambridge Times on Thursday, June 15, 2017 and the Waterloo Region Record on Saturday, June 17, 2017.



June 8, 2017  
Ms. Kathryn McGarry, MPP – Cambridge  
Page 2 of 2

Reference: **NOTICE OF STUDY UPDATE**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3060-11-00)**

A Public Information Centre (PIC) will be held in the fall 2017 to present the Recommended Plan for improvements, as well as the proposed construction phasing and traffic management plan. You will be notified directly in advance of the PIC.

Yours truly,

**STANTEC CONSULTING LTD.**

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

Encl: Ontario Government Notice of Study Update

- c. S. Howard, Senior Project Manager, MTO West Region
- G. Mahabir, Area Manager, MTO West Region
- K. Houston, Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

June 12, 2017  
File: 165000897

Attention: First Name Last Name, Position  
Recipient's Address

**External Agencies**

Dear Title Last Name,

Reference: **NOTICE OF STUDY UPDATE**  
**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00)**

The Ministry of Transportation, Ontario (MTO) has retained Stantec Consulting Ltd. to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The enclosed *Ontario Government Notice* (OGN) provides a key plan and additional details for your reference.

The *Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR) which received Environmental Assessment (EA) clearance in November 2013.

The purpose of this letter is to inform you that since the issuance of the Notice of Study Commencement in May 2014, the project now includes the expansion of Highway 401 to ten lanes, including High Occupancy Vehicle (HOV) lanes. Also, the project has been divided into two phases and is tentatively scheduled for consecutive construction, subject to completion of the Detailed Design and Class EA, funding and approvals. The enclosed Comment Form provides an opportunity to confirm your agency's continued interest in the project; submit new information and/or identify approvals or permits that may be required.

A Public Information Centre (PIC) is planned for the fall 2017 to present the Recommended Plan for improvements, as well as the proposed construction phasing and traffic management plan. A separate External Agency and municipal session will also be scheduled to coincide with the PIC.

We respectfully request that you complete and return the enclosed form by **July 14, 2017** so that your comments may be considered early in the design process. Comments may also be submitted via the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

If you have any questions regarding the project, please contact either the undersigned or one of the individuals named in the enclosed material.



June 12, 2017

First Name Last Name, Position

Page 2 of 2

Reference: **NOTICE OF STUDY UPDATE**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00)**

Regards,

**STANTEC CONSULTING LTD.**

A handwritten signature in black ink that reads "Paula Burnard".

Paula Burnard, MScPI, MCIP, RPP

Senior Environmental Planner

Phone: 519-675-6666

Fax: 519-645-6575

paula.burnard@stantec.com

Attachment: Notice of Study Update, Comment Form

- c. S. Howard, Senior Project Manager, MTO West Region
- S. Reid, Area Manager, MTO West Region
- K. Houston, Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange & Highway Improvements**  
**from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3222-15-00)**

**COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017**

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Date: \_\_\_\_\_

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

<b>Contact Information on Study Mailing List</b> «Title» «FirstName» «LastName», «Position» «OrganizationName» «Region», «Address» «Address_2» «City» «Province» «PostalCode» Tel: «WorkPhone»	<i>If this information is incorrect please provide updated information or identify the key project contact below.</i>
---	---

**Please return the completed form to:**  
Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
paula.burnard@stantec.com

---

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:

---



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.



**From:** [Burnard, Paula](#)  
**To:** [Bergman, Stephanie](#)  
**Subject:** FW: Notification regarding construction timing  
**Date:** Friday, July 14, 2017 4:27:24 PM

---

**From:** James Etienne <etiennej@cambridge.ca>  
**Subject:** Notification regarding construction timing  
**Preference:**  
**Mailing List:**

**Message Body:**  
To whom it may concern:

Recent phases of 401 construction between King Street and Hespeler Road in conjunction with major municipal projects in Blair and Preston have resulted in significant traffic disruptions between Cambridge and Kitchener. Details of the construction phasing timing and traffic management plan should be provided to the City Engineering Division with enough lead time to allow for coordination of detour routes and/or timing of major reconstruction projects.

Sincerely,

James Etienne, P.Eng.  
City Engineer

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** Burnard, Paula  
**To:** ["Kathy Padgett"](#)  
**Subject:** RE: Highway 401 Interchange and Highway Improvements  
**Date:** Friday, July 07, 2017 10:17:00 AM

---

Hi Kathy,  
Thank you for submitting your comment form! I will make note of your request to stay informed as the project progresses.  
Regards,  
Paula

**Paula Burnard, MScPI, MCIP, RPP**  
Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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**From:** Kathy Padgett [mailto:[PadgettK@cambridge.ca](mailto:PadgettK@cambridge.ca)]  
**Sent:** Thursday, July 06, 2017 10:40 AM  
**To:** Burnard, Paula <[Paula.Burnard@stantec.com](mailto:Paula.Burnard@stantec.com)>  
**Subject:** Highway 401 Interchange and Highway Improvements

Good morning Paula,

As requested, I am returning the completed form provided to the Cambridge Environmental Advisory Committee (CEAC) regarding the Highway 401 Interchange and Highway Improvements from Hespeler to Townline Road. The Committee currently has no additional comments but are interested in staying involved as the project progresses.

Thanks,

Kathy

**Kathy Padgett, MES (PI.)**  
**Senior Planner – Environment**

City of Cambridge  
Community Development Department  
50 Dickson St, 3<sup>rd</sup> Floor  
PO Box 669  
Cambridge ON N1R 5W8  
Phone (519) 621-0740 ext. 4826  
Fax (519) 740-9545  
[PadgettK@Cambridge.ca](mailto:PadgettK@Cambridge.ca)

**From:** [Howard \(Shen-Hao\) Chang](#)  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Date:** Monday, June 19, 2017 4:36:46 PM

---

**From:** Howard (Shen-Hao) Chang <[schang@regionofwaterloo.ca](mailto:schang@regionofwaterloo.ca)>  
**Subject:**  
**Preference:** Email  
**Mailing List:** Yes, add me to the project mailing list.

Message Body:

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** [Blair Allen](mailto:Blair.Allen)  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Study Consideration to GRT Bus Operations  
**Date:** Monday, June 19, 2017 4:35:42 PM

---

From: Blair Allen <ballen@regionofwaterloo.ca>  
Subject: Study Consideration to GRT Bus Operations  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

GRT currently operates three bus routes within the study limit. During weekdays, Route 51 and 200 iXpress (to be rebranded as ION Bus) operate at 15 minute peak headway each on Hespeler Rd. Route 203 iXpress operates at 30 minute peak headway on Franklin Blvd.

Phase 1 requires temporary full closure of Franklin boulevard bridge in 2018. This would impact existing 203 iXpress bus route operating through the bridge. The route may detour via Pinebush Rd , Townline Rd and Jamieson Pkwy.

Hespeler Rd is a major transit corridor with frequent bus service. GRT Transit Development requests the transit only left turn from Hespeler towards the Smart Centre to be maintained throughout the duration of construction.

Transit Development supports the Highway 401 bypass shoulder west wide of the Hespeler intersection. This would help ION BRT buses minimize delays and maintain on-time performance during major bottlenecks.

The Region of Waterloo (incl. GRT) is working with MTO on future opportunities for transit priority measures in the study area.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** [Welker, Kevin](#)  
**To:** [Emma King](#)  
**Cc:** "[scott.howard@ontario.ca](mailto:scott.howard@ontario.ca)"; [Burnard, Paula](#)  
**Subject:** RE: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update  
**Date:** Friday, September 15, 2017 8:17:22 AM

---

Hi Emma,

Thanks for your interest in the project. We will add you to the contact list.

**Kevin Welker, P.Eng., ENV SP**

Senior Associate, Transportation  
Stantec  
600-171 Queens Avenue, London ON N6A 5J7  
Phone: (519) 675-6652  
Cell: (226) 919-5979  
[kevin.welker@stantec.com](mailto:kevin.welker@stantec.com)

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**From:** Emma King [mailto:[EKing@regionofwaterloo.ca](mailto:EKing@regionofwaterloo.ca)]  
**Sent:** Friday, September 15, 2017 8:00 AM  
**To:** Welker, Kevin <[Kevin.Welker@stantec.com](mailto:Kevin.Welker@stantec.com)>; '[scott.howard@ontario.ca](mailto:scott.howard@ontario.ca)' <[scott.howard@ontario.ca](mailto:scott.howard@ontario.ca)>  
**Subject:** Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update

Hello Scott and Kevin,

I had the notice of study forwarded to me by a co-worker. I coordinate bus detours for Grand River Transit. We have several bus routes that use both the Franklin Blvd overpass and the Hespeler Rd/Highway 24 overpass in Cambridge. I would like to be added to the mailing list for this project.

Regards,

**Emma King**

Co-Ordinator, On-Street Passenger Amenities  
Region of Waterloo, Grand River Transit (GRT)  
250 Strasburg Rd, Kitchener, ON N2E 3M6  
P: 519.585.7597 x7211 | C: 226-750-6946  
F: 519.585.7640 |email: [eking@regionofwaterloo.ca](mailto:eking@regionofwaterloo.ca)  
[www.regionofwaterloo.ca](http://www.regionofwaterloo.ca) | [www.grt.ca](http://www.grt.ca)

 [@GRT\\_ROW](#)  [GRT-Grand-River-Transit](#)

 Please consider the environment before printing this e-mail

Confidentiality Notice: This e-mail correspondence (including any attachments) may contain information which is confidential and/or exempt from disclosure under applicable law, and is intended only for the use of the

**From:** [doug.hudspeth@hydroone.com](mailto:doug.hudspeth@hydroone.com)  
**To:** [Burnard, Paula](#)  
**Cc:** [Zone8Scheduling@HydroOne.com](mailto:Zone8Scheduling@HydroOne.com); [Lana.Kegal@HydroOne.com](mailto:Lana.Kegal@HydroOne.com)  
**Subject:** RE: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update  
**Date:** Friday, July 14, 2017 8:05:45 AM

---

Hi Paula

Hydro One has Transmission Lines crossing the 401 immediately west of Hespeler Road and other Transmission Line Crossing Hespeler Road immediately south of Hwy 401. If your project is encroaching on the Hydro One right of way, please send details of your project to our Real Estate Dept for review. The Senior Real Estate Coordinator for this area is Lana Kegal. Lana's e-mail is [Lana.Kegal@HydroOne.com](mailto:Lana.Kegal@HydroOne.com).

Thanks

Doug

---

**From:** LAWRENCE Lindsay  
**Sent:** Monday, June 19, 2017 9:22 AM  
**To:** ZONE 8 SCHEDULING  
**Cc:** LAWRENCE Lindsay  
**Subject:** FW: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update

Good Morning,

Please see the attached final notice of study. The Guelph Supervisor believes this may affect transmissions.

Thank you,

**Lindsay Lawrence**

Zone Distribution Planning Tech, Western Field Business Centre  
Hydro One Networks Inc  
16 Graham St., Woodstock, ON, N4S 6J6  
Tel: 519-537-7172 x2226  
Fax: 519-537-5081  
Email: [Lindsay.Lawrence@HydroOne.com](mailto:Lindsay.Lawrence@HydroOne.com)

---

**From:** SMITH Andrew  
**Sent:** Monday, June 19, 2017 9:19 AM  
**To:** LAWRENCE Lindsay  
**Subject:** RE: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update

Lindsay:

This would not be in Zone 2 Provincial Lines, maybe a transmission TECH should look at it

Thank you

Andrew Smith  
Supervisor Distribution Technician  
Provincial Lines GU2  
Hydro One Networks Inc.  
Tel: (800) 743-6893 ext. 3229  
Cell: (519) 820-1729  
Fax: (519) 822-0760  
[andrew.smith@Hydroone.com](mailto:andrew.smith@Hydroone.com)

---

**From:** LAWRENCE Lindsay  
**Sent:** Tuesday, June 13, 2017 2:46 PM  
**To:** SMITH Andrew  
**Cc:** LAWRENCE Lindsay  
**Subject:** FW: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update

Hi Andre,

Please see the final notice of study attached, and let me know if this needs to be scheduled to an ADET.

Thanks,

**Lindsay Lawrence**

Zone Distribution Planning Tech, Western Field Business Centre  
Hydro One Networks Inc  
16 Graham St., Woodstock, ON, N4S 6J6  
Tel: 519-537-7172 x2226  
Fax: 519-537-5081  
Email: [Lindsay.Lawrence@HydroOne.com](mailto:Lindsay.Lawrence@HydroOne.com)

---

**From:** Burnard, Paula [<mailto:Paula.Burnard@stantec.com>]  
**Sent:** Tuesday, June 13, 2017 10:18 AM  
**To:** Burnard, Paula  
**Subject:** Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3222-15-00) - Notice of Study Update

Good morning,

The Ministry of Transportation, Ontario (MTO) has retained Stantec Consulting Ltd. to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The enclosed Ontario Government Notice (OGN) provides a key plan and additional details for your reference.

Please let me know if you have any questions or comments.

Thank you and regards,  
Paula

**Paula Burnard, MScPI, MCIP, RPP**  
Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7

**From:** [Slattery, Barbara \(MOECC\)](#)  
**To:** [Burnard, Paula](#)  
**Subject:** Notice of Study Update (GWP 3222-15-00)  
**Date:** Tuesday, June 20, 2017 12:16:22 PM

---

Hello Paula,

In reference to the Notice of Study Update for the Highway 401 interchange and highway improvements between Hespeler Road and Townline Road, please continue to provide me with notifications and access to an e-copy of the final document for review.

Thank you

Barb Slattery, EA/Planning Coordinator  
**Ministry of the Environment and Climate Change**  
West Central Region  
(905) 521-7864

---

**From:** srmorrell srmorrell [mailto:srmorrell@sympatico.ca]  
**Sent:** Friday, June 16, 2017 9:36 AM  
**To:** Welker, Kevin <Kevin.Welker@stantec.com>  
**Cc:** Paul Holbrook <pjholbrook@gto.net>  
**Subject:** Study update 401

Kevin,

I received your information letter regarding the 401 updates and assessments and would appreciate being on your mailing list. I am a deacon and chair of the property committee at Temple Baptist Church, 400 Holiday Inn Dr. in Cambridge. (corner of Franklin and Holiday Inn Dr.)

Thank you,

Ross Morrell

**From:** [Burnard, Paula](#)  
**To:** [Bergman, Stephanie](#)  
**Subject:** FW: Hwy 401 interchange and highway improvements Hespeler-Townline GWP 3222-15-00  
**Date:** Friday, July 14, 2017 10:45:21 AM  
**Attachments:** [Hwy 401 Hespeler-Townline interchanges GWP 3060-11-00 2014-06-27 CSU MTCS comments.pdf](#)  
[let\\_897\\_mtcs\\_20141117.pdf](#)

---

---

**From:** Muller, Joseph (MTCS) [mailto:Joseph.Muller@ontario.ca]  
**Sent:** Friday, July 14, 2017 10:19 AM  
**To:** Burnard, Paula <Paula.Burnard@stantec.com>  
**Cc:** Howard, Scott (MTO) <Scott.Howard@ontario.ca>  
**Subject:** Hwy 401 interchange and highway improvements Hespeler-Townline GWP 3222-15-00

Hello Paula Burnard:

Thank-you for your notice of study update for the above project. As per the attached prior correspondence re: the TESR, if the project footprint has not changed, MTCS has no further comments, while if the scope has expanded, further screening will be warranted. Please contact me if you have any questions or would like to further discuss the file – thank-you for your assistance,

Joe

**Joseph Muller, RPP, MCIP**

Heritage Planner

Ministry of Tourism, Culture and Sport

Culture Division | Programs and Services Branch | Heritage Program Unit

401 Bay Street, Suite 1700

Toronto, Ontario M7A 0A7

Tel. 416.314.7145 | Fax. 416.212.1802

**Ministry of Tourism,  
Culture and Sport**

Culture Services Unit  
Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON M7A 0A7  
Tel: 416 314 7145  
Fax: 416 212 1802

**Ministère du Tourisme,  
de la Culture et du Sport**

Unité des services culturels  
Direction des programmes et des services  
401, rue Bay, Bureau 1700  
Toronto ON M7A 0A7  
Tél: 416 314 7145  
Télééc: 416 212 1802



June 27, 2014 (EMAIL ONLY)

Donna Madden  
Stantec Consulting Ltd.  
200-835 Paramount Drive  
Stoney Creek, ON L8J 0B4  
E: Donna.Madden@stantec.com

**MTCS file #: 0001519**  
**Proponent: Ministry of Transportation**  
**Subject: Notice of Commencement, Highway 401 Interchange Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP 3060-11-00)**  
**Location: City of Cambridge, Ontario**

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Dear Donna Madden:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the Notice of Commencement for your project. MTCS's interest in this EA project relates to its mandate of protecting, conserving and preserving Ontario's culture heritage, which includes:

- Archaeological resources, including land-based and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources. Please advise MTCS whether an archaeological assessment and/or a heritage impact assessment will be completed for your EA project, and provide them to MTCS before issuing a Notice of Completion.

Please note that the [Standards and Guidelines for Conservation of Provincial Heritage Properties](#) (S&G), prepared pursuant to Section 25.2 of the *Ontario Heritage Act (OHA)*, came into effect on July 1, 2010. All Ontario government ministries and public bodies that are prescribed under Ontario Regulation 157/10 must comply with the S&Gs. They apply to property that is owned or controlled by the Crown in right of Ontario or by a prescribed public body.

Aboriginal communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Aboriginal communities includes a discussion about known or potential cultural heritage resources that are of value to these communities.

I understand that this project may in whole or in part fall within the scope of the Preliminary Design Study GWP 8-00-00. Please confirm if this is the case, and if so how recommendations made in the TESR were followed up for this Detail Design component of the project.

**Archaeological Resources**

Your EA project may impact archaeological resources and you may screen the project with the MTCS [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MTCS archaeological sites data are available at [archaeologicalsites@ontario.ca](mailto:archaeologicalsites@ontario.ca). If your EA project area exhibits archaeological potential, then an archaeological assessment (AA) by an *Ontario Heritage Act*

(OHA) licensed archaeologist, who is responsible for submitting the report directly to MTCS for review, is recommended.

### **Built Heritage and Cultural Heritage Landscapes**

The attached MTCS checklist *Screening for Impacts to Built Heritage and Cultural Heritage Landscapes* helps determine whether your EA project may impact cultural heritage resources. The clerks for the City of Cambridge and Region of Waterloo can provide information on property registered or designated under the *Ontario Heritage Act*.

The draft [MTO Ontario Heritage Bridge Guidelines for Provincially Owned Bridges](#) screening criteria have also been established for cultural heritage evaluation of bridges under the Class EA for Provincial Transportation Facilities.

A Cultural Heritage Evaluation Report is used to determine the cultural heritage value or interest of a potential Provincial Heritage Property. If your EA project will impact heritage resources, MTCS recommends that a Heritage Impact Assessment (HIA) be prepared by a qualified consultant. Our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. Please send HIAs to MTCS for review, and make it available to local organizations or individuals who have expressed their interest in heritage.

### **Environmental Assessment Reporting**

HIA and AA reports and their recommendations are to be addressed and incorporated into EA projects. If your screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file. MTCS is in no way liable if the information in the completed checklists is found to be inaccurate or incomplete.

Thank-you for circulating MTCS on this project: please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Joseph Muller, RPP/MCIP  
Heritage Planner  
Joseph.Muller@Ontario.ca

Copied to: Adam Barg, Consultant Project Manager, Stantec Consulting Ltd.  
Roger Ward, Senior Project Manager, Ministry of Transportation

Please notify MTCS if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out a determination of their nature and significance.

If human remains are encountered, all activities must cease immediately and the local police be contacted as well as the Cemeteries Regulation Unit of the Ministry of Consumer Services must be contacted. In situations where human remains are associated with archaeological resources, MTCS should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.



**Stantec Consulting Ltd.**  
200 - 835 Paramount Drive, Stoney Creek ON L8J 0B4

November 17, 2014  
File: 165000897  
MTCS File # 0001519

**Attention: Mr. Joseph Muller, Heritage Planner**  
Ministry of Tourism, Culture and Sport  
Culture Services Unit, Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON L8J 0B4  
E: joseph.muller@ontario.ca

Dear Mr. Muller,

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP. 3060-11-00)**

Thank you for your June 27, 2014 letter in response to the Notice of Study Commencement for the Highway 401 Interchange Improvements project. Stantec confirms that this project is the initial phase of the Preliminary Design Study (G.W.P. 8-00-00) documented in a Transportation Environmental Study Report (TESR) which received environmental clearance in November 2013. The TESR is available for review on the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

The Stage 1 archaeological assessment concluded that the Highway 401 study area (i.e., from west of Hespeler Road to the Wellington County/Halton Regional Boundary) is disturbed. A copy of the original MTC acceptance letter, dated June 2011 is attached. Stantec will undertake Stage 2 investigations during this phase of the project for any new property required for development outside the boundaries of the existing corridor. The Stage 2 report will be submitted to MTCS during this detailed design study.

The cultural heritage resource assessment carried out during preliminary design identified the following resources that may be indirectly affected by this project:

- A farmhouse and barn complex located at 2089 Townline Road, Lot 1 Concession 2, Township of Puslinch, Wellington County
- The Highway 401 corridor throughout the study area; and
- Waterloo Regional Road 24

The TESR did not recommend site specific mitigation measures because it is not anticipated that the road improvements will adversely affect these cultural heritage resources. This will be confirmed during detailed design.

A Cultural Heritage Evaluation was undertaken in 2010 for Waterloo Regional Road #24 (Hespeler Road) Underpass S.B.L., Site No. 33-150-S and Waterloo Regional Road #36 (Franklin Boulevard)



November 17, 2014  
Mr. Joseph Muller, Heritage Planner  
Page 2 of 2

**Reference: Detailed Design and Class Environmental Assessment Study  
Highway 401 Interchange Improvements from Hespeler Road to Townline Road  
City of Cambridge (Project Reference: GWP. 3060-11-00)**

Underpass, Site No. 33-151. The evaluations concluded that the structures are not worthy of consideration for listing on the Ontario Heritage Bridge List. The Cultural Heritage Evaluation Report (CHER) serves as the documentation record of their evaluation. The MTO Heritage Bridge Committee Forms are attached for your reference.

I trust this information is helpful. We will continue to circulate MTCS on this project through the EA process. A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The notice outlining the DCR viewing locations will be published in local papers closer to that time.

Thank you for taking time to respond to our request for comments. We encourage you to visit the project website at [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca) for updates as this project progresses.

Regards,

**STANTEC CONSULTING LTD.**

*Donna M. Madden*

Donna M. Madden, CPT  
Senior Environmental Planner  
Phone: 905-381-3217  
Fax: 905-385-3534  
donna.madden@stantec.com

Attachment: MTCS Acceptance Letter  
MTO Heritage Bridge Committee Forms

- c. R. Ward, Senior Project Manager, MTO West Region
- A. Mochrie, Senior Environmental Planner, MTO West Region
- A. Barg, Project Manager, Stantec Consulting Ltd.

Ministry of Tourism and Culture  
Culture Programs Unit  
Programs & Services Br.  
900 Highbury Avenue  
London, ON N5Y 1A4  
Tel: 519-675-6898  
Fax: 519-675-7777  
e-mail: [shari.prowse@ontario.ca](mailto:shari.prowse@ontario.ca)

October 3, 2011

Ministre du Tourisme et de la Culture  
Unité des programmes culturels  
Direction des programmes et des services  
900, av. Highbury  
London, ON N5Y 1A4  
Tél: 519-675-6898  
Télé: 519-675-7777  
e-mail: [shari.prowse@ontario.ca](mailto:shari.prowse@ontario.ca)



Mr. Philip Woodley  
New Directions Archaeology Ltd.  
55 Valmont Street  
Ancaster, Ontario, L9G 4Z4

**RE: Review and Acceptance into the Provincial Register of Reports: Archaeological Assessment Report Entitled, "Stage 1 Archaeological Assessment of Highway 401, From West of Hespeler Road to the Halton Region Boundary" January 2011, Received March 30, 2011, Licence/PIF # P018-272-2009, G.W.P 8-00-00, MTC File 401H114**

Dear Mr. Woodley:

This office has reviewed the above-mentioned report which has been submitted to this Ministry as a condition of licensing in accordance with Part VI of the Ontario Heritage Act, R.S.O. 1990, c 0.18. This review is to ensure that the licensed professional consultant archaeologist has met the terms and conditions of their archaeological licence, that archaeological sites have been identified and documented according to the 1993 technical guidelines set by the Ministry and that the archaeological fieldwork and report recommendations ensure the conservation, protection and preservation of the cultural heritage of Ontario.\*

As the result of our review, this Ministry will accept the above titled report into the Ontario Public Register of Archaeological Reports. The report indicates that portions of the subject property as detailed in Figures 5-8 have archaeological potential and, consequently, should be subject to a Stage 2 archaeological assessment. This Ministry concurs with this recommendation.

I trust this information is of assistance. Should you require any further information regarding this matter, please feel free to contact me.

Sincerely,

A handwritten signature in black ink, appearing to read "Shari Prowse".

Shari Prowse  
Archaeology Review Office

cc. Archaeology Licence Office  
Ecoplans Ltd.

\* In no way will the Ministry be liable for any harm, damages, costs, expenses, losses, claims or actions that may result: (a) if the Report(s) or its recommendations are discovered to be inaccurate, incomplete, misleading or fraudulent; or (b) from the issuance of this letter. Further measures may need to be taken in the event that additional artifacts or archaeological sites are identified or the Report(s) is otherwise found to be inaccurate, incomplete, misleading or fraudulent.



MTO Heritage Bridge Committee  
Decision Form

<b>Structure Name: Waterloo RR #24 (SBL) Underpass</b>	
<b>Structure ID: 33-150S</b>	<b>Highway: 401</b>
<b>Structure Type: Continuous Deck Slab (Rectangular Voids)</b>	<b>Year of Construction: 1960</b>
<b>Municipality: Puslinch, Wellington County</b>	<b>CHER Date: Feb 2010</b>
<b>CHER Author: Unterman McPhail</b>	
<b>CHER Recommendation:</b> Is not an Ontario Heritage Bridge List candidate	
<b>The Heritage Bridge Committee (HBC) decided that this structure:</b> Is not an Ontario Heritage Bridge List candidate	
<b>Rationale for Decision:</b>	
<b>(1) Design/Physical Value:</b>	
<b>(2) Contextual Value:</b>	
<b>(3) Historic/ Associative Value:</b>	
<b>Final Statements:</b> The HBC is in general agreement with the evaluation and the conclusion that the bridge does not have Provincial Heritage Significance.	



MTO Heritage Bridge Committee  
Decision Form

**HBC Scoring Matrix**

Category	Criterion	Maximum Score	CHER Score
<b>Design / Physical Value</b>	Functional Design	20	20
	Visual Appeal	20	4
	Materials	10	0
<b>Contextual Value</b>	Landmark	15	9
	Character Contribution	10	6
<b>Historical/ Associative Value</b>	Designer / Construction Firm	15	9
	Association with a historical theme, person or event	10	6
<b>Total Score</b>		<b>100</b>	<b>54</b>

**Heritage Bridge Committee Members:**

Chair  
Dino Bagnariol, P.Eng.  
Head Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
St. Catharines ON  
(905) 704-2399

Walter Kenedi, P.Eng.  
Sr. Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
St. Catharines ON  
(905) 704-2399

Leslie Currie  
Regional Archaeologist  
Provincial Highways Management  
Central Region Planning & Environmental  
1201 Wilson Ave. Building D, 3<sup>rd</sup> Floor  
Downsview ON  
(416) 235-5541

Martin Haalstra, P.Eng.  
Sr. Structural Engineer  
Provincial Highways Management  
West Region Engineering  
659 Exeter Rd., 3rd Floor  
London ON  
(519) 873-4348

<b>Structure Name: Waterloo RR #36 Underpass</b>	
<b>Structure ID: 33-151</b>	<b>Highway: 401</b>
<b>Structure Type: Continuous Deck Slab (Rectangular Voids)</b>	<b>Year of Construction: 1960</b>
<b>Municipality: Puslinch, Wellington County</b>	<b>CHER Date: Feb 2010</b>
<b>CHER Author: Unterman McPhail</b>	
<b>CHER Recommendation:</b> Is not an Ontario Heritage Bridge List candidate	
<b>The Heritage Bridge Committee (HBC) decided that this structure:</b> Is not an Ontario Heritage Bridge List candidate	
<b>Rationale for Decision:</b>	
<b>(1) Design/Physical Value:</b>	
<b>(2) Contextual Value:</b>	
<b>(3) Historic/ Associative Value:</b>	
<b>Final Statements:</b> The HBC is in general agreement with the evaluation and the conclusion that the bridge does not have Provincial Heritage Significance.	



MTO Heritage Bridge Committee  
Decision Form

**HBC Scoring Matrix:**

Category	Criterion	Maximum Score	CHER Score
Design / Physical Value	Functional Design	20	16
	Visual Appeal	20	4
	Materials	10	0
Contextual Value	Landmark	15	3
	Character Contribution	10	6
Historical/ Associative Value	Designer / Construction Firm	15	9
	Association with a historical theme, person or event	10	0
<b>Total Score</b>		<b>100</b>	<b>38</b>

**Heritage Bridge Committee Members:**

Chair  
Dino Bagnariol, P.Eng.  
Head Evaluation & Inspection Engineer  
Provincial Highways Management  
Highway Standards, Bridge Office  
301 St. Paul Street,  
Garden City Tower, 2nd Floor  
St. Catharines ON  
(905) 704-2399

Walter Kenedi, P.Eng.  
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Martin Haalstra, P.Eng.  
Sr. Structural Engineer  
Provincial Highways Management  
West Region Engineering  
659 Exeter Rd., 3rd Floor  
London ON  
(519) 873-4348

**From:** [Michelle Pinto](#)  
**To:** [Burnard, Paula](#)  
**Cc:** [Geoffrey Keyworth](#)  
**Subject:** Highway 401 Interchange & Highway Improvements (Hespeler Road - Townline Road) - GWP 3222-15-00 - Region of Waterloo Comments  
**Date:** Friday, July 14, 2017 5:27:12 PM

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Good Afternoon Paula:

This is to inform you that the Region of Waterloo has received the Notice of Study Update for the Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road in the City of Cambridge (GWP 3222-15-00). Region staff will be submitting comments as early as possible, next week.

Sorry for the delay, and thank you for the opportunity to comment on this project.

Best Regards,

**Michelle Pinto, E.I.T.**

**Strategic Transportation Planning | Transportation and Environmental Services Department  
Region of Waterloo**

150 Frederick St., 6<sup>th</sup> Floor | Kitchener, ON, Canada | N2G 4J3

Phone: (519) 575-4527 | Fax: (519) 575-4449

[MiPinto@regionofwaterloo.ca](mailto:MiPinto@regionofwaterloo.ca)

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Land owner  
**Date:** Monday, August 21, 2017 10:04:34 PM

---

From: Land owner  
Subject: Email  
Preference:  
Mailing List: Yes, add me to the project mailing list.

Message Body:

Hi there. I own a property on Townline Road and I have a question regards to Future Rod project would you please call me back at thank you

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Frankin Blvd/HWY 401 Interchange  
**Date:** Monday, June 19, 2017 11:39:03 AM

---

From:  
Subject: Frankin Blvd/HWY 401 Interchange  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

May I propose a weaving analysis be conducted, if not already done so, to determine the effect of an on ramp from NB Franklin Blvd to EB 401? Such an on ramp would greatly reduce the traffic flow on Hespeler Road NB as the only other access to EB 401 is at Townline Road.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** [Welker, Kevin](#)  
**To:** [Burnard, Paula](#); [Emery, Dave \(Hamilton\)](#)  
**Subject:** FW: Study update - 401 from Hespeler to Townline  
**Date:** Monday, June 19, 2017 4:12:04 PM  
**Attachments:** [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)

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**Kevin Welker, P.Eng., ENV SP**

Senior Associate, Transportation  
Stantec  
600-171 Queens Avenue, London ON N6A 5J7  
Phone: (519) 675-6652  
Cell: (226) 919-5979  
Fax: (519) 645-6575  
[kevin.welker@stantec.com](mailto:kevin.welker@stantec.com)

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**From:**  
**Sent:** Tuesday, June 13, 2017 11:01 AM  
**To:** [scott.howard@ontario.ca](mailto:scott.howard@ontario.ca)  
**Cc:** Welker, Kevin <[Kevin.Welker@stantec.com](mailto:Kevin.Welker@stantec.com)>  
**Subject:** Study update - 401 from Hespeler to Townline

Gentlemen, the notice I received today is fairly vague but when I read "some work at the interchange" in regards to the Franklin Blvd section, I am left wondering if the Franklin interchange is going to be expanded into a full interchange. If not, then I think that the entire MTO staff should be replaced with people that have some understanding of traffic flow. If that comment seems harsh, I can share with you the story of the 407 being built and MTO thinking that making that road 4 lanes from a 6 lane 401 over to the 410 was acceptable. I have spent my entire adult life driving around the GTA area to see clients and it is a joke how badly planned our road systems are. But getting back to Franklin, you have a core road cutting through an industrial area in Cambridge where you force trucks to make multiple turns and use 2 lane roads just to get to a ramp to the 401, where most of them want to go. This is simply poor planning. You have a chance to make this right with the 10 lanes you are switching to.

And yes, I'd be happy to drop whatever I am doing and come and meet with anyone who will listen to what a customer (taxpayer) has to say about this.



The information contained in this message is for the exclusive use of the intended recipient(s) and may be confidential, proprietary, and/or legally privileged. If you receive this message or any attachments in error, you are hereby notified that any review, dissemination,

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange & Highway Improvements**  
**from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3222-15-00)**

**COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017**

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Date: July 6/17

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Ms. Kathy Padgett, Senior Environmental Planner  
Cambridge Environmental Advisory Committee  
50 Dickson Street PO Box 669  
Cambridge ON N1R 5W8  
Tel: (519) 621-0740 Ext. 4826

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange & Highway Improvements  
from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3222-15-00)

COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017

Date: July 14/17

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

Please direct all engineering related inquiries to the key contact below, who will coordinate City comments.

- Michael McGann & Shannon Noonan can be removed from the contact list
- Kent McVittie has retired & can also be removed

Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Ms. Sarah Austin, P. Eng, Manager of Development Engineering  
City of Cambridge  
50 Dickson Street, PO Box 669  
Cambridge Ontario N1R 5W8  
Tel:

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

**Key Project Contact:**

Sarah Austin

Job Title: Manager of Development Engineering

Name of Group/Agency: City of Cambridge

Mailing Address: 50 Dickson PO Box 669 Cambridge, N1R 5W8

Tel: 519 621-0740 ext. 4552 Fax: 519 740-7729 E-mail: austins@cambridge.ca



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange & Highway Improvements**  
**from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3222-15-00)**

**COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017**

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Date: June 19, 2017

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

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Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Mr. Don Creed, Director of Public Works and Parks  
Township of Puslinch  
7404 Wellington Road 34  
Puslinch ON NOB 2J0  
Tel: (519) 763-1470

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
paula.burnard@stantec.com

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange & Highway Improvements  
from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3222-15-00)

COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017

Date: June 19 2017

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

*[Handwritten signature]*

Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Mr. Fred Natolochny, Supervisor of Resource Planning  
Grand River Conservation Authority  
400 Clyde Road PO Box 729  
Cambridge ON N1R 5W6  
Tel: (519) 621-2761 ext. 2229

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
paula.burnard@stantec.com

Key Project Contact:

SAME AS ABOVE

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange & Highway Improvements  
from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3222-15-00)

COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017

Date: JUNE 15, 2017

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.

THE CLOSURE OF FRANKLIN BRIDGE IS PROBLEMATIC  
WE NEED TO UNDERSTAND THE TIMING SO WE  
CAN CHANGE SCHOOL BUS ROUTINES.

THIS WILL HAVE SIGNIFICANT FINANCIAL IMPACT  
IS THERE SUPPORT AVAILABLE?

Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Mr. Benoit Bourgault, General Manager  
Student Transportation Services of Waterloo Region  
130 – 4275 King Street East  
Kitchener ON N2P 2E9  
Tel: (519) 650-4934 ext. 222

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
paula.burnard@stantec.com

**Key Project Contact:**

Job Title:

Name of Group/Agency:

Mailing Address:

Tel:

Fax:

E-mail:



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.

DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY  
Highway 401 Interchange & Highway Improvements  
from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3222-15-00)

COMMENT FORM - PLEASE RETURN BY FRIDAY, JULY 14, 2017

WE ALSO NEED TO UNDERSTAND  
THE TRAFFIC IMPACT OF THE BRIDGE  
REPLACEMENT @ HESPELER ROAD AS IT WILL  
IMPACT STUDENT TRANSPORTATION  
CONSIDERABLY.

MAJOR CLOSURE SHOULD START  
AFTER THE END OF THE SCHOOL YEAR  
SO TRAFFIC DIVERSION START AT  
LOW VOLUME WHICH HELP  
MINIMIZE THE IMPACT ON OUR  
SERVICES

THANK YOU FOR CONSIDERING OUR NEEDS



**DETAILED DESIGN AND CLASS ENVIRONMENTAL ASSESSMENT STUDY**  
**Highway 401 Interchange & Highway Improvements**  
**from Hespeler Road to Townline Road,**  
**City of Cambridge (GWP 3222-15-00)**

**COMMENT FORM – PLEASE RETURN BY FRIDAY, JULY 14, 2017**

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**Date:** \_\_\_\_\_

- Please remove this agency from the study mailing list.
- Comments have not changed since Preliminary Design.
- Please consider the following input during the Detailed Design Study.
- 
- 
- 
- 
- 
- 
- 
- 

Comments can be submitted via the project website:  
[www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

**Contact Information on Study Mailing List**

Mr. Burkhard Mausberg, CEO  
Friends of the Greenbelt Foundation  
661 Yonge Street Suite 500  
Toronto ON M4Y 1Z9  
Tel: (416) 960-0001

*If this information is incorrect please provide updated information or identify the key project contact below.*

**Please return the completed form to:**

Paula Burnard, Senior Environmental Planner  
600-171 Queens Avenue, London ON N6A 5J7  
Tel: (519) 675-6666 Fax: (519) 645-6575  
paula.burnard@stantec.com

---

**Key Project Contact:**

Job Title: \_\_\_\_\_

Name of Group/Agency: \_\_\_\_\_

Mailing Address: \_\_\_\_\_

Tel: \_\_\_\_\_

Fax: \_\_\_\_\_

E-mail: \_\_\_\_\_

---



Comments and information regarding this project are being collected to assist the Ministry of Transportation in meeting the requirements of the *Environmental Assessment Act*. This material will be maintained on file for use during the project and may be included in project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*.



TRANSPORTATION AND ENVIRONMENTAL SERVICES  
Transportation  
150 Frederick Street  
Kitchener Ontario N2G 4J3 Canada  
Telephone: 519-575-4400  
TTY: 519-575-4608  
Fax: 519-575-4453  
[www.regionofwaterloo.ca](http://www.regionofwaterloo.ca)

**Refer To:**

Michelle Pinto, E.I.T.  
519-575-4527

**File Number:**

File #T04-40

**Date:**

July 19, 2017.

Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON, N6A 5J7

Scott Howard  
Senior Project Manager  
Ministry of Transportation.  
659 Exeter Road, 3<sup>rd</sup> Floor  
London, ON, N6E 1L3

Paula Burnard  
Senior Environmental Planner  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON, N6A 5J7

Dear Kevin, Scott and Paula:

**Re: Notice of Study Update – Highway 401 Interchange and Highway Improvements from Hespeler Road to Townline Road, City of Cambridge (GWP-3222-15-00)**

Thank you for providing the Regional Municipality of Waterloo with the Notice of Study Update, for the Detailed Design and Class Environmental Assessment (Class EA) study, for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge. Region staff appreciates the opportunity to provide input on this study. Staff offers the following comments on this Class EA study, relating to active transportation improvements and the Park and Ride lot on Townline Road.

The provision for pedestrian/cyclist travel across Highway 401 on Franklin Boulevard and Hespeler Road aligns with the Region of Waterloo's and City of Cambridge's active transportation goals, to provide high quality active transportation links over Highway 401. The Region of Waterloo, in co-ordination with the Ministry of Transportation and



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Fax: 519-575-4453  
[www.regionofwaterloo.ca](http://www.regionofwaterloo.ca)

City of Cambridge, prepared feasibility and preliminary design studies to determine preferred alternatives for pedestrian and cyclist facilities across Highway 401 at the Hespeler Road and Franklin Boulevard interchanges. Below are details about the preferred active transportation facilities across these interchanges, for the Ministry's consideration in the detailed design stage of this EA. The intention is to work with the Ministry to incorporate these proposed active transportation facilities into the detailed design being presented at the Public Information Centre (PIC) this fall.

### **Hespeler Road**

The Preferred Alternative identified in the 2015 Feasibility Study for active transportation improvements on the Hespeler Road bridge across Highway 401 includes the following:

- 4.0 metre boulevard multi-use trail (MUT) on the east side of the bridge, including a minimum 1.0 metre high barrier between the MUT and the motor vehicle traffic lanes along the bridge,
- 3.5 metre wide MUT along the east boulevard bridge approaches, and a 2.0metre wide splash strip or buffer, and
- Four at-grade crossing points.

### **Franklin Boulevard**

The Preferred Alternative identified in the April 2014 Feasibility Study for active transportation improvements on the Franklin Boulevard bridge across Highway 401 includes the following:

- 4.0 metre boulevard MUT on the east side of the bridge over Highway 401, and on the north and south bridge approaches, including a barrier between the MUT and the motor vehicle traffic lanes, and
- 3.0 metre multi-use trail on the east side of Franklin Boulevard between Pinebush Road and Holiday Inn Drive/Jamieson Parkway.

Based on previous discussions, the Ministry has committed to replacing the existing active transportation infrastructure (i.e. the existing sidewalks) across these interchanges. Additional active transportation improvements beyond what currently exists is to be cost-shared with the Region and the City of Cambridge.



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Fax: 519-575-4453  
[www.regionofwaterloo.ca](http://www.regionofwaterloo.ca)

### **Park and Ride Lot (Lake Road)**

The existing commuter parking lot at the Townline Road interchange is accessed from Lake Road. As part of this Class EA study, the Region is proposing further investigation of an access to this commuter parking lot from Townline Road, to provide a more direct connection to the lot, and increase the potential for usage. Relocation of the access would also provide better access for local transit and inter-city buses. MTO should also consider discussing with operators, such as GO Transit (Metrolinx) and Greyhound, how the commuter parking lot could potentially be modified to accommodate inter-city buses to better take advantage of the planned investment in HOV lanes along the highway.

### **Comments from Transit Services (Grand River Transit)**

The following comments from GRT Transit Development were also provided, via the online contact form on the project website:

- GRT currently operates three bus routes within the study limit. During weekdays, Routes 51 and 200 iXpress (to be rebranded as ION Bus) operate at 15 minute peak headways each on Hespeler Road. Route 203 iXpress operates at 30 minute peak headways on Franklin Boulevard.
- Phase 1 requires temporary full closure of the Franklin Boulevard bridge in 2018. This would impact the existing 203 iXpress bus route operating through the bridge. The route may detour via Pinebush Road , Townline Road and Jamieson Parkway.
- Hespeler Road is a major transit corridor with frequent bus service. GRT Transit Development requests the transit only left turn lane from Hespeler Road towards the Smart Centre be maintained throughout the duration of construction.
- Transit Development supports the Highway 401 bypass shoulder west of the Hespeler Road intersection. This would help ION BRT buses minimize delays and maintain on-time performance during major bottlenecks.

Thank you once again for the opportunity to comment on this project. Please contact the undersigned if you have questions or would like to discuss the comments in this memo. Please also keep us updated on the schedule for the PIC in the fall, and coordinating next steps.



TRANSPORTATION AND  
ENVIRONMENTAL SERVICES  
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TTY: 519-575-4608  
Fax: 519-575-4453  
[www.regionofwaterloo.ca](http://www.regionofwaterloo.ca)

Yours truly,

*Michelle Pinto*

Michelle Pinto  
Engineer-in-Training, Transportation Planning, Region of Waterloo  
150 Frederick Street, 7<sup>th</sup> Floor  
Kitchener ON Canada N2G 4J3  
519-575-4527  
[mipinto@regionofwaterloo.ca](mailto:mipinto@regionofwaterloo.ca)

/MP

cc:

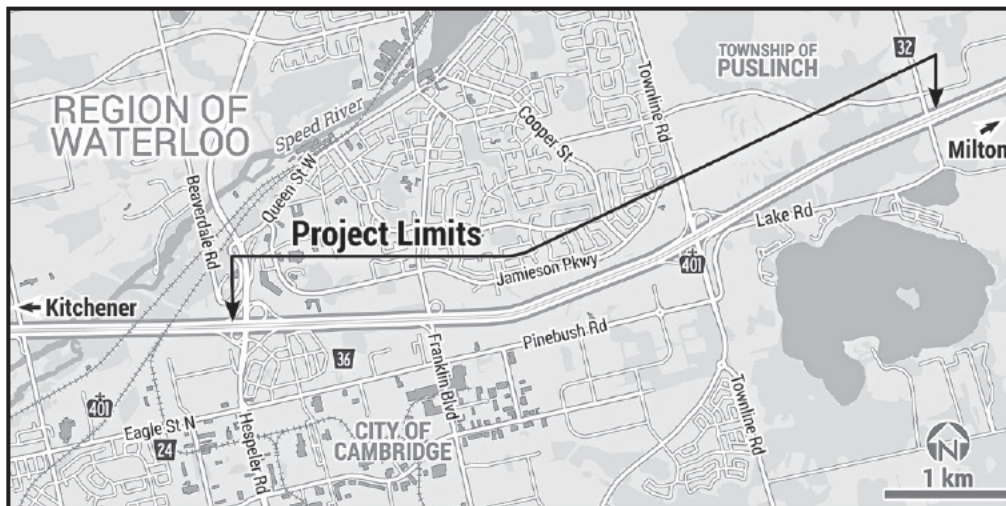
Geoffrey Keyworth, Acting Manager, Transportation Planning, Region of Waterloo  
Blair Allen, Supervisor, Transit Development, Region of Waterloo  
Howard Chang, Planner, Transit Development, Region of Waterloo

# NOTICE OF SUBMISSION - DESIGN AND CONSTRUCTION REPORT

## Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge - Phase II (GWP 3222-15-00) Detailed Design and Class Environmental Assessment Study

### THE STUDY

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo as shown on the key plan below. Since the issuance of the Notice of Study Commencement in May 2014 (formerly GWP 3060-11-00), the project has been divided into two phases to support construction staging. Phase I construction, GWP 3125-07-00, was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard.



Phase II, GWP 3222-15-00, includes:

- Widening of Highway 401 to ten lanes, including Managed Lanes (previously named High Occupancy Vehicle lanes)
- Reconstruction of the Highway 401 interchange at Hespeler Road
- Replacement of the Hespeler Road bridge (with provision for pedestrian/cyclist travel across Highway 401, in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge)
- Reconstruction of ramp tie-ins at Franklin Boulevard and Townline Road interchanges
- Retaining walls as needed
- Expansion of the commuter parking lot at the Townline Road interchange
- Drainage improvements, illumination, overhead signs and traffic signals
- A section of new noise wall constructed along the south side of Highway 401, west of Franklin Boulevard
- Rehabilitation of the Wellington Road 32 Underpass

Phase II construction will start in the fall 2019 to 2022, subject to completion of the Study, funding and approvals.

### PROCESS

This project was approved in November 2013 following the preparation and filing of a Transportation Environmental Study Report (TESR) in November 2012 (GWP 8-00-00).

This Phase II Design and Construction Report was prepared under the Ministry of Transportation's *Class Environmental Assessment (EA) for Provincial Transportation Facilities, 2000* as a Group 'B' project. The Phase II DCR is available for a 30-day review period from **Tuesday, March 12, 2019 to Wednesday, April 10, 2019**, during regular business hours only, at the following three review locations, and on the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)):

**City of Cambridge City Hall**  
Clerk's Office, 2nd Floor  
50 Dickson Street, Cambridge, ON

**Ontario Ministry of Transportation**  
West Region, Front Lobby  
659 Exeter Road, London, ON

**Cambridge Public Library**  
Idea Exchange - Hespeler Branch  
5 Tannery Street East, Cambridge, ON

The Notice of Submission of Design and Construction Report documenting Phase I of the project (GWP 3125-07-00) was issued in November 2017.

### COMMENTS

Comments during the review period may be submitted via the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)) or by contacting one of the following individuals:

**Mr. Kevin Welker, P.Eng., ENV SP**  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue, London, ON N6A 5J7  
tel: 519-675-6652 (+0 collect)  
e-mail: kevin.welker@stantec.com

**Mr. Scott Howard**  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3rd Floor, London, ON N6E 1L3  
tel: 1-800-265-6072, ext. 519-873-4568  
e-mail: scott.howard@ontario.ca

If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**Ministry of Transportation**

Engineering Office  
Environmental Section  
West Region  
659 Exeter Road  
London Ontario N6E 1L3  
Telephone: (519) 873-4560  
Facsimile: (519) 873-4600  
Email: cathy.giesbrecht@ontario.ca

**Ministère des Transports**

Bureau du génie  
Section de l'environnement  
Région de l'Ouest  
659, rue Exeter  
London (Ontario) N6E 1L3  
Téléphone: (519) 873-4560  
Télécopieur: (519) 873-4600



February 26, 2019

Chief Stacey LaForme  
Mississaugas of the Credit First Nation  
2789 Mississauga Road, RR 6  
Hagersville, ON  
N0A 1H0

Dear Chief LaForme,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road  
to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The Ontario Ministry of Transportation (MTO) has retained Stantec Consulting Ltd. to complete the Detailed Design and Class Environmental Assessment (Class EA) study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The enclosed Notice provides a key plan and additional details for your reference.

The Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study was documented in a Transportation Environmental Study Report (TESR) which received Environmental Clearance in November 2013.

As outlined in our letter dated November 29, 2017, the project has been divided into two phases to support construction phasing. Phase I construction was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard.

The purpose of this letter is to advise you of an opportunity to review and comment on the Phase II improvements, as well as the proposed construction staging and traffic management plans, related environmental impacts and proposed mitigation measures. The **Design and Construction Report (DCR) documenting Phase II (GWP 3222-15-00)** has been prepared under the Ministry of Transportation's *Class Environmental Assessment for Provincial Transportation Facilities, 2000*. This Phase II DCR is available for a 30-day review period from **Tuesday, March 12, 2019 to Wednesday, April 10, 2019**, during regular business hours only, at the locations outlined in the attached Notice, and on the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)).

Should you have any questions, please contact me at 519-873-4560, or by email at [cathy.giesbrecht@ontario.ca](mailto:cathy.giesbrecht@ontario.ca). Alternatively, questions or comments regarding the study can be submitted via the project website. If you would prefer a meeting with ministry staff, please contact me at your earliest convenience.

Yours truly,

A handwritten signature in black ink, appearing to read 'Cathy Giesbrecht', with a long horizontal stroke extending to the right.

Cathy Giesbrecht  
Head, Environmental Section

- c. F.D. Sault, Consultation Manager, Mississaugas of the Credit First Nation
- S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- R. Wallis, Indigenous Liaison, MTO West Region
- P. Burnard, Consultant Environmental Planner, Stantec Consulting Ltd.

**Ministry of Transportation**

Engineering Office  
Environmental Section  
West Region  
659 Exeter Road  
London Ontario N6E 1L3  
Telephone: (519) 873-4560  
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**Ministère des Transports**

Bureau du génie  
Section de l'environnement  
Région de l'Ouest  
659, rue Exeter  
London (Ontario) N6E 1L3  
Téléphone: (519) 873-4560  
Télécopieur: (519) 873-4600



February 26, 2019

Hohahes Leroy Hill  
Secretary of Haudenosaunee Confederacy  
Chiefs Council  
16 Sunrise Court, Suite 407  
P.O. Box 714  
Ohsweken, ON, N0A 1M0

*sent by mail and by fax to 519-445-2389*

Dear Hohahes Leroy Hill,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road  
to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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Yours truly,

A handwritten signature in black ink, appearing to read 'Cathy Giesbrecht', with a long horizontal line extending to the right.

Cathy Giesbrecht  
Head, Environmental Section

- c. H. Hill, Haudenosaunee Development Institute, Haudenosaunee Confederacy Chiefs Council
- S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- R. Wallis, Indigenous Liaison, MTO West Region
- P. Burnard, Consultant Environmental Planner, Stantec Consulting Ltd.

Ministry of Transportation

Engineering Office  
Environmental Section  
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Ministère des Transports

Bureau du génie  
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659, rue Exeter  
London (Ontario) N6E 1L3  
Téléphone: (519) 873-4560  
Télécopieur: (519) 873-4600



February 26, 2019

Chief Ava Hill  
Six Nations Chief and Council  
1695 Chiefswood Road  
P.O. Box 5000  
Oshweken, ON N0A 1M0

Dear Chief Hill,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road  
to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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Yours truly,

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Cathy Giesbrecht  
Head, Environmental Section

- c. P. General, Six Nations Wildlife Management, Six Nations of the Grand River
- J. Thomas, Consultation Supervisor, Six Nations of the Grand River
- D. Laforme, Six Nations of the Grand River
- S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- R. Wallis, Indigenous Liaison, MTO West Region
- P. Burnard, Consultant Environmental Planner, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

February 26, 2019  
File: 165000897

Dear Sir/Madam,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline  
Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The Ontario Ministry of Transportation (MTO) in partnership with Stantec Consulting Ltd. is undertaking the Detailed Design and Class Environmental Assessment study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The project includes the expansion of Highway 401 to ten lanes, including Managed Lanes (previously known as High Occupancy Vehicle lanes). The project has been divided into two phases to support construction staging. Phase I construction, GWP 3125-07-00, was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard. Phase II of the project is tentatively scheduled for construction in fall 2019, subject to completion of the study, funding and approvals.

The *Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR) in 2012, and received Environmental Assessment clearance in November 2013.

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Clerk's Office, 2<sup>nd</sup> Floor  
50 Dickson Street  
Cambridge, ON

**Ministry of Transportation**  
West Region, Front Lobby  
659 Exeter Road  
London, ON

**Cambridge Public Library**  
Idea Exchange - Hespeler Branch  
5 Tannery Street East  
Cambridge, ON



February 26, 2019

Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**  
**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The enclosed *Ontario Government Notice* (OGN) provides additional details for your reference. Current information is available on the project website, [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).

Regards,

**STANTEC CONSULTING LTD.**

A handwritten signature in black ink that reads "Paula Burnard".

Paula Burnard, MScPI, MCIP, RPP  
Senior Environmental Planner  
Phone: 519-675-6666  
Fax: 519-645-6575  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

Attachment: Ontario Government Notice of Submission of Design and Construction Report

- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

February 22, 2019  
File: 165000897

**Attention: Hon. Michael Chong, MP – Wellington-Halton Hills**  
Constituency Office  
A – 190 Saint David Street South  
Fergus ON N1M 2L3

Dear Hon. Chong,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to  
Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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659 Exeter Road  
London, ON

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Cambridge, ON

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February 22, 2019  
Hon. Michael Chong, MP- Wellington-Halton Hills  
Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The OGN will be published in the Cambridge Times on Tuesday, March 5, 2019, the Turtle Island News and the Two Row Times on Wednesday, March 6, 2019, and the Waterloo Regional Record on Saturday, March 9, 2019.

Yours truly,

**STANTEC CONSULTING LTD.**

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

Encl: Ontario Government Notice of Submission of Design and Construction Report

- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

February 22, 2019  
File: 165000897

**Attention: Bryan May, MP – Cambridge**  
Constituency Office  
Suite A4, 534 Hespeler Road  
Cambridge ON N1R 6J7

Dear MP May,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to  
Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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Cambridge, ON

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659 Exeter Road  
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5 Tannery Street East  
Cambridge, ON

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February 22, 2019  
Bryan May, MP – Cambridge  
Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**

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Yours truly,

**STANTEC CONSULTING LTD.**

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Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

Encl: Ontario Government Notice of Submission of Design and Construction Report

- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

February 22, 2019  
File: 165000897

**Attention: Marwan Tabbara, MP – Kitchener South-Hespeler**  
Constituency Office  
Suite 2A - 153 Country Hill Drive  
Kitchener ON N2E 2G7

Dear MP Tabbara,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
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Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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February 22, 2019  
Marwan Tabbara, MP – Kitchener South-Hespeler  
Page 2 of 2

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Yours truly,

**STANTEC CONSULTING LTD.**

A handwritten signature in black ink that reads "Paula Burnard".

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

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- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.



Stantec Consulting Ltd.  
600-171 Queens Avenue, London ON N6A 5J7

February 22, 2019  
File: 165000897

**Attention: Hon. Ted Arnott, MPP – Wellington-Halton Hills**  
Constituency Office  
2<sup>nd</sup> Floor, 181 St. Andrew Street East  
Fergus ON N1M 1P9

Dear Hon. Arnott,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to  
Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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February 22, 2019  
Hon. Ted Arnott, MPP – Wellington-Halton Hills  
Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

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Yours truly,

**STANTEC CONSULTING LTD.**

A handwritten signature in black ink that reads "Paula Burnard".

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

Encl: Ontario Government Notice of Submission of Design and Construction Report

- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.

February 22, 2019  
File: 165000897

**Attention: Amy Fee, MPP – Kitchener South-Hespeler**  
Constituency Office  
Unit 4 – 4281 King St. E  
Kitchener ON N2P 2E9

Dear MPP Fee,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to  
Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The Ontario Ministry of Transportation (MTO) in partnership with Stantec Consulting Ltd. is undertaking the Detailed Design and Class Environmental Assessment study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The project includes the expansion of Highway 401 to ten lanes, including Managed Lanes (previously known as High Occupancy Vehicle lanes). The project has been divided into two phases to support construction staging. Phase I construction, GWP 3125-07-00, was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard. Phase II of the project is tentatively scheduled for construction in fall 2019, subject to completion of the study, funding and approvals.

*The Preliminary Design and Class EA Study for Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study* was documented in a Transportation Environmental Study Report (TESR) in 2012, and received Environmental Assessment clearance in November 2013.

The purpose of this letter is to inform you that the **Design and Construction Report (DCR) documenting Phase II (GWP 3222-15-00)** has been prepared under the Ministry of Transportation's *Class Environmental Assessment for Provincial Transportation Facilities, 2000*. This Phase II DCR is available for a 30-day review period from **Tuesday, March 12, 2019 to Wednesday, April 10, 2019**, during regular business hours only, at the following three review locations, and on the project website ([www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)):

**City of Cambridge City Hall**  
Clerk's Office, 2<sup>nd</sup> Floor  
50 Dickson Street  
Cambridge, ON

**Ministry of Transportation**  
West Region, Front Lobby  
659 Exeter Road  
London, ON

**Cambridge Public Library**  
Idea Exchange - Hespeler Branch  
5 Tannery Street East  
Cambridge, ON

The enclosed *Ontario Government Notice (OGN)* provides additional details for your reference. Current information is available on the project website, [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca).



February 22, 2019  
Amy Fee, MPP – Kitchener South-Hespeler  
Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of  
Cambridge – Phase II (GWP 3222-15-00)**

The OGN will be published in the Cambridge Times on Tuesday, March 5, 2019, the Turtle Island News and the Two Row Times on Wednesday, March 6, 2019, and the Waterloo Regional Record on Saturday, March 9, 2019.

Yours truly,

**STANTEC CONSULTING LTD.**

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

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- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.

February 22, 2019  
File: 165000897

**Attention: Belinda Karahalios, MPP – Cambridge**  
Constituency Office  
498 Eagle Street North  
Cambridge ON N3H 1C2

Dear MPP Karahalios,

**Reference: NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT  
Highway 401 Interchange & Highway Improvements from Hespeler Road to  
Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The Ontario Ministry of Transportation (MTO) in partnership with Stantec Consulting Ltd. is undertaking the Detailed Design and Class Environmental Assessment study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo. The project includes the expansion of Highway 401 to ten lanes, including Managed Lanes (previously known as High Occupancy Vehicle lanes). The project has been divided into two phases to support construction staging. Phase I construction, GWP 3125-07-00, was completed in 2018 and included the replacement of the Franklin Boulevard bridge, some interchange work, and replacement of guiderail and municipal illumination on Franklin Boulevard. Phase II of the project is tentatively scheduled for construction in fall 2019, subject to completion of the study, funding and approvals.

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February 22, 2019  
Belinda Karahalios, MPP - Cambridge  
Page 2 of 2

Reference: **NOTICE OF SUBMISSION OF DESIGN AND CONSTRUCTION REPORT**

**Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of Cambridge – Phase II (GWP 3222-15-00)**

The OGN will be published in the Cambridge Times on Tuesday, March 5, 2019, the Turtle Island News and the Two Row Times on Wednesday, March 6, 2019, and the Waterloo Regional Record on Saturday, March 9, 2019.

Yours truly,

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Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Tel: (519) 675-6666  
Fax: (519) 645-6575  
paula.burnard@stantec.com

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- c. S. Howard, Senior Project Manager, MTO West Region
- K. Houston, Senior Environmental Planner, MTO West Region
- K. Welker, Consultant Project Manager, Stantec Consulting Ltd.
- D. Emery, Consultant Project Director, Stantec Consulting Ltd.

---

**External Agency**

---

**From:** [Warren Barlow](#)  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Franklin Bridge Closure  
**Date:** Thursday, November 09, 2017 3:32:02 PM

---

**From:** Warren Barlow <WBarlow@regionofwaterloo.ca>  
**Subject:** Franklin Bridge Closure  
**Preference:** Email  
**Mailing List:** Yes, add me to the project mailing list.

Message Body:

Hi,

As the Scheduler responsible for Grand River Transit service for Cambridge routes, I would like to get a detailed estimate on when Franklin will be completely closed, and an estimate on when it will be available for traffic again upon completion.

Our Winter Schedule (work the Bus Operators sign) lasts until April 29th, 2018. We can probably handle lane restrictions through April, but it would be appreciated if the full closure didn't happen until early May.

If that's not possible, we will require a firm date when the road is planned to be closed, several weeks ahead of time, so that we can plan our contingency (hiring of extra Bus Operators and printing detour schedules for our customers).

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>



**Waterloo Region  
District School Board**

**Education Centre**

51 Ardelt Avenue, Kitchener ON N2C 2R5  
phone: 519 570 0003 fax: 519 570 2172  
www.wrdsb.ca

November 21, 2017

Via email to: [kevin.welker@stantec.com](mailto:kevin.welker@stantec.com) and [scott.howard@ontario.ca](mailto:scott.howard@ontario.ca)

Mr. Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7

Mr. Scott Howard  
Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3<sup>rd</sup> Floor  
London, ON N6E 1L3

**Re: Highway 401 Interchange & Highway Improvements from Hespeler Road to Townline Road, City of  
Cambridge (GWP 3222-15-00)**

Dear Mr. Welker and Mr. Howard:

The Waterloo Region District School Board has historically served both sides of Highway 401 in the project vicinity by schools north of the 401. This includes both elementary students and secondary students.

While this service area has continued as primarily bussed for those students south of the 401, there is a tremendous opportunity to encourage more active transportation for these students by providing suitable multi-use paths within the proposed bridge reconstruction.

For this reason, the Waterloo Region District School Board would like to continue to be involved as the interchange improvement plans progress.

Sincerely,

A handwritten signature in black ink that reads "Shawn G. Callon".

Shawn G. Callon, MCIP, RPP  
Principal Planner  
519-570-0003 ext 4308  
[Shawn\\_Callon@wrdsb.ca](mailto:Shawn_Callon@wrdsb.ca)

cc: Lauren Agar, Manager of Planning  
Leslie Maxwell, STSWR

**From:** Paul Johnson  
**To:** [Burnard, Paula](#); [Renaud, Adam](#); [Sarah Austin](#); [Don Creed](#); [Geoffrey Keyworth](#); [SvanDeKeere@regionofwaterloo.ca](#); [Gary MacDonald](#); [Josh Graham](#); [Mike Jones](#); [BHenderson@regionofwaterloo.ca](#); [HWahib@regionofwaterloo.ca](#); [Kevin Dolishny](#); [Brad Hutchinson](#); [Don Kudo](#); [pheft@regionofwaterloo.ca](#)  
**Cc:** [Howard, Scott \(MTO\)](#); [Welker, Kevin](#); [Hochstenbach, Frank \(MTO\)](#); [Emery, Dave](#)  
**Subject:** RE: Highway 401 Interchange & Highway Improvements, Cambridge - Municipal Meeting  
**Date:** Thursday, January 17, 2019 11:45:00 AM

---

Hi Paula

I don't see any problems with the proposed work and detours at our end.

Thanks Paul

John (Paul) Johnson C.E.T., C.S.T.  
Operations Manager  
County of Wellington  
74 Woolwich Street  
Guelph ON N1H3T9  
519-837-2601 ext 2230  
519-823-3255 cell  
[paulj@wellington.ca](mailto:paulj@wellington.ca)

---

**From:** Burnard, Paula <Paula.Burnard@stantec.com>  
**Sent:** Wednesday, January 16, 2019 10:10 AM  
**To:** Renaud, Adam <Adam.Renaud@stantec.com>; Sarah Austin <AustinS@cambridge.ca>; Don Creed <dcreed@puslinch.ca>; Geoffrey Keyworth <GKeyworth@regionofwaterloo.ca>; SvanDeKeere@regionofwaterloo.ca; Paul Johnson <paulj@wellington.ca>; Gary MacDonald <GMacDonald@regionofwaterloo.ca>; Josh Graham <JGraham@regionofwaterloo.ca>; Mike Jones <MJones@regionofwaterloo.ca>; BHenderson@regionofwaterloo.ca; HWahib@regionofwaterloo.ca; Kevin Dolishny <KDolishny@regionofwaterloo.ca>; Brad Hutchinson <bradh@wellington.ca>; Don Kudo <donk@wellington.ca>; pheft@regionofwaterloo.ca  
**Cc:** Howard, Scott (MTO) <Scott.Howard@ontario.ca>; Welker, Kevin <Kevin.Welker@stantec.com>; Hochstenbach, Frank (MTO) <Frank.Hochstenbach@ontario.ca>; Emery, Dave <dave.emery@stantec.com>  
**Subject:** RE: Highway 401 Interchange & Highway Improvements, Cambridge - Municipal Meeting  
**Importance:** High

Good morning,  
The project team would like to follow up with you regarding any comments or concerns you may have on the staging plan provided by Adam Renaud via ftp on January 7<sup>th</sup>. We are looking to finalize this plan and ask that you provide any comments prior to Friday, January 18<sup>th</sup>.  
Thank you and regards,  
Paula

**Paula Burnard, MScPI, MCIP, RPP**  
Senior Environmental Planner

Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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**From:** Renaud, Adam <[Adam.Renaud@stantec.com](mailto:Adam.Renaud@stantec.com)>  
**Sent:** Monday, January 07, 2019 9:08 AM  
**To:** Sarah Austin <[AustinS@cambridge.ca](mailto:AustinS@cambridge.ca)>; Don Creed <[dcreed@puslinch.ca](mailto:dcreed@puslinch.ca)>; Geoffrey Keyworth <[GKeyworth@regionofwaterloo.ca](mailto:GKeyworth@regionofwaterloo.ca)>; SvanDeKeere@regionofwaterloo.ca; [paulj@wellington.ca](mailto:paulj@wellington.ca); Gary MacDonald <[GMacDonald@regionofwaterloo.ca](mailto:GMacDonald@regionofwaterloo.ca)>; Josh Graham <[JGraham@regionofwaterloo.ca](mailto:JGraham@regionofwaterloo.ca)>; Mike Jones <[MJones@regionofwaterloo.ca](mailto:MJones@regionofwaterloo.ca)>; [BHenderson@regionofwaterloo.ca](mailto:BHenderson@regionofwaterloo.ca); [HWahib@regionofwaterloo.ca](mailto:HWahib@regionofwaterloo.ca); Kevin Dolishny <[KDolishny@regionofwaterloo.ca](mailto:KDolishny@regionofwaterloo.ca)>; [bradh@wellington.ca](mailto:bradh@wellington.ca); [donk@wellington.ca](mailto:donk@wellington.ca); [pheft@regionofwaterloo.ca](mailto:pheft@regionofwaterloo.ca)  
**Cc:** Howard, Scott (MTO) <[Scott.Howard@ontario.ca](mailto:Scott.Howard@ontario.ca)>; Welker, Kevin <[Kevin.Welker@stantec.com](mailto:Kevin.Welker@stantec.com)>; Hochstenbach, Frank (MTO) <[Frank.Hochstenbach@ontario.ca](mailto:Frank.Hochstenbach@ontario.ca)>; Burnard, Paula <[Paula.Burnard@stantec.com](mailto:Paula.Burnard@stantec.com)>; Emery, Dave <[dave.emery@stantec.com](mailto:dave.emery@stantec.com)>  
**Subject:** Highway 401 Interchange & Highway Improvements, Cambridge - Municipal Meeting

Hello All,

Please see attached a pdf copy of the presentation that was held on December 17 in Waterloo for the above mentioned project. As requested at the meeting, I have also uploaded the staging drawings to the below FTP link.

## Login Information

**Browser link:** <https://tmpsftp.stantec.com>

**FTP Client Hostname:** tmpsftp.stantec.com **Port:** 22 (can be used within an FTP client to view and transfer files and folders; e.g., FileZilla)

**Login name:** s0121064348

**Password:** 7416944

**Disk Quota:** 2GB

**Expiry Date:** 1/21/2019

Please let me know if you have any issues downloading the staging plans from the FTP site.

Regards,

**Adam Renaud**

Transportation Engineering Intern  
Stantec  
200-835 Paramount Drive, Stoney Creek ON L8J 0B4  
Phone: (905) 385-3234  
Fax: (905) 385-3534  
[Adam.Renaud@stantec.com](mailto:Adam.Renaud@stantec.com)

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**General Public**

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**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

- ENSURE DURING FULL CLOSURES OF HWY. 401, THERE IS POLICE TRAFFIC ENFORCEMENT AT THE FRANKLIN + PINEBUSH ROUNDABOUT. IT IS A 3 LANE ROUNDABOUT AND OUT-OF-REGION DRIVERS ARE UNFAMILIAR HOW TO DRIVE IN ROUNDABOUTS, LET ALONE ONE THAT IS 3 LANES.
- CONCERNS HAVING DEPEND TRAFFIC USE ROUNDABOUTS
- MAKE TEMPORARY LANE MARKINGS MORE VISIBLE, ESPECIALLY IN WINTER AND RAIN; LINES ARE HARD TO SEE.

Completed comment forms can be left in the drop box provided or submitted by **November 22, 2017** to:

Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:

**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

Please check here if a response is not required.

Name (Optional): \_\_\_\_\_

Mailing Address (O \_\_\_\_\_

(include postal coc \_\_\_\_\_

Tel: \_\_\_\_\_

Fax: \_\_\_\_\_

Email: \_\_\_\_\_

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM  
HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**  
Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017



**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

Generally I am very supportive of the proposed 401 improvements. However, there are three areas where I believe there could be some improvements...

1 - Expansion of the carport lot at Townline should include provisions for a GO Transit and Grand River Transit Terminal. Most of the eastside Cambridge neighborhoods have large COVER →

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Please check here if a response is not required.

\_\_\_\_\_  
Name

\_\_\_\_\_  
Mailing

\_\_\_\_\_  
(include)

\_\_\_\_\_  
Telephone

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**PUBLIC INFORMATION CENTRE COMMENT FORM**

CONTINUED →] numbers of commuters between Waterloo Region and the GTA which do not have convenient transit connections. GRT could reorganize east Cambridge routes to serve the new facility and GO could carry those passengers along the 401.

2 - As a GRT bus operator I would request that the bus only turn lane on southbound Hespeler Road into the Smart Centres be expanded to accommodate two buses so they can wait for the transit priority signal without impeding the through lanes on Hespeler Road.

3 - More study/review needs to be done to improve the weaving distance between the off-ramp to Franklin Boulevard and the roundabout at Franklin and Pinebush. It is very difficult for traffic, including buses, coming south on Franklin to make lane changes to turn right onto Pinebush or travel through the roundabout in the right lane since there are 3 through lanes at the roundabout in the southbound direction. In addition, traffic coming off the 401 at Franklin travels at high speeds which makes lane changes here even more difficult and dangerous.

Enfund & PIC



**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

*need speed cameras incorporated into the design for both construction period & after. to reduce speed & help reduce accidents.*

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Please check here if a response is not required.

Name (Optional): \_\_\_\_\_  
Mailing Address (Optional): \_\_\_\_\_  
(include postal code) \_\_\_\_\_  
Tel: \_\_\_\_\_ Fax: \_\_\_\_\_ Email: \_\_\_\_\_

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

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**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

Multi-use trails at Hespeler Rd and Franklin are good (also essential)

Prefer if would close ~~the~~ south-bound ramp on west side of Franklin & have multi-use trail on west side of bridge

Completed comment forms can be left in the drop box provided or submitted by **November 22, 2017** to:

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Name (Optional):

Mailing Address (Optional):

(include postal code)

Tel:

Fax:

Email:

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**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**  
Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

-has the city and/or contractor looked at possible traffic impacts outside of the proposed construction zones & detour zones? If so, what were the possible impacts?

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Please check here if a response is not required.

Name (Optional) \_\_\_\_\_

Mailing Address (Optional) \_\_\_\_\_  
(include postal code) \_\_\_\_\_

Tel: \_\_\_\_\_

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**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

City of Cambridge and Waterloo Region should invest in pedestrian and cycling ~~approach~~ access to the new bridges crossing the 401. When traffic is pluggd they can proceed. A trail along the 401 would be green and good also

Completed comment forms can be left in the drop box provided or submitted by **November 22, 2017** to:

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Consultant Project Manager  
Stantec Consulting Ltd.  
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London, ON N6A 5J7  
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Please check here if a response is not required.

Name (Optional):

Mailing Address (Optional):

(include postal code)

Tel:

Fax:

Email:

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**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

There is a very strong need for pedestrian sidewalk / bridge.

People are crossing at the Franklins roundabout (lots of high school students) & at Hwy 24 - Both areas are very dangerous! People walking instead of driving are not polluting, they eliminate cars, and need to be factored in.

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Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
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fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:  
**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

Please check here if a response is not required.

Name (Optional): \_\_\_\_\_  
Mailing Address (\_\_\_\_\_  
(include postal code) \_\_\_\_\_  
Tel: \_\_\_\_\_

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

Entered @ PIC,



**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

I think the old ramp from the 401 onto Franklin Road should be either deleted or come out to a traffic light such as the one coming off the 401 at Hwy 24.

Completed comment forms can be left in the drop box provided or submitted by **November 22, 2017** to:

Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:  
**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

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accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

NAME	ADDRESS	CITY	PC	PHONE	EMAIL	COMMENTS	Date Submitted
						City of Cambridge and Waterloo Region should invest in pedestrian and cycling access to the new bridges crossing the 401. When traffic is plugged they can proceed. A trail along the 401 When traffic is plugged they can proceed. A trail along the 401 would be green and good also.	11/14/2017
		Cambridge				The MTO is behind in their planning for these projects. These projects should be started before traffic conditions get to the levels that are frequently observed.	11/14/2017
		Cambridge				Has the City and/or contractor looked at possible traffic impacts outside of the proposed construction zones and detour zones? If so what w the possible impacts?	11/14/2017
						Multi-use trails at Hespeler Rd and Franklin are good (also essential). Prefer if would close southbound ramp on west side of Franklin and have multi-use trail on west side of bridge. No response is required.	11/15/2017
		Kitchener				Ensure during full closure of hwy 401 there is police traffic enforcement of the Franklin and Pinebush roundabout. It is a 3 lane roundabout and out-of-region drivers are unfamiliar how to drive in roundabouts let along on that is 3 lanes. Concerns having detour traffic with roundabouts. Make temporary lane markings more visible especially in winter and rain. Lines are hard to see	11/15/2017
		Kitchener				Generally I am very supportive of the proposed 401 improvements. However there are 3 areas I believe there could be some improvements. 1. Expansion of the carpool lot at Townline should include provisions for a GO Transit and Grand River Transit Terminal. Most of the eastside Cambridge neighborhoods have large numbers of commuters between Waterloo Region and the GTA which do not have commuter transit connections. GRT could recognize east Cambridge routes to serve the new facility and GO could carry those passengers along the 401. 2. As a GRT bus operator I would request that the bus only turn lane on southbound Hespeler rd into the Smart Centers be expanded to accommodate two buses so they can wait for the transit priority signal without impeding the through lands on Hespeler rd. 3. More study/review needs to be done to improve the weaving distance between the off-ramp to Franklin Blvd and the roundabout at Franklin and Pinebush. It is very difficult for traffic including buses coming south on Franklin to make lane changes to turn right onto Pinebush or travel through the roundabout in the right lane since there are 3 through lanes at the roundabout in the southbound direction. In addition traffic coming off the 401 at Franklin travels at high speeds which makes lane changes here even more difficult and dangerous.	11/15/2017
		Cambridge				Very happy you have considered pedestrians and cyclists. We have been isolated since the under 401 trail was closed for previous lane widening. Having access to two multi use trails will make it easier to feel like Hespeler is a part of greater Cambridge.	11/14/2017
		Cambridge				Need speed cameras incorporated into the design for both construction period and after to reduce speed and help reduce accidents	
		Cambridge				No response required	11/15/2017
		Cambridge				I think the off ramp from the 401 onto Franklin road should be either deleted or come out to a traffic light such as the one coming off the 401 at Hwy 24	11/14/2017
		Cambridge				Require full details on class B drainage from 401 travelling south to my property at 420 Pinebush Rd where drainage pond was unlawfully removed in rear of my of site adjacent to the 401 It is imperative I know drainage from 401	11/14/2017
		Cambridge				How high will the sound barrier be? Will there be any encroachment on the south side of the 401 to properties along this stretch particularly near the Franklin off ramp?	11/14/2017
		Cambridge				With a growing population traffic is expected to increase in this area especially for those commuting to Toronto now moving here. Traffic on roundabouts is increasing and poses a challenge for walkers - we know this. Extending the trail through a roundabout and placing crossing lights does not mitigate the risk of an accident or ease of what a roundabout is intended to do which is make travel seamless. Even with proper lighting this poses a challenge to flow traffic. Perhaps an overhead walking bridge with proper approvals would be better option to explore that does not interfere with the flow of traffic which people walking across does. Informing the community of your decision around this is important because it is a talking point.	11/14/2017
		Cambridge				There is a very strong need for pedestrian sidewalk/bridge. People are crossing at the Franklin round about (lots of high school students) and at Hwy 24 both areas are very dangerous! People walking instead of driving are not polluting they eliminate cars and need to be factored.	11/14/2017
		Cambridge				Please add an eastbound onramp at Franklin Avenue in Cambridge	11/14/2017
		Cambridge				Looks good. My only concern is that WB 401 traffic will decide to avoid the construction zone by getting off at Townline and detouring through my neighbourhood.	11/15/2017
		Cambridge				Due to the many accidents on the 401 in recent times and owing to the fact that I am older and don't see as well at night it would be helpful to have lighting around construction sites and reflective paint to outline lanes better. I leave at 7:00 am and travel westbound from Townline and having the construction area have better lighting during construction would help not only me but many people who have difficulty seeing in the dark. Hopefully this measure would prevent accidents as well.	11/15/2017
		Cambridge				The noise is bad enough now. Once the lanes increase our levels are going to be unbearable. We need a beam barrier or some type of noise reduction made.	11/14/2017
		Cambridge				Noise concern re no sound barrier wall proposed for residents south of 401	11/14/2017
		Cambridge				Did not address noise barrier wall for residents on the south side of the 401	11/14/2017
		Cambridge				Speed construction as much as you can	11/14/2017
		Puslinch				Really wish the Franklin interchange was a full 4 direction one. In lieu of that at the least have access to 401 EB so all of the industry on Franklin has better access to the biggest market in Canada the GTA.	11/14/2017
		Cambridge				We hope to have a left hand turning lane into Melran Drive which is opposite our driveway. We are concerned that cars are using the shoulder instead of waiting for cars to turn left. We have been hit while turning into our driveway due to people thinking we were turning left. We spoke to Geoffrey Keyworth and he indicated our comments would be forwarded to the Region.	11/14/2017

**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM  
HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**  
Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

THE FACT NO COMPLAINTS HAVE BEEN FILED  
FOLLOWING THE MOE ASSESSMENT FOR OUR AREA  
3 YEARS AGO DOES NOT MEAN THAT THE NOISE  
LEVEL WAS EASY TO ACCEPT OR IS ACCEPTABLE.  
ACTUALLY THE LEVEL HAS BEEN INCREASING  
OVERTIME AND EVEN IF THE 10 LANES TO  
TOWNLINE GO BACK TO 6 AFTER THE TOWNLINE  
BRIDGE, THIS TREND WILL CONTINUE AND WE  
ARE ONLY A FEW 100 METRES AWAY FROM THE 401 →  
CON'D.

Completed comment forms can be left in the drop box provided or submitted by  
**November 22, 2017** to:

Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-  
townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be  
submitted via the project  
website:

**[highway401-hespeler-  
townline.ca](http://highway401-hespeler-<br/>townline.ca)**

Please check here if a response is not required.

\_\_\_\_\_  
Mailing Address (Optional)

\_\_\_\_\_  
(include postal code)

Tel \_\_\_\_\_

fax \_\_\_\_\_

email: \_\_\_\_\_

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accordance with the *Freedom of Information and Protection of Privacy Act*.  
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the public record.

CONTINUED

A NOISE WALL FROM THE TOWNLINE EAST BOUND ON RAMP TO THE ON ROUTE REST STATION WOULD PROTECT OUR COMMUNITY FROM THE EXPECTED HIGHER NOISE LEVEL, ESPECIALLY STRONG, WHEN THE WIND DIRECTION IS NORTH OR NORTH WEST.

**From:**  
**To:** [Burnard, Paula](#)  
**Subject:** Re: Hwy401/Hespeler  
**Date:** Wednesday, October 25, 2017 9:21:50 AM

---

Thanks Paula for the quick reply.

I will add it to my calendar.

Sincerely,

Sent from my BlackBerry 10 smartphone on the Rogers network.

Original Message  
From: Burnard, Paula  
Sent: Wednesday, October 25, 2017 9:11 AM  
To:  
Subject: RE: Hwy401/Hespeler

Hi

We have added you to the project contact list. Notices of the upcoming Public Information Centre will be sent out soon. The Public Information Centre date is secured for November 14th from 4:00 to 8:00 pm. Details of the meeting will be sent out soon and published in the Waterloo Record, Cambridge Times, Two Row Times and Turtle Island News. Information will also be posted on the project website: [www.highway401-hespeler-townline.ca](http://www.highway401-hespeler-townline.ca)

Thank you,  
Paula

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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Please consider the environment before printing this email.

-----Original Message-----

From:  
Sent: Tuesday, October 24, 2017 12:17 PM  
To: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
Subject:

From:  
Subject:  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

Can you please add me to the mailing list and advise when the upcoming public information center is to be held.

**From:** [Welker, Kevin](#)  
**To:** [Burnard, Paula](#)  
**Subject:** FW: PIC - GWP 3222-15-00  
**Date:** Monday, November 13, 2017 4:03:10 PM

---

**Kevin Welker, P.Eng., ENV SP**

Senior Associate, Transportation  
Stantec  
600-171 Queens Avenue, London ON N6A 5J7  
Phone: (519) 675-6652  
Cell: (226) 919-5979  
[kevin.welker@stantec.com](mailto:kevin.welker@stantec.com)

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 Please consider the environment before printing this email.

**From:**  
**Sent:** Monday, November 13, 2017 3:59 PM  
**To:** Welker, Kevin <[Kevin.Welker@stantec.com](mailto:Kevin.Welker@stantec.com)>  
**Subject:** PIC - GWP 3222-15-00

Good afternoon,

I would like to be added to your mailing list. May I ask, since Phase 1 is scheduled for 2018, what exactly is being done right now on the 401 in this corridor? There is a lot of construction already happening that does not appear to be involved in these phases.

Regards,

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Franklin Bridge  
**Date:** Wednesday, November 15, 2017 10:35:13 PM

---

From:  
Subject: Franklin Bridge  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

I have a question regarding the Franklin Bridge. Since this bridge is being rebuilt soon has any thought being considered to having an off-ramp going east on to the 401 from Franklin Blvd and an on-ramp on to Franklin from the 401 going westbound. This would make it more convenient for commuters instead of driving down to Townline Road to access the 401 going eastbound.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** [Howard, Scott \(MTO\)](#)  
**To:** [Burnard, Paula](#)  
**Subject:** FW: highway401-hespeler-townline  
**Date:** Wednesday, November 15, 2017 10:56:17 AM

---

-----Original Message-----

**From:**  
**Sent:** November-15-17 10:45 AM  
**To:** Howard, Scott (MTO); kevin.welker@stantec.com  
**Subject:** highway401-hespeler-townline

Good Morning,

Can you please add my email address to the mailing list regarding this project? If the informatio is getting sent by Canada Post, our home address is

We are not in favour of the widening of the 401 up to Townline Rd. The noise volume is already very loud and disruptive, especially the trucks with their loud exhaust, road noise, etc. At the very least, sound barriers should be installed from Townline Rd to Wellington Rd 35 regardless of whether it is decided to widen the highway or not.

While I do understand traffic levels have increased and there is money to be made by the companies building the roads and bridges, it is not fair to the residents who live along the area affected. It affects our daily lives.

Thank you,

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** 401 widening Hespeler Rd to Townline Rd. and any future widening  
**Date:** Wednesday, November 15, 2017 12:22:29 PM

---

From:  
Subject: 401 widening Hespeler Rd to Townline Rd. and any future widening  
Preference: Email  
Mailing List:

Message Body:

I live on Puslinch Lake, on the NW side, the side closest to the 401. I already hear noise from the 401 24/7. I'm concerned that the widening will increase the noise. I'd like to request a sound barrier be erected as close as possible to the west AND EAST sides of the Townline Rd. bridge.

I'm also concerned about future 401 widening east of Townline Rd. The construction noise and dust/dirt, as well as the enhanced proximity of the physical highway to my home could increase noise levels substantially, affecting sleep, having windows open, and enjoying time on my deck.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** 401 widening Hespeler Rd to Townline Rd. and any future widening  
**Date:** Wednesday, November 15, 2017 12:23:43 PM

---

From:  
Subject: 401 widening Hespeler Rd to Townline Rd. and any future widening  
Preference: Email  
Mailing List:

Message Body:

I live on Puslinch Lake, on the NW side, the side closest to the 401. I already hear noise from the 401 24/7. I'm concerned that the widening will increase the noise. I'd like to request a sound barrier be erected as close as possible to the west AND EAST sides of the Townline Rd. bridge.

I'm also concerned about future 401 widening east of Townline Rd. The construction noise and dust/dirt, as well as the enhanced proximity of the physical highway to my home could increase noise levels substantially, affecting sleep, having windows open, and enjoying time on my deck.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Sound barriers for 401 widening  
**Date:** Wednesday, November 15, 2017 1:34:56 PM

---

From:  
Subject: Sound barriers for 401 widening  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:  
Good afternoon,

I would like to register my input that if there is to be any widening of the 401 past Townline Road then adequate measures must be taken to mitigate the sound impact to area residents. The traffic noise as it stands is obtrusive, but I recognize that the highway was there before I moved in. But with the proposed highway changes it would be an act of negligence to not address the sound pollution

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Noise at the Lake  
**Date:** Wednesday, November 15, 2017 5:21:38 PM

---

From:  
Subject: Noise at the Lake  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

Regarding: The Hwy 401 Improvement from Hespeler Road to Townline Road

I would like to bring to your notice my concern with the expanding usage of the 401 to 10 lanes in that it will greatly increase the already aggravating noise level at my home.

I live on Puslinch Lake and am often woken up during the night with the noise from the 401 with its current level of traffic. The on-ramp at Townline Road, merging on to the 401, is a ramp that seems to be a favorite of cars, motorcycles and loud trucks to speed up to highway speeds. The need to merge causes the vehicles to accelerate and produce a lot of noise.

The highway is also very loud summer and winter. In the summer with spending more time outside the noise is very pervasive. But even with more inside living during the winter, with less foliage the noise is still a big issue for us on the lake.

I dread the thought of a busier highway infringing on my lifestyle of out-door living which is a major reason we enjoy living on a lake. The noise will defiantly lower the value of my unique and wonderful property.

I am asking you to take my concerns into consideration as you lay plans for this huge expansion.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Franklin Blvd multi-use trail component for Hespeler community  
**Date:** Thursday, November 16, 2017 10:48:05 AM

---

From: Franklin Blvd multi-use trail component for Hespeler community  
Preference: Email  
Mailing List:

Message Body:  
To the Project Team:

I attended the PIC on the next phase of the 401 widening through Cambridge yesterday evening and was extremely disappointed to see that the proposed pedestrian/cyclist/other crossing of the 401 at Franklin Blvd is expected to cross the westbound 401 on-ramp at grade!!

The design profile (MTO part) is well conceived with a completely separated lane for this multi-use trail, but the connection north from there (RMOW) is simply not going to be as safe as it should and must be. The design being limited of course by retaining the westbound ramp and making do with the situation.

For the Project Team's information, a proper ped/cyclist crossing of the 401 has been an outstanding issue for MANY years, pretty much since the Hespeler community became part of Cambridge, and over time many of the retail and service locations ended up on the south side of the 401. In the last fifteen years or so, many student part time job opportunities required them to make this crossing, and while transit does exist it wasn't as direct or convenient as Franklin Blvd.

Franklin Blvd is the most natural path for this community connection: Hespeler Road is simply too big, busy and not ped or cycle friendly. Townline's most recent rebuild is better, but it is too far to the east to serve the bulk of potential users. You would be hard pressed to ever see a pedestrian using Townline, although cyclists do.

The community has been waiting for this rebuild of Franklin Blvd as the opportunity to address this longstanding and absolutely critical ped/cyclist facility. Probably the ONLY opportunity for a long time to come?

So the proposed at grade crossing of the Franklin multi-use trail shown on Nov 14/17, where pedestrians, cyclists and other mobility aid users will have to beware of and give way to the on- ramp traffic is simply not an acceptable design solution.

I would be pleased to discuss this concerns further, should you wish to contact me.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Cycle / Pedestrian routes  
**Date:** Thursday, November 16, 2017 1:42:51 PM

---

From:  
Subject: Cycle / Pedestrian routes  
Preference: Email  
Mailing List:

Message Body:

Although it would be additional cost to the planned multi user pathways on HWY 24 and Franklin bridges, a surely logical extension and safer route, segregating self-powered users from motorised traffic, would be a bridge (like at Homer Watson) from Rona (Smart Centre Plaza) to the Zehrs parking lot linking to the Conestoga Boulevard multi-use trail currently being constructed. Access to both sides of the bridge would be via far quieter routes. HWY 24 and Franklin nodes are way too busy for cyclist and pedestrian safety to be a priority

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Noise Barriers  
**Date:** Thursday, November 16, 2017 2:09:57 PM

---

From:  
Subject: Noise Barriers  
Preference: Email  
Mailing List: Yes, add me to the project mailing list.

Message Body:

For the Townline Road to Highway 6 widening, could you install noise barriers east of Townline Road for the Bayview Cottage area to the south?

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>



**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

While we support the addition of the extra lanes to allow better traffic flow on the 401 we do realize that the increased volume it allows will create increased road noise.

As such we respectfully request that a similar noise barrier structure be erected along the area of the 401 East of Cambridge Townline Road, especially the south side where all of the residents of Pushtinch Lake Community live. We suggest that this barrier extend from Cambridge Townline Road to Wellington Rd 32 where it crosses the 401. This is only about a 1km area and would make

Completed comment forms can be left in the drop box provided or submitted by a major difference to the noise levels.  
**November 22, 2017 to:**  
Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:  
**highway401-hespeler-townline.ca**

Please check here if a response is not required.

Name (Optional):

Mailing Address (Optional)

(include postal code)

Tel:

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**HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM  
HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)**

Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

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Please provide your comments on the project. *(Use the back of this page if you need more space.)*

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Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
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tel.: 519-675-6652 (+0 collect)  
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Comments may also be  
submitted via the project  
website:

**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

Please check here if a response is not required.

Name (Optional): \_\_\_\_\_

Mailing Address (Optional) \_\_\_\_\_

(include postal code) \_\_\_\_\_

Tel: \_\_\_\_\_

Fax: \_\_\_\_\_

Email: \_\_\_\_\_

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Noise Complaint  
**Date:** Saturday, November 18, 2017 4:15:39 PM

---

From:  
Noise Complaint  
Preference:  
Mailing List:

Message Body:

Thank you for taking the comments and concerns for community surrounding this project. My husband and I live in a small subdivision adjacent to the 401 on Puslinch Lake and already experience high levels of noise coming from the highway. The sound of trucks roaring by are audible day & night and particularly in the summer when our windows are open, even though we are set far back from the highway. It seems to be getting worse every year. It is my understanding that a sound barrier wall will not be extended beyond Townline Rd during this phase of the project. When the highway is expanded in the future between Townline Rd and Guelph I would hope that a wall will be considered along the residential area that borders our subdivision. Thanks again and no need to respond.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

RECEIVED  
NOV 19 2017

STANTEC CONSULTING LTD.

---

To the Project Team:

I attended the PIC on the next phase of the 401 widening through Cambridge yesterday evening and was extremely disappointed to see that the proposed pedestrian/cyclist/other crossing of the 401 at Franklin Blvd is expected to cross the westbound 401 on-ramp at grade!!

The design profile (MTO part) is well conceived with a completely separated lane for this multi-use trail, but the connection north from there (RMOW) is simply not going to be as safe as it should and must be. The design being limited of course by retaining the westbound ramp and making do with the situation.

For the Project Team's information, a proper ped/cyclist crossing of the 401 has been an outstanding issue for MANY years, pretty much since the Hespeler community became part of Cambridge, and over time many of the retail and service locations ended up on the south side of the 401. In the last fifteen years or so, many student part time job opportunities required them to make this crossing, and while transit does exist it wasn't as direct or convenient as Franklin Blvd.

Franklin Blvd is the most natural path for this community connection: Hespeler Road is simply too big, busy and not ped or cycle friendly. Townline's most recent rebuild is better, but it is too far to the east to serve the bulk of potential users. You would be hard pressed to ever see a pedestrian using Townline, although cyclists do.

The community has been waiting for this rebuild of Franklin Blvd as the opportunity to address this longstanding and absolutely critical ped/cyclist facility. Probably the ONLY opportunity for a long time to come?

So the proposed at grade crossing of the Franklin multi-use trail shown on Nov 14/17, where pedestrians, cyclists and other mobility aid users will have to beware of and give way to the on-ramp traffic is simply not an acceptable design solution.

I would be pleased to discuss this concerns further, should you wish to contact me.

HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM  
HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)  
Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

RECEIVED Ontario  
NOV 19 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

We live in the north-west area of Puskech Lake. (Bayview Community) The present noise we have subjected to from the ceaseless traffic on 401 is unacceptable. The increase in vehicular flow will only be a further detriment to the peace and enjoyment of our property. When the traffic lanes are increased, a substantial berm must be included to provide adequate noise abatement.

Completed comment forms can be left in the drop box provided or submitted by **November 22, 2017** to:

Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:

**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

Please check here if a response is not required

Name

Mailing

(includ

Tel:

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Sound Barrier S. of 401 Hespeler Rd.  
**Date:** Monday, November 20, 2017 12:28:51 PM

---

From:  
Subject: Sound Barrier S. of 401 Hespeler Rd.  
Preference:  
Mailing List:

Message Body:

To: Kevin Welker  
Consultant Project Mgr.

As an attendee at the meeting held at the Hespeler Arena Nov. 14th I am writing to convey my concerns re: the sound barrier installation. As I understand it, the barrier will be installed only up to Townline Rd.

As a resident living just east of Townline south of the 401 off lake Rd. we have noticed increased noise levels as the volume of traffic has continued to pick up over the years which I assume is reason for the entire project in the first place. That being said I would also assume that traffic noise that residents in this stretch will be subjected to will also be increasing exponentially particularly due to truck traffic.

I question the value of noise barriers extending from Hespeler Rd to Townline protecting the lands south of the 401 along Eagle St that are entirely industrial or vacant yet leave the only residential area east of Hespeler Rd unprotected? I urge you to seriously consider extending the barrier to at least road #34 to protect the health and well being of the affected residents.

Yours Truly,

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>



HIGHWAY 401 INTERCHANGE & HIGHWAY IMPROVEMENTS FROM  
HESPELER ROAD TO TOWNLINE ROAD, CITY OF CAMBRIDGE (GWP 3222-15-00)  
Hespeler Memorial Arena, Gallery Room – Tuesday, November 14, 2017

**PUBLIC INFORMATION CENTRE COMMENT FORM**

Please provide your comments on the project. (Use the back of this page if you need more space.)

- DURING CONSTRUCTION IS THE IDEAL OPPORTUNITY TO HELP US RESIDENTS <sup>TAXPAYERS</sup> WHO LIVE IN CLOSE PROXIMITY BY PUTTING UP A SOUND BARRIER TO PREVENT EXCESSIVE ROAD NOISE FROM CONTINUOUS TRAFFIC & PRESERVE WILDLIFE
- COST OF THE BARRIER RELATIVE TO COST OF 401 EXPANSION WOULD BE VERY MINIMAL
- FAMILY MEMBER WITH CHRONIC DISEASE WHO ONLY SLEEPS FOR SHORT PERIODS OF TIME
- ADDITIONAL ROAD NOISE RESULTING FROM EXPANSION WOULD FURTHER EXACERBATE THIS PROBLEM

Completed comment forms can be left in the drop box provided or submitted by

**November 22, 2017 to:**  
Kevin Welker, P.Eng., ENV SP  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel.: 519-675-6652 (+0 collect)  
fax: 519-645-6575  
e-mail: [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

Comments may also be submitted via the project website:  
**[highway401-hespeler-townline.ca](http://highway401-hespeler-townline.ca)**

Please check here if a response is not required.

Name (Optional):

Mailing Address (Optional):

(include postal code)

Tel:

Fax:

Email:

**Thank you for your participation.** Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

**From:** [Burnard, Paula](#)  
**To:** [Werner, Julie](#)  
**Subject:** FW: Sound barrier for 401 expansion east of Townline Rd.  
**Date:** Tuesday, November 21, 2017 12:14:21 PM

---

Pls save on the network and add to our comments.  
Thank you!

Paula Burnard, MScPl, MCIP, RPP  
Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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-----Original Message-----

**From:**  
**Sent:** Tuesday, November 21, 2017 11:48 AM  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Sound barrier for 401 expansion east of Townline Rd.

**From:**  
**Subject:** Sound barrier for 401 expansion east of Townline Rd.  
**Preference:** Email  
**Mailing List:** Yes, add me to the project mailing list.

**Message Body:**

Hello,

As residents that form the community just off Lake Rd. east of Townline Rd., we have experienced constant noise for the 401. This noise has continued to increase by the year as the volume of traffic has increased and has now reached a point where we can no longer enjoy sitting outside in our own backyard due to the background volume. With the proposed widening of the highway between Hespeler and Townline, this traffic volume will only increase. I understand that the proposed sound barrier is only planned to extend to Townline leaving our community on Lake Rd exposed to the additional noise (volume, breaking, etc.) resulting from traffic flowing from the expanded lanes back to 6 lanes as well as the expected increased volume of traffic using the expanded highway. I am requesting on behalf of myself and all of us who live off Lake Rd east of Towline that the sound barrier be expanded to at least Wellington Rd 32 (just west of the service centre) to protect our community.

Thank you for your consideration and support,

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

-----Original Message-----

From:

Sent: Wednesday, November 22, 2017 4:04 PM

To: comments@highway401-hespeler-townline.ca

Subject: sound barrier wall

From:

Subject: sound barrier wall

Preference: Email

Mailing List: Yes, add me to the project mailing list.

Message Body:

My wife and I live on Puslinch Lake, on Swastika Trail, just east of Townline. Right now we can hear the 401 like it's right in our kitchen. We recently replaced all our windows in the house to 2 pane glass, and it is still very loud. We are worried that once the 401 is expanded we won't be able to hear ourselves think. A sound barrier wall east of Townline is greatly needed and would be greatly appreciated. A wall of trees would also be needed. Thank you,

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**

**Sent:** Thursday, November 23, 2017 9:46 AM

**To:** comments@highway401-hespeler-townline.ca

**Subject:** Re: HWY 401 Planned Expansion from Townline to Guelph

To the HWY 401 Project Team:

We live at \_\_\_\_\_ We already hear noise from the 401 in our neighborhood already as it is (both while outdoors and from several bedrooms while indoors), and the planned expansion to the highway from Townline towards Guelph will only increase the level of that noise.

Out of consideration for those of us that live within close proximity to the highway we would ask that a sound barrier please be put in place as the highways is expanded around the area that would affect our neighborhood.

Thank you very much for considering our request.

-----Original Message-----

From:

Sent: Thursday, November 23, 2017 10:13 AM

To: comments@highway401-hespeler-townline.ca

Subject: Sound Barrier Wall

From:

Subject: Sound Barrier Wall

Preference: Email

Mailing List: Yes, add me to the project mailing list.

Message Body:

We understand that the 401 will be expanded to 10 lanes of traffic from Franklin Blvd. to Townline Road. We live on Puslinch Lake right behind the 401 and can clearly hear the traffic, especially semi trucks, at all hours of the day and night. It is our concern that the noise will be greatly increased going from 4 lanes to 10. We respectfully ask that a sound barrier wall be constructed along the 401 from Townline Road until just before the ON Route to muffle the traffic noise for the residents on Lake Road, including our neighbourhood.

Thank you.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

-----Original Message-----

From:

Sent: Thursday, November 23, 2017 10:40 AM

To: comments@highway401-hespeler-townline.ca

Subject: noise

From:

Subject: noise

Preference: Land mail

Mailing List: Yes, add me to the project mailing list.

Message Body:

I live at

Traffic noise from 401 is very loud, especially if the wind blowing from anywhere on the compass from west to north east. Two neighbours have moved away because of the noise.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** 401 widening Hespeler Rd to Townline Rd. and any future widening  
**Date:** Sunday, November 26, 2017 8:08:06 PM

---

From:  
Subject: 401 widening Hespeler Rd to Townline Rd. and any future widening  
Preference:  
Mailing List:

Message Body:

I live on Puslinch Lake, on the NW side, the side closest to the 401. I already hear noise from the 401 24/7. I'm concerned that the widening will increase the noise. I'd like to request a sound barrier be erected as close as possible to the west AND EAST sides of the Townline Rd. bridge. The wind blows predominantly from the NW, so the sound from west of the Townline Rd. bridge is usually the loudest.

I'm also concerned about future 401 widening east of Townline Rd. The construction noise and dust/dirt, as well as the enhanced proximity of the physical highway to my home could increase noise levels substantially, affecting sleep, having windows open, and enjoying time on my deck.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** 401 widening Hespeler Rd to Townline Rd. and any future widening  
**Date:** Sunday, November 26, 2017 8:10:39 PM

---

From:  
Subject: 401 widening Hespeler Rd to Townline Rd. and any future widening  
Preference:  
Mailing List:

Message Body:

I live on Puslinch Lake, on the NW side, the side closest to the 401. I already hear noise from the 401 24/7. I'm concerned that the widening will increase the noise. I'd like to request a sound barrier be erected as close as possible to the west AND EAST sides of the Townline Rd. bridge. The wind blows predominantly from the NW, so the sound from west of the Townline Rd. bridge is usually the loudest.

I'm also concerned about future 401 widening east of Townline Rd. The construction noise and dust/dirt, as well as the enhanced proximity of the physical highway to my home could increase noise levels substantially, affecting sleep, having windows open, and enjoying time on my deck.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:**  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** 401 widening Hespeler Rd to Townline Rd. and any future widening  
**Date:** Sunday, November 26, 2017 8:12:48 PM

---

From:  
Subject: 401 widening Hespeler Rd to Townline Rd. and any future widening  
Preference:  
Mailing List:

Message Body:

My daughter lives on Puslinch Lake, on the NW side, the side closest to the 401. I visit often, and already I hear noise from the 401 every time I'm there. I'm concerned that the widening will increase the noise. I'd like to request a sound barrier be erected as close as possible to the west AND EAST sides of the Townline Rd. bridge. The wind blows predominantly from the NW, so the sound from west of the Townline Rd. bridge is usually the loudest. I'm also concerned about future 401 widening east of Townline Rd. The construction noise and dust/dirt, as well as the enhanced proximity of the physical highway to my daughter's home could increase noise levels substantially, affecting her sleep, having her windows open, and all of us enjoying time on her deck.

--

This e-mail was sent from a contact form at <http://highway401-hespeler-townline.ca>

**From:** [Burnard, Paula](#)  
**To:**  
**Subject:** RE: Franklin bridge and why 401 rework  
**Date:** Monday, November 19, 2018 3:23:34 PM

---

Hi ,

Thank you for your email. Your interest in an eastbound on-ramp to Highway 401 has been noted. The provision of an exit off Franklin to go east to Toronto was reviewed during the Preliminary Design stage. Due to the major property impacts and the proximity of the Hespeler Road and Townline Road interchanges, this alternative was not carried forward. The preferred alternative was to have Franklin Boulevard remain as a partial interchange (i.e., in its current configuration). Please let us know if you have any further questions.

Thank you and regards,  
Paula

**Paula Burnard, MScPI, MCIP, RPP**

Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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---

**From:**  
**Sent:** Monday, November 19, 2018 10:55 AM  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Franklin bridge and why 401 rework

As a resident of the area of Cambridge know as Hespeler , I would like to know if there are any plans to include ramps for drivers to have access to the 401 East and West from the north side of the 401. That is , if you are leaving the Hespeler area using Franklin Blvd. will it be possible to get onto the 401? And if no decisions have been made to do this , what are the reasons.

A very good reason for these ramps being constructed is that it would help minimize the traffic on Queen Street West from Hespeler road to Cooper Street. The narrow road and sidewalks of Queen Street, with no boulevard separating pedestrians from heavy traffic , pose a safety issue for anyone using this route and we should be looking ways to reduce traffic along this route . Ramps at Franklin would do just that as most traffic here is going to areas east of Franklin and East of Cooper St. You would think that the Townline ramps would help take away traffic from this area but human nature is to take a direct route to a destination rather than backtrack as Townline does.

Thank You

Cambridge, Ontario  
N3C1G2

**From:** [Burnard, Paula](#)  
**To:**  
**Cc:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca); [Howard, Scott \(MTO\)](#); [Mitchell, Terence \(MTO\)](#); [Welker, Kevin](#)  
**Subject:** RE: Franklin Blvd  
**Date:** Friday, November 02, 2018 10:48:52 AM

---

Good morning,

Thank you for your email. Your interest in an eastbound on-ramp to Highway 401 has been noted. The provision of an exit off Franklin to go east to Toronto was reviewed during the Preliminary Design stage. Due to the major property impacts and the proximity of the Hespeler Road and Townline Road interchanges, this alternative was not carried forward. The preferred alternative was to have Franklin Boulevard remain as a partial interchange (i.e., in its current configuration).

Please let us know if you have any further questions.

Thank you and regards,  
Paula

**Paula Burnard, MScPI, MCIP, RPP**  
Senior Environmental Planner  
Stantec  
600-171 Queens Avenue London ON N6A 5J7  
Phone: 519-675-6666  
[paula.burnard@stantec.com](mailto:paula.burnard@stantec.com)

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---

**From:**  
**Sent:** Tuesday, October 30, 2018 2:49 PM  
**To:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)  
**Subject:** Franklin Blvd  
**Importance:** High

To whom it may concern

Thankyou for your update on opening up Franklin Blvd at Hwy 401.

Please advise if your next phase of construction from 2019 onwards will include an exit off Franklin to go east to Toronto. At present you only have an exit going west to Kitchener

Thank you

Cambridge Ont

# PIC Materials



# Appendix B

# Accessibility



## Integrated Accessibility Standards Regulation

The Ontario Ministry of Transportation is committed to ensuring this Public Information Centre is accessible to all participants by incorporating the following accessibility features:

- Accessible venue and location, including ramps, accessible washrooms, and parking
- For people requiring assistance:
  - study team members will verbally explain presentation board content
  - study team members will assist with written submission of comment forms
  - service animals are welcome
- Presentation boards and displays printed in large, legible font
- Reading aids are available (such as magnifying glasses)

# Welcome

---

*Public Information Centre for the*

## Highway 401 Interchange & Highway Improvements

*from Hespeler Road to Townline Road,  
City of Cambridge (GWP 3222-15-00)*



### ***Please sign-in***

*and take time to review the information.  
Representatives from the MTO and Stantec  
are available to discuss the project with you.  
We also encourage you to complete a comment  
sheet before leaving.*



### Public Information Centre Objectives

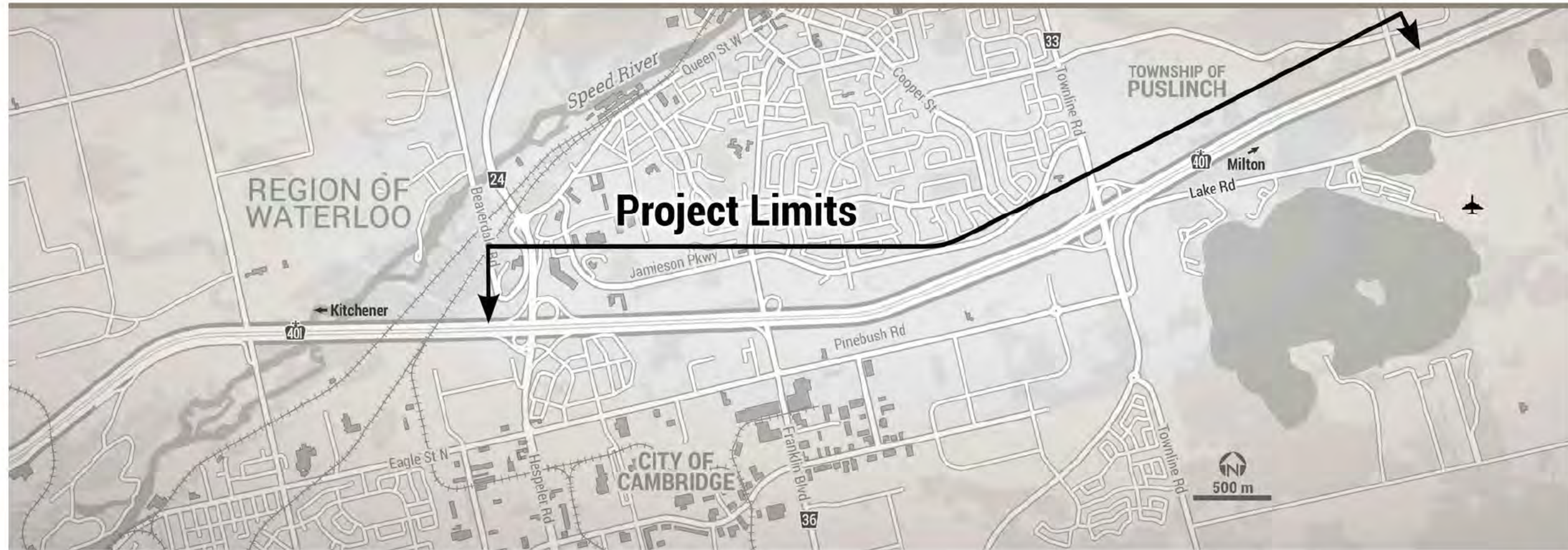
- Present information on highway and interchange improvements, including construction staging and traffic management plans
- Gather input on the project
- Answer questions about the design and construction of the project



### Freedom of Information & Protection of Privacy Act

Information collected will be used in accordance with the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

# About the Project



## Project description

The Approved Plan from the preliminary design and EA study includes the reconstruction of the Highway 401 interchanges at Hespeler Road and Franklin Boulevard, structural repairs at Towlinae Road, and the additional reconstruction/widening to the ultimate 10-lane cross-section.

Since the commencement of the Detailed Design, the project has been divided into two phases, to support construction staging.

## Phase I

- Replacement of Franklin Blvd bridge, including some interchange work
- Pedestrian/cyclist facilities across Highway 401 on Franklin Blvd bridge, in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge
- Replacement of guiderail and municipal illumination on Franklin Blvd

## Phase II

- Widening Highway 401 to 10 lanes, including HOV lanes
- Reconstruction of Hespeler Road interchange
- Reconstruction of ramp tie-ins at Franklin Blvd and Towlinae Road interchanges
- Replacement of Hespeler Road bridge and retaining walls
- Pedestrian/cyclist facilities across Highway 401 on Hespeler Road bridge, in accordance with the active transportation goals of the Region of Waterloo and the City of Cambridge
- Drainage improvements, illumination, overhead signs, traffic signals and noise barrier wall
- Rehabilitation of retaining walls at Towlinae Road bridge
- Expansion of commuter parking lot at Towlinae Road interchange

# About the Process

The Ontario Ministry of Transportation (MTO) retained Stantec Consulting Ltd. to undertake the Detailed Design and Class Environmental Assessment (Class EA) Study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo.

The Preliminary Design of the Group 'B' project was included in the Highway 401 from Hespeler Road easterly to the Wellington County/Halton Region Boundary Study (GWP 8-00-00), which concluded with the publication of a Transportation Environmental Study Report (TESR). The TESR received Environmental Assessment (EA) clearance in November 2013.

This Detailed Design and Class EA Study for improvements on Highway 401 from Hespeler Road to Townline Road in the City of Cambridge, Region of Waterloo commenced in May 2014.

The Notice of Study Commencement was provided in June 2014. A Notice of Study Update, which included the widening of Highway 401 to ten lanes and contract phasing, was provided in June 2017.

## THE CLASS ENVIRONMENTAL ASSESSMENT PROCESS for GROUP 'B' PROJECTS



## Next Steps...



Input received from this PIC will be reviewed and considered as the detailed design is finalized. A Design and Construction Report (DCR) documenting Phase I will be available for a 33 day review period Friday, December 8, 2017. A notice with this information will be provided in advance.

The DCR documenting Phase II will be available for a 30 day review in Spring 2018. The Project Team will make reasonable efforts to resolve issues or concerns raised during the review period for the DCR.

# Environmental Management

Environmental factors were assessed during the Preliminary Design Study to determine existing conditions and identify mitigation measures to minimize and/or address potential impacts associated with the proposed works. Additional field work has been undertaken to update information and identify changes to statutory requirements, based on the Approved Plan.

Contract documentation will include general environmental management and specific components to address environmental mitigation related to the following components:

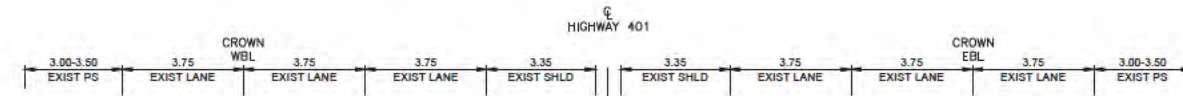
- noise
- groundwater resources
- surface water
- air quality
- vegetation
- erosion and sediment control
- archaeology
- wildlife habitat
- management of excess material

## Potential Environmental Impacts

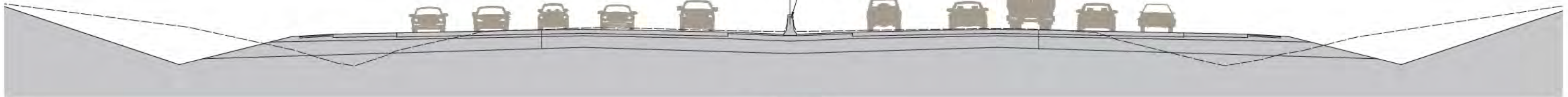
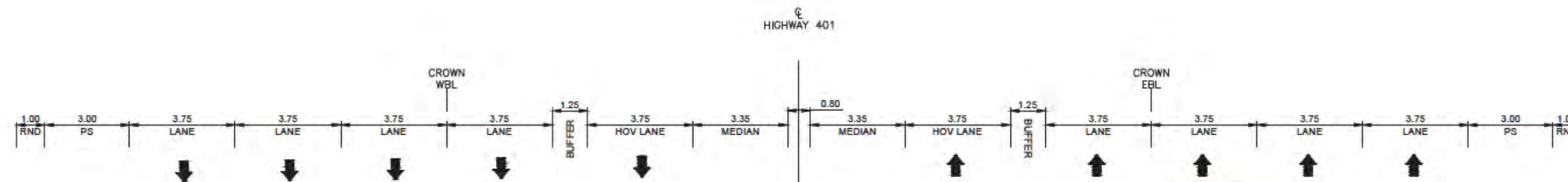
<i>Type</i>	<i>Proposed Mitigation Measures</i>
Traffic Delays	<ul style="list-style-type: none"> <li>• Advanced notification of construction start will be provided via road signage</li> <li>• EMS, municipalities, transit services and school boards will be notified in advance of lane closures</li> <li>• Interchange ramps will remain open as much as possible. However, long term closure of the Franklin Blvd westbound on-ramp will be required while the bridge over Highway 401 is under construction.</li> </ul>
Socio-economic	<ul style="list-style-type: none"> <li>• Noise by-law exemption will be obtained for overnight work</li> <li>• Best practices will be in place to reduce potential air quality/dust impacts</li> <li>• Traffic delays have been minimized to the extent possible</li> </ul>
Natural Environment	<ul style="list-style-type: none"> <li>• Construction will occur within the existing right-of-way with minimal impacts to vegetation</li> <li>• No species at risk were observed within the study area</li> </ul>
Utilities	<ul style="list-style-type: none"> <li>• Utilities in conflict with construction will be relocated prior to construction</li> </ul>
Cultural Environment	<ul style="list-style-type: none"> <li>• Archaeological assessment and clearance was completed during Preliminary Design; no impacts to archaeological resources anticipated</li> </ul>



# Highway 401: before and after



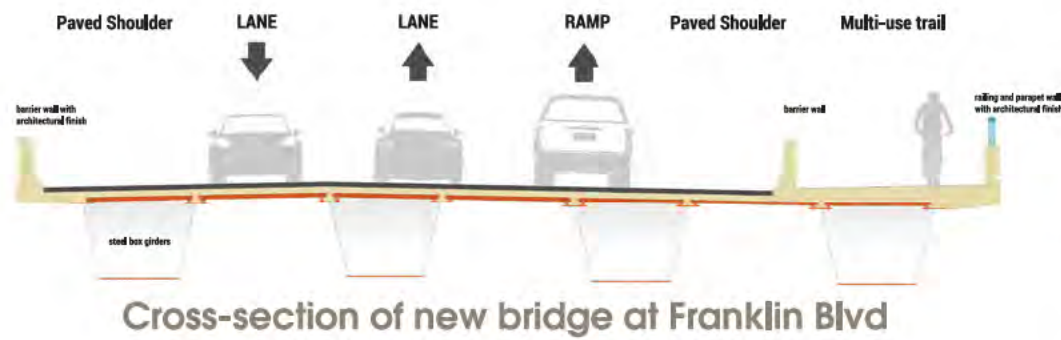
BEFORE



AFTER

# Phase I: Franklin Blvd

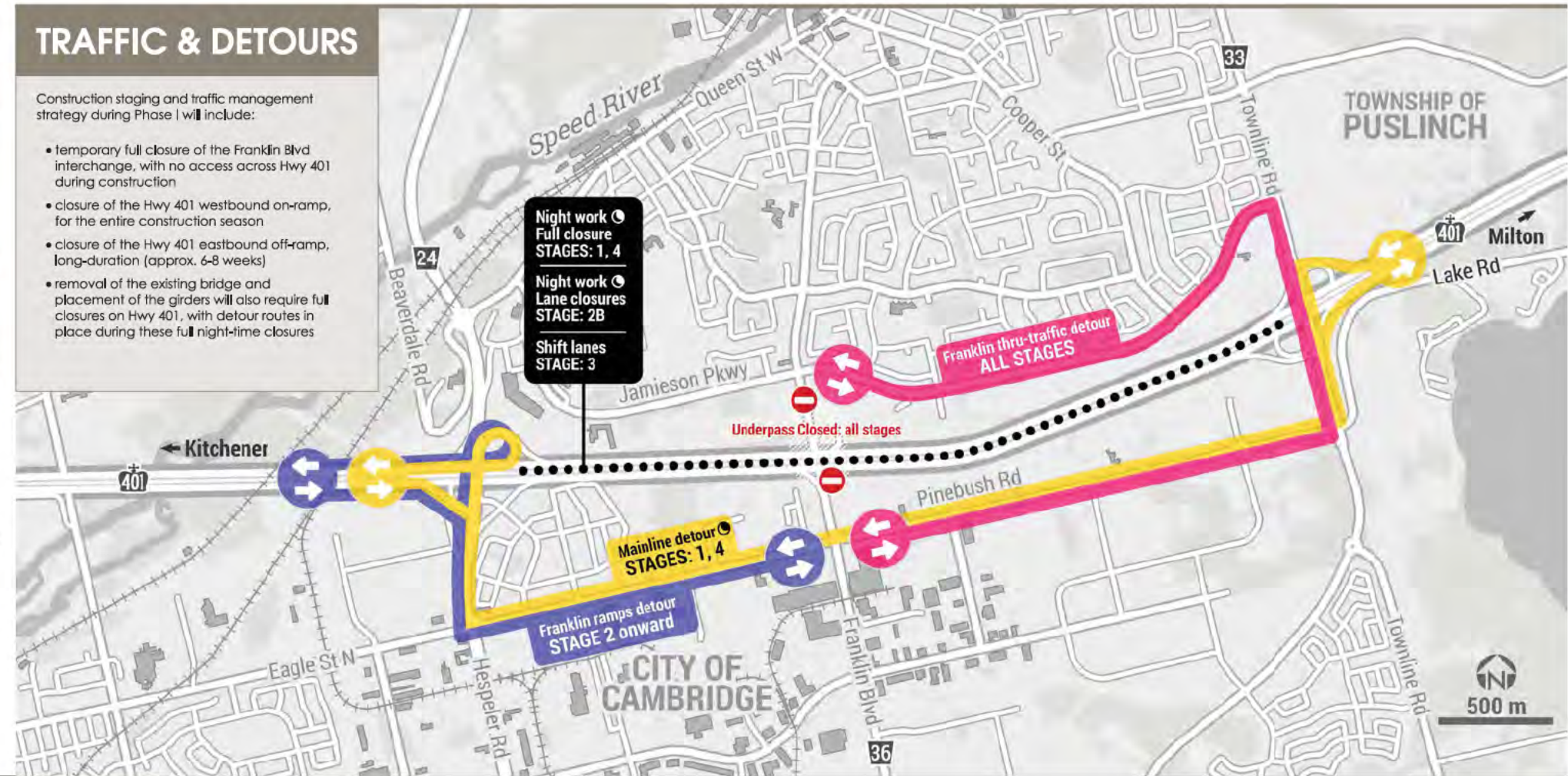
## APPROVED PLAN



## TRAFFIC & DETOURS

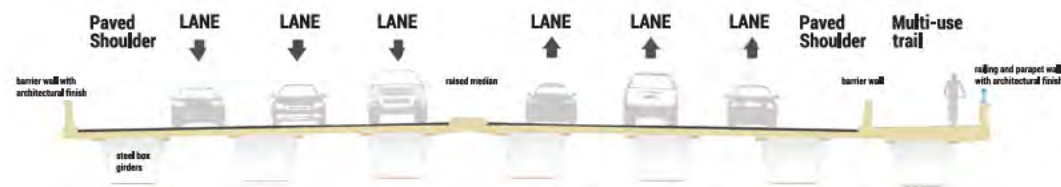
Construction staging and traffic management strategy during Phase I will include:

- temporary full closure of the Franklin Blvd interchange, with no access across Hwy 401 during construction
- closure of the Hwy 401 westbound on-ramp, for the entire construction season
- closure of the Hwy 401 eastbound off-ramp, long-duration (approx. 6-8 weeks)
- removal of the existing bridge and placement of the girders will also require full closures on Hwy 401, with detour routes in place during these full night-time closures



# Phase II: Mainline

## APPROVED PLAN

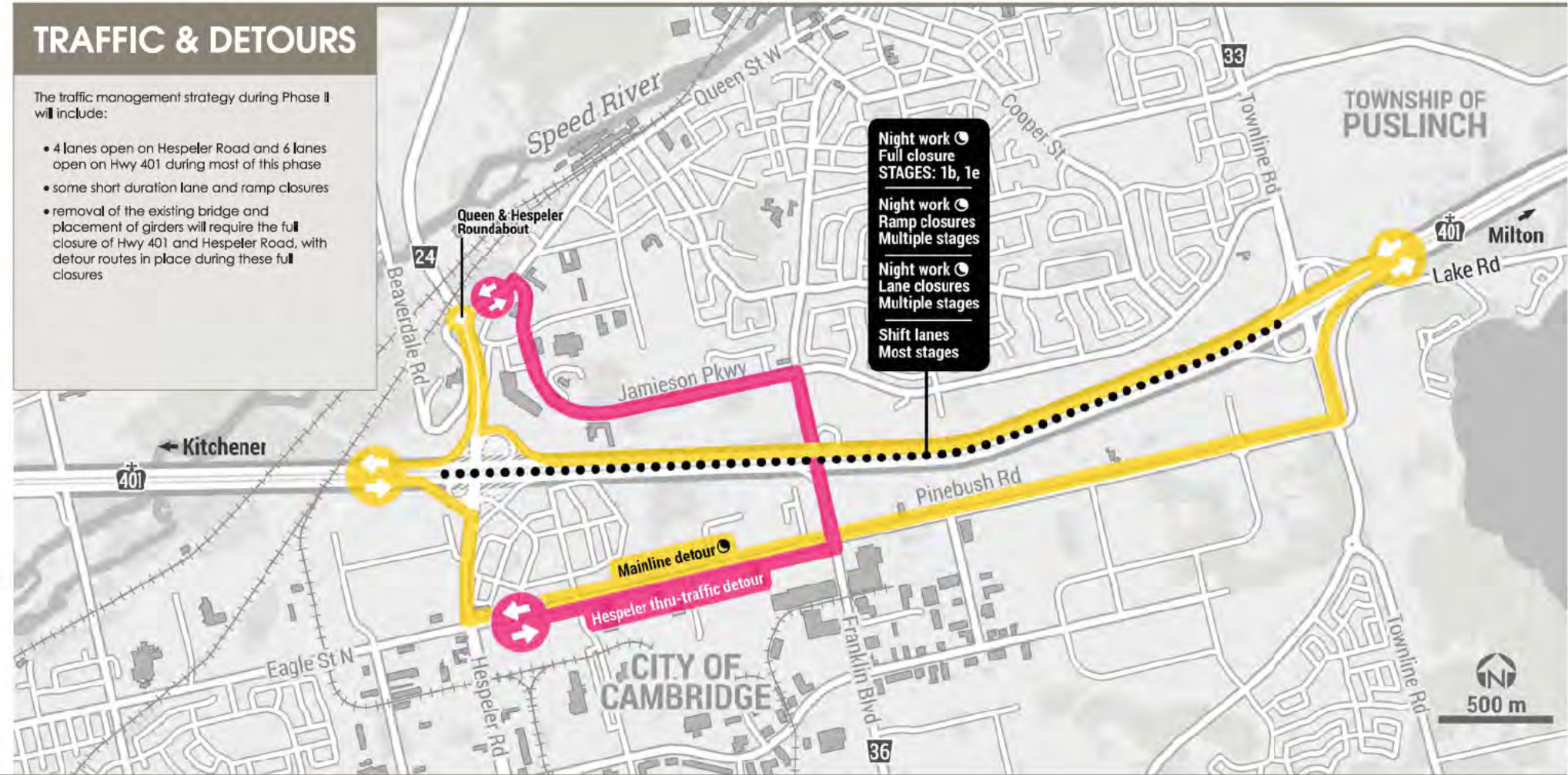


Cross-section of new bridge at Hespeler Rd

## TRAFFIC & DETOURS

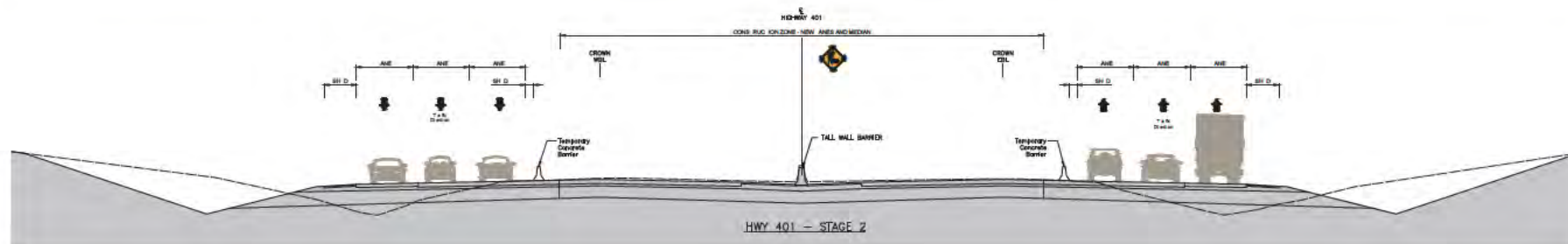
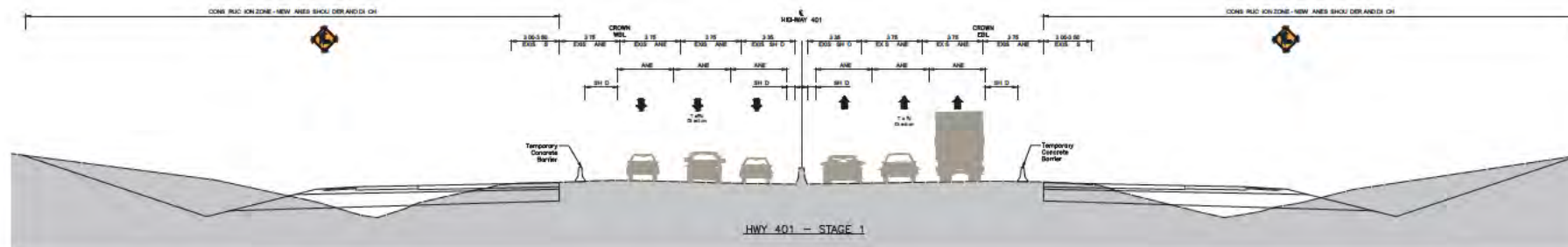
The traffic management strategy during Phase II will include:

- 4 lanes open on Hespeler Road and 6 lanes open on Hwy 401 during most of this phase
- some short duration lane and ramp closures
- removal of the existing bridge and placement of girders will require the full closure of Hwy 401 and Hespeler Road, with detour routes in place during these full closures



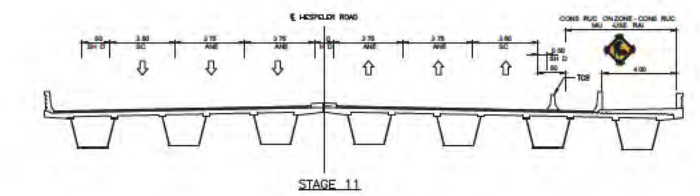
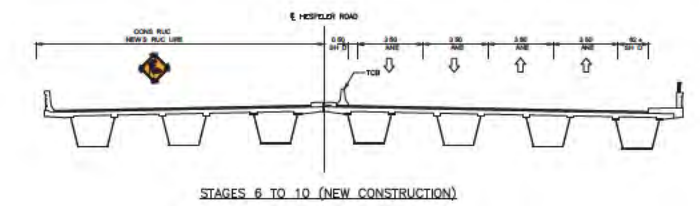
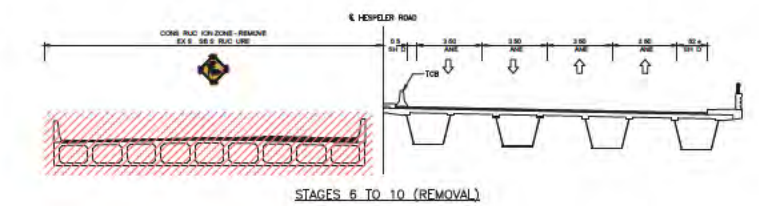
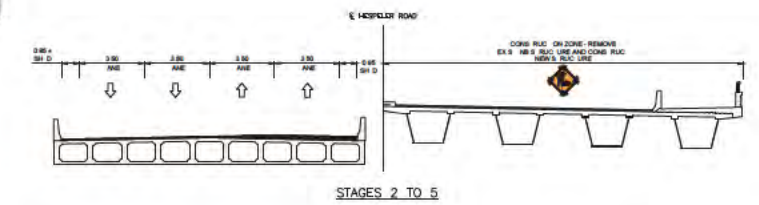
# Construction Staging

## Mainline Staging



## Hespeler Underpass Staging

- Highway 40 and Hespeler Road full closures and detours required during bridge demolition
- Maintain 2 lanes in each direction on Hespeler during most of construction. Some additional off peak lane closures may be required
- Long duration (approx 4.5 months) closure of Mall entrance for intersection reconstruction due to grade raise
- Short duration ramp closures will be required for pavement transitions to Hespeler Road and Highway 40
- Pedestrian access across Highway 40 on Hespeler will be maintained during construction



# Thank you for attending

***Your input is important!***

To provide your comments,  
please fill out a comment sheet and place it in the comment box  
at today's meeting, or send your comments to:

**Mr Kevin Welker, P.Eng., ENV SP**

Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
☎ tel: 519-675-6652 (+0 collect)  
📠 fax: 519-645-6575

**Mr Scott Howard**

Senior Project Manager  
Ministry of Transportation  
659 Exeter Road, 3rd Floor  
London, ON N6E 1L3  
☎ tel: 519-873-4568, or  
toll-free 1-800-265-6072  
ext. 519-873-4568  
📠 fax: 519-873-4600

✉ **email:** [comments@highway401-hespeler-townline.ca](mailto:comments@highway401-hespeler-townline.ca)

*We would appreciate receiving your comments by  
November 22, 2017.*

visit the project website:

[http:// \*\*highway401-hespeler-townline.ca\*\*](http://highway401-hespeler-townline.ca)

















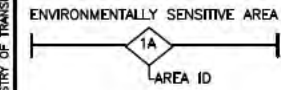
# Detail Design Plan



# Appendix C

**SUPPLEMENTAL LEGEND:**

-  RIP RAP OPSD 810.010 TYPE 'B' (SEE DETAIL)
-  GORE AREA (SEE DETAIL)
-  HOT MIX ASPHALT MISCELLANEOUS
-  SOD
-  FIBRE TUBE FLOW CHECK OPSD 219.191
-  TEMPORARY ROCK FLOW CHECK DAM OPSD 219.210, OPSD 219.211
-  BARRIER FOR TREE PROTECTION OPSD 220.010
-  HEAVY DUTY WIRE BACKED SILT FENCE OPSD 219.131
-  CB TO BE ADJUSTED
-  EROSION AND SEDIMENT MEASURES INSTALLED AT NEW CB/DI (SEE DETAIL)



**NOTES:**

1. DRAWINGS TO BE READ IN CONJUNCTION WITH:
  - OPSD 100 & 200 SERIES
  - TYPICALS AND DETAILS
2. ALL DISTURBED AREAS TO BE REINSTATED WITH TOPSOIL, SEED AND MATRIX
3. ALL SUBDRAINS AND SUBDRAIN OUTLETS ARE TO BE INSTALLED AS PER SUBDRAIN DETAILS
4. SILT FENCE TO BE INSTALLED AT RIP RAP (SEWER/CULVERT OUTLET) AND WATERCOURSES AS PER DETAILS
5. FOR CURB AND GUTTER, GUIDE RAIL, AND PAVEMENT DETAILS AT STRUCTURES SEE STRUCTURE APPROACH DETAIL
6. REINSTATEMENT RELATED TO CONSTRUCTION AND TRAFFIC STAGING IS SHOWN ON STAGING DRAWINGS
7. MEDIAN BARRIER WALL TRANSITIONS AS PER DETAILS

**ABBREVIATIONS:**

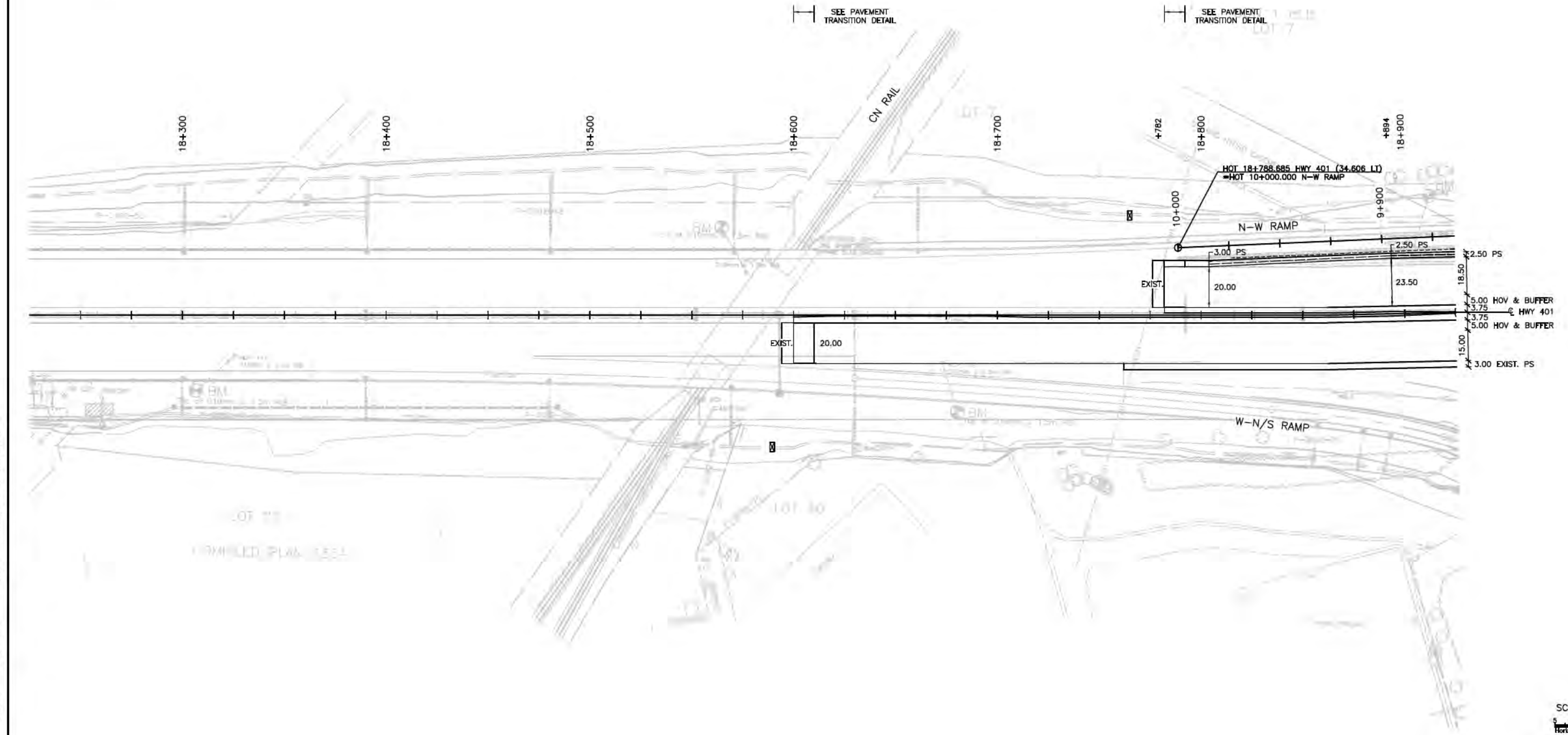
- EXIST. = EXISTING
- LTL = LEFT TURN LANE
- MED = MEDIAN
- PS = PAVED SHOULDER
- PPS = PARTIALLY PAVED SHOULDER
- RND = ROUNDING
- RTL = RIGHT TURN LANE
- SCL = SPEED CHANGE LANE
- WBSF = WIRE BACK SILT FENCE
- SBGR = STEEL BEAM GUIDERAIL, OPSD 912.185
- SBEAT = STEEL BEAM ENERGY ATTENUATING TERMINAL, TYPE MGS

**METRIC**  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No	CONT XXXX-XXXX	
	WP 3222-15-00	
<b>NEW CONSTRUCTION</b>		<b>SHEET</b>
STA 18+225 TO STA 18+925		166
Survey JUNE 2004 Revised NOVEMBER 2018		



**NOT FOR CONSTRUCTION**



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MODIFIED: Wednesday, March 06, 2019 2:06:10 PM



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AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

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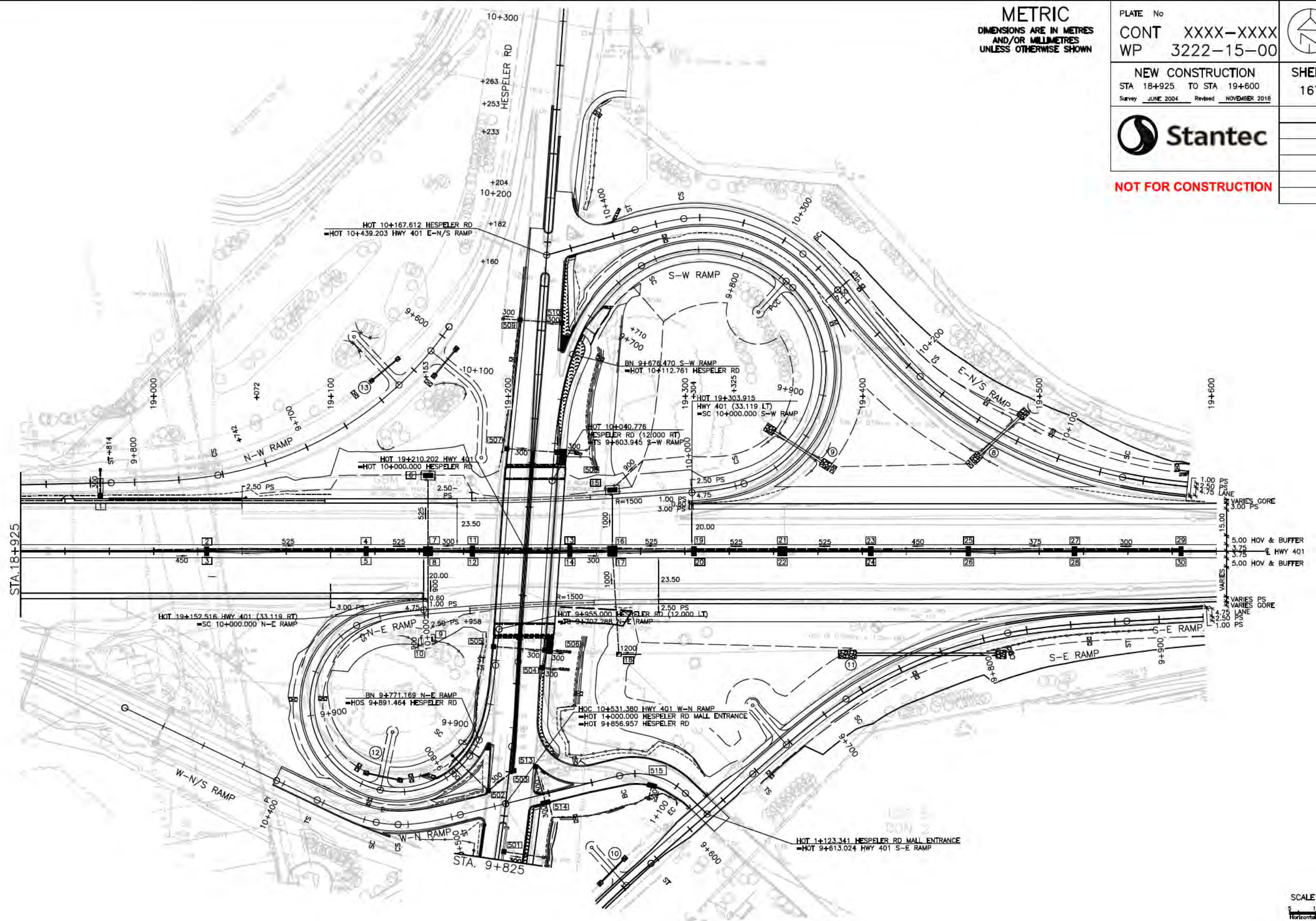


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Survey JUNE 2004 Revised NOVEMBER 2018

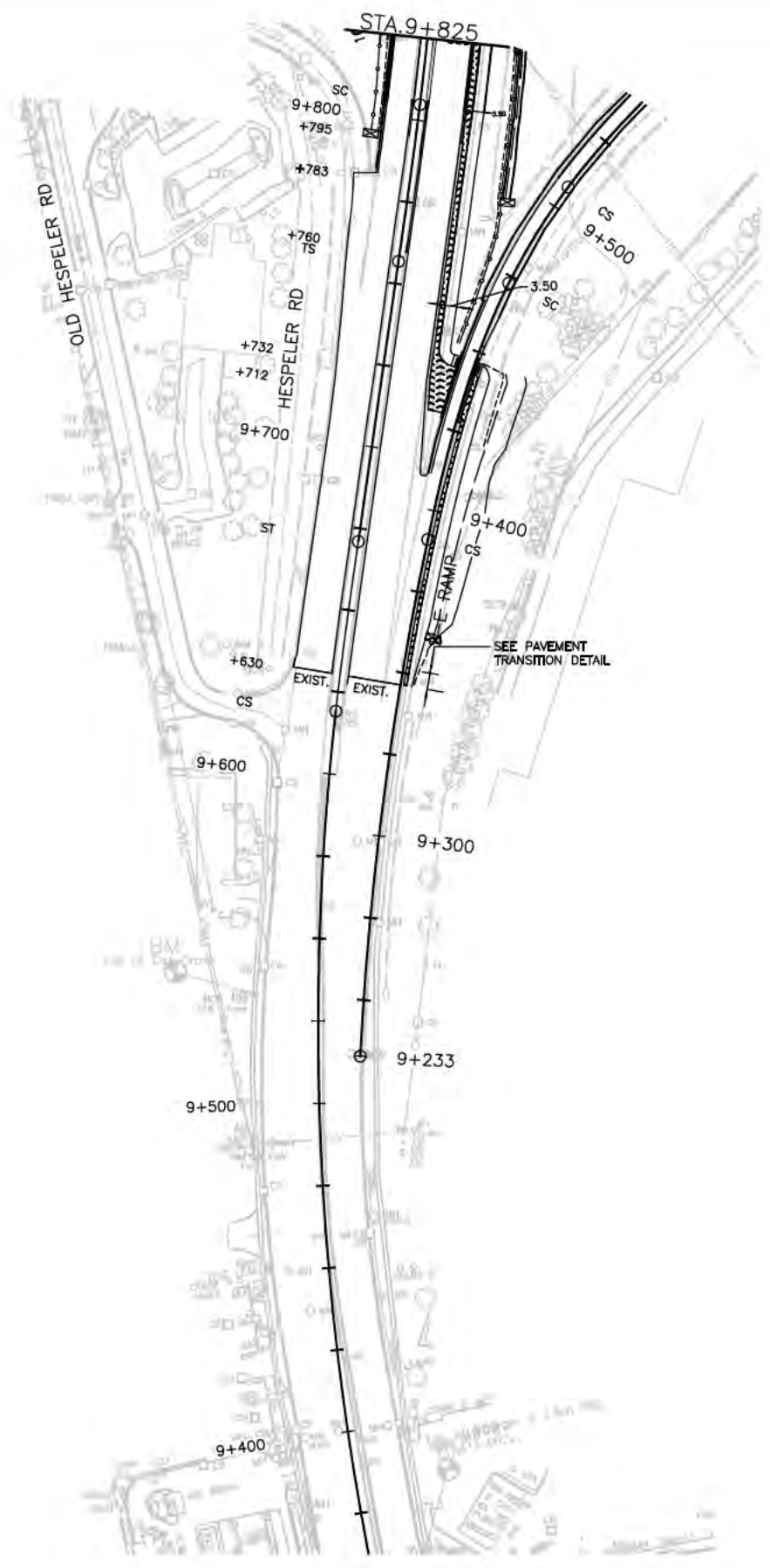
**SHEET**  
167



**NOT FOR CONSTRUCTION**



SCALE  
5 10  
Horizontal



METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No  
CONT XXXX-XXXX  
WP 3222-15-00



NEW CONSTRUCTION  
HESPELER RD

SHEET  
168



NOT FOR CONSTRUCTION

SCALE  
5 10  
Horizontal





METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No  
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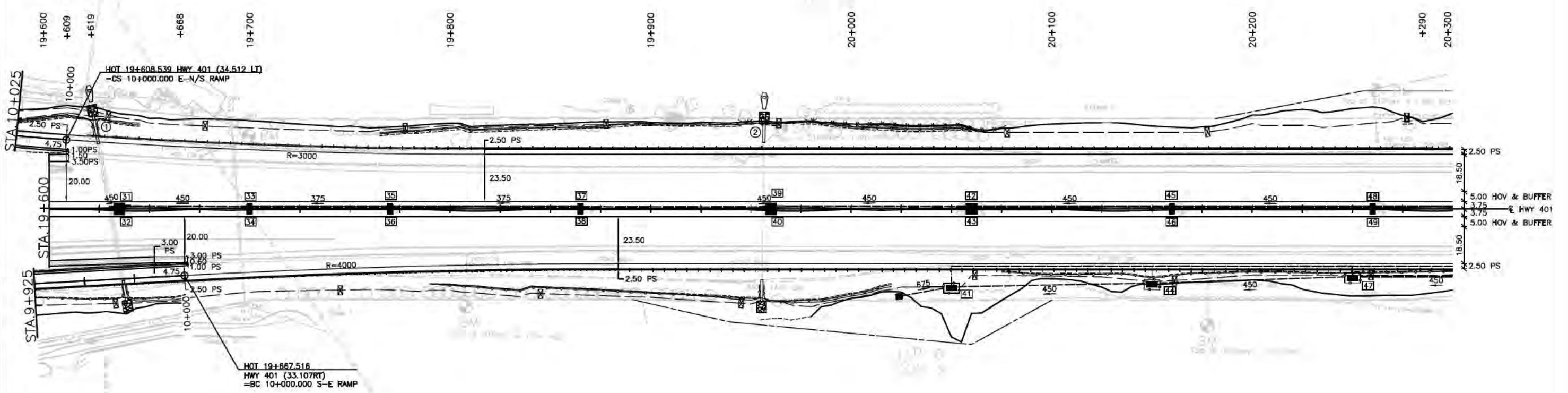


NEW CONSTRUCTION  
STA 19+600 TO STA 20+300  
Survey JUNE 2004 Revised NOVEMBER 2018

SHEET  
171



NOT FOR CONSTRUCTION



SCALE  
5 10  
Horizontal

**METRIC**  
 DIMENSIONS ARE IN METRES  
 AND/OR MILLIMETRES  
 UNLESS OTHERWISE SHOWN

PLATE No  
 CONT XXXX-XXXX  
 WP 3222-15-00

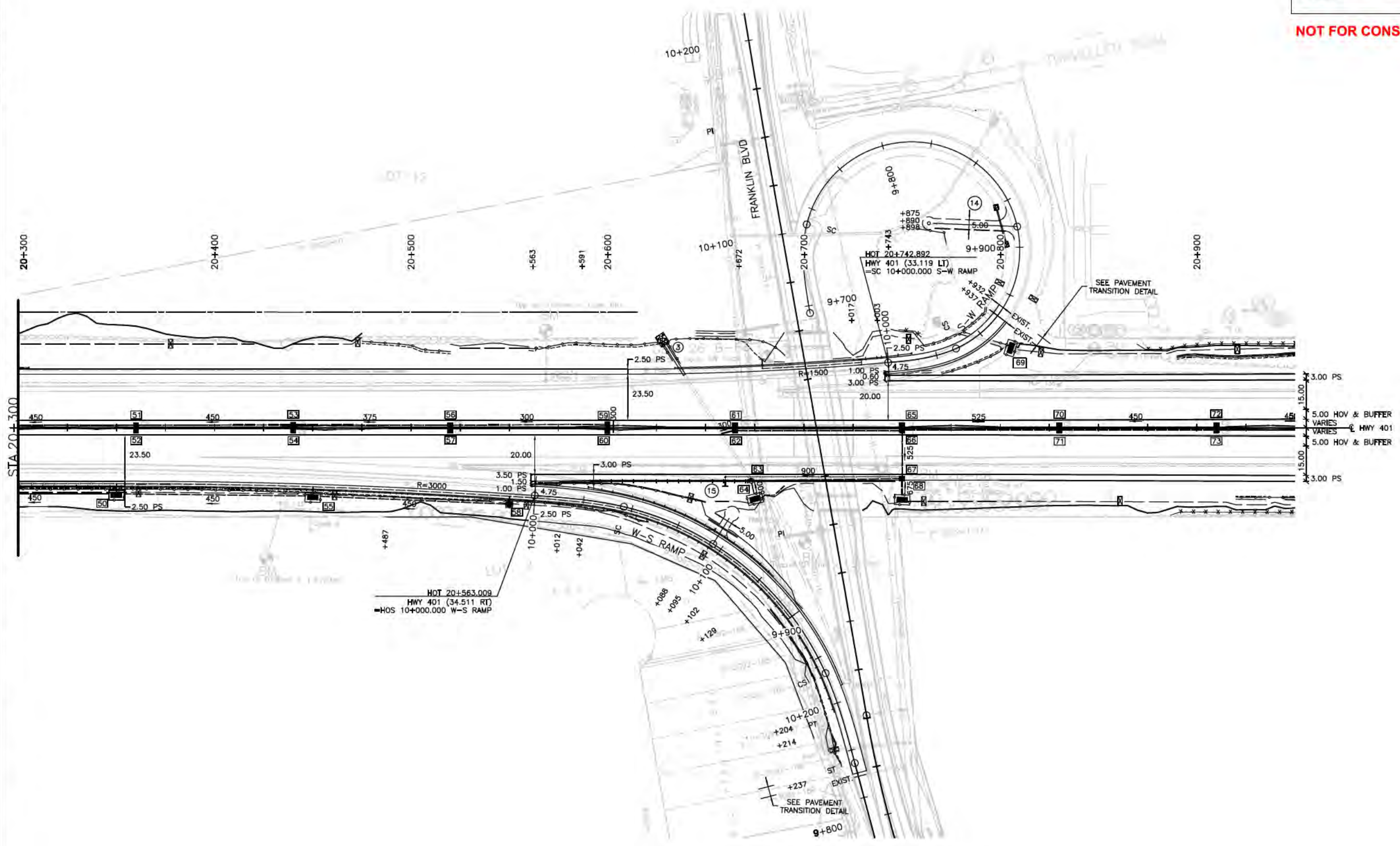


**NEW CONSTRUCTION**  
 STA 20+300 TO STA 20+950  
 Survey JUNE 2004 Revised NOVEMBER 2018

**SHEET**  
 172



**NOT FOR CONSTRUCTION**



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SCALE  
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AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No  
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WP 3222-15-00

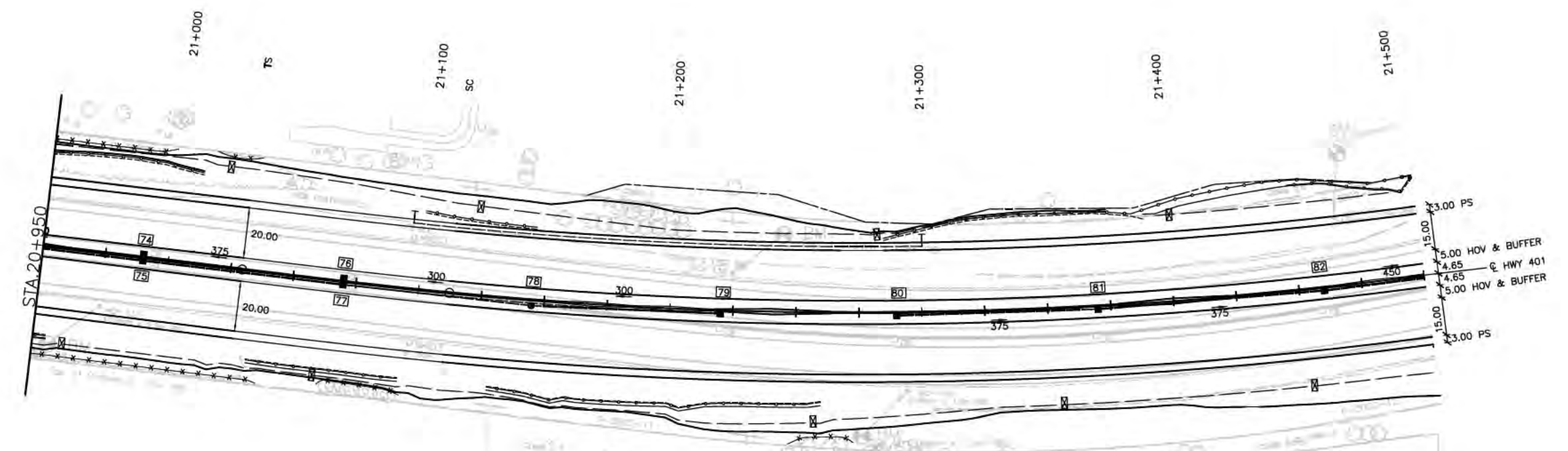


**NEW CONSTRUCTION**  
STA 20+950 TO STA 21+500  
Survey JUNE 2004 Revised NOVEMBER 2018

**SHEET**  
173



**NOT FOR CONSTRUCTION**



SCALE  
5 10  
Horizontal

METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

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WP 3222-15-00

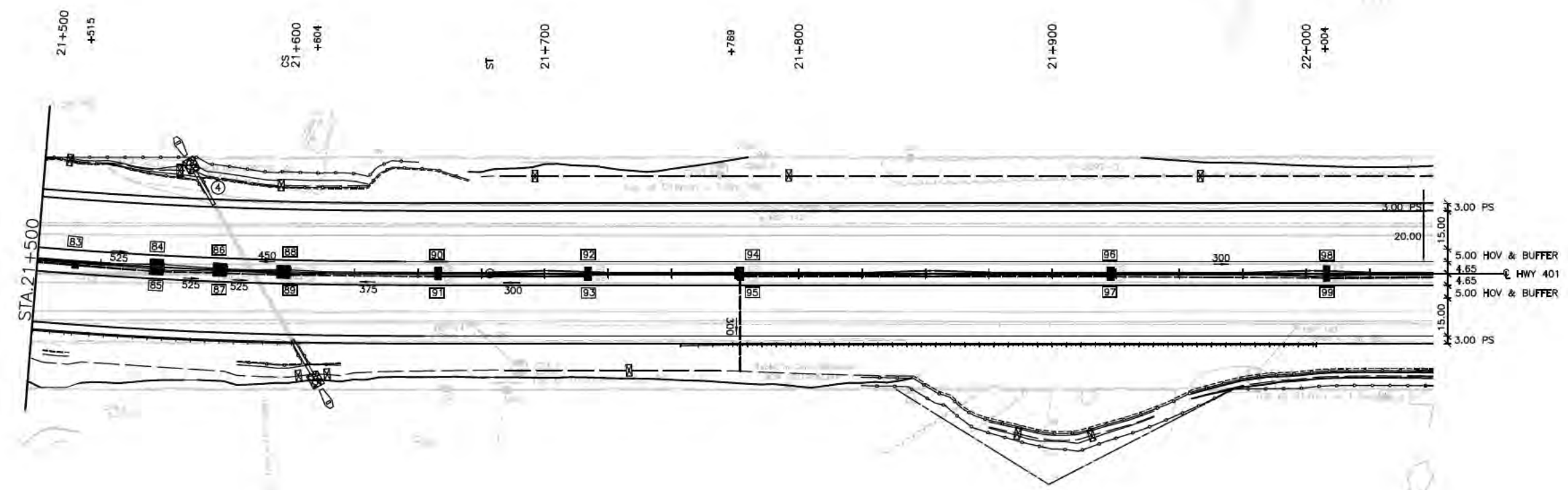


NEW CONSTRUCTION  
STA 21+500 TO STA 22+050  
Survey JUNE 2004 Revised NOVEMBER 2018

SHEET  
174



NOT FOR CONSTRUCTION



SCALE  
5 10  
Horizontal

METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No  
CONT XXXX-XXXX  
WP 3222-15-00



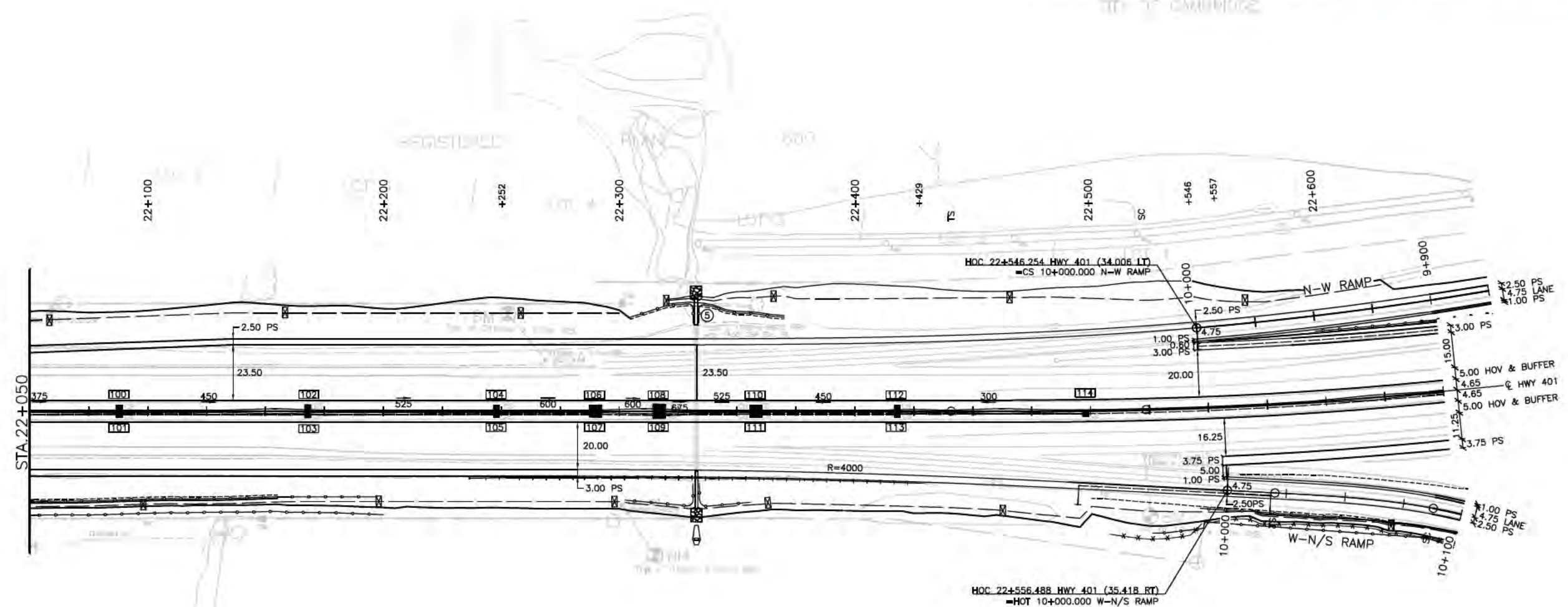
NEW CONSTRUCTION  
STA 22+050 TO STA 22+650  
Survey JUNE 2004 Revised NOVEMBER 2018

SHEET  
175



NOT FOR CONSTRUCTION

REGIONAL MUNICIPALITY OF WATERLOO  
CITY OF CAMBRIDGE



SCALE  
5 10  
Horizontal

**METRIC**  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

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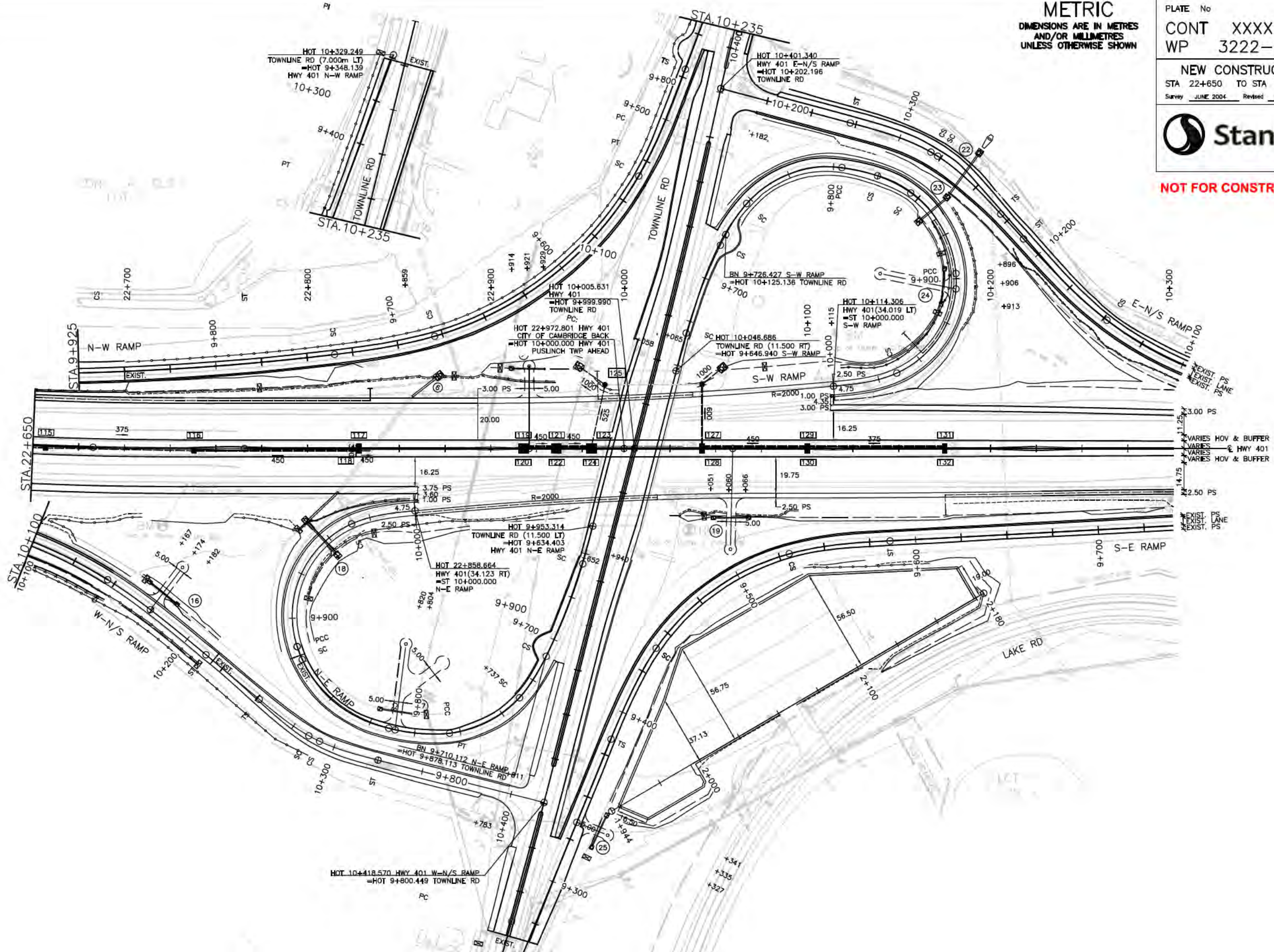


**NEW CONSTRUCTION**  
STA 22+650 TO STA 10+300  
Survey JUNE 2004 Revised NOVEMBER 2018

**SHEET**  
176



**NOT FOR CONSTRUCTION**



SCALE  
5 10  
Horizontal

MINISTRY OF TRANSPORTATION, ONTARIO PR-D-707 88-09

METRIC  
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AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

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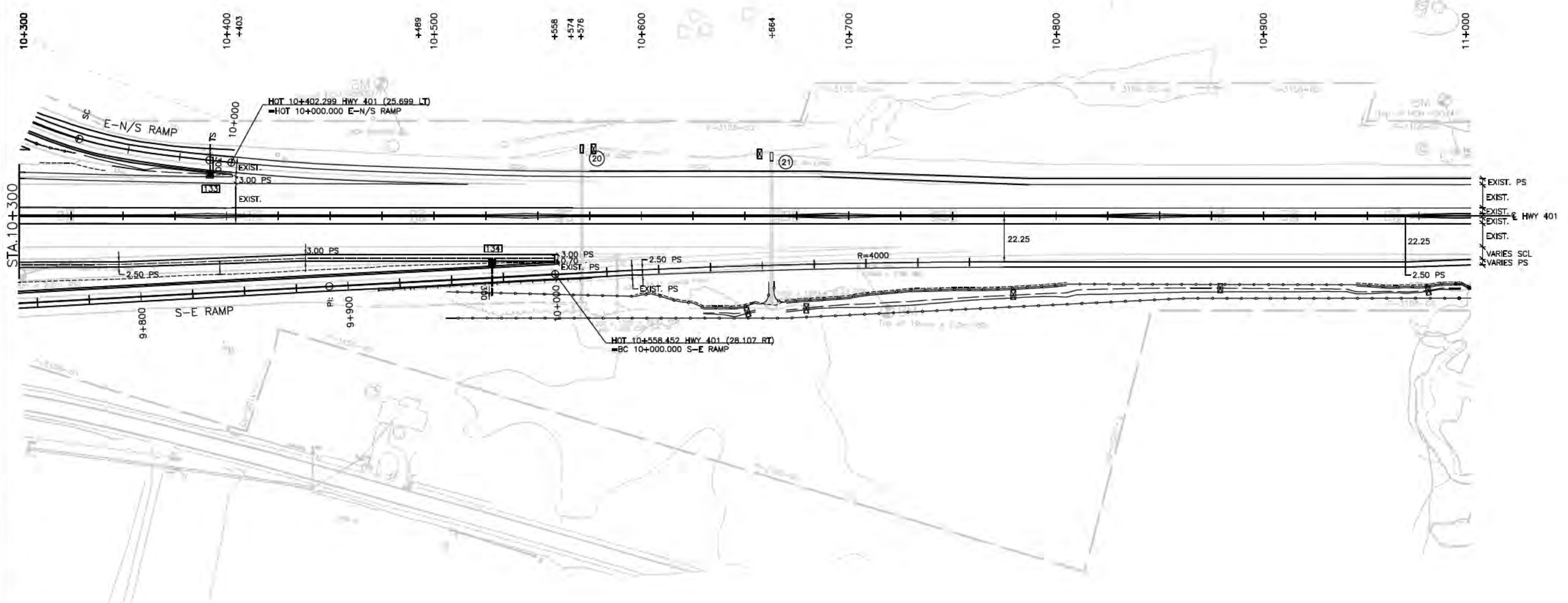
NEW CONSTRUCTION  
STA 10+300 TO STA 11+000  
Survey AUGUST 2009 Revised

SHEET  
177



NOT FOR CONSTRUCTION

ELLINGTON  
RD OF PUGHMILL



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MODIFIED: Wednesday, March 06, 2019 2:06:10 PM

SCALE  
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AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

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WP 3222-15-00

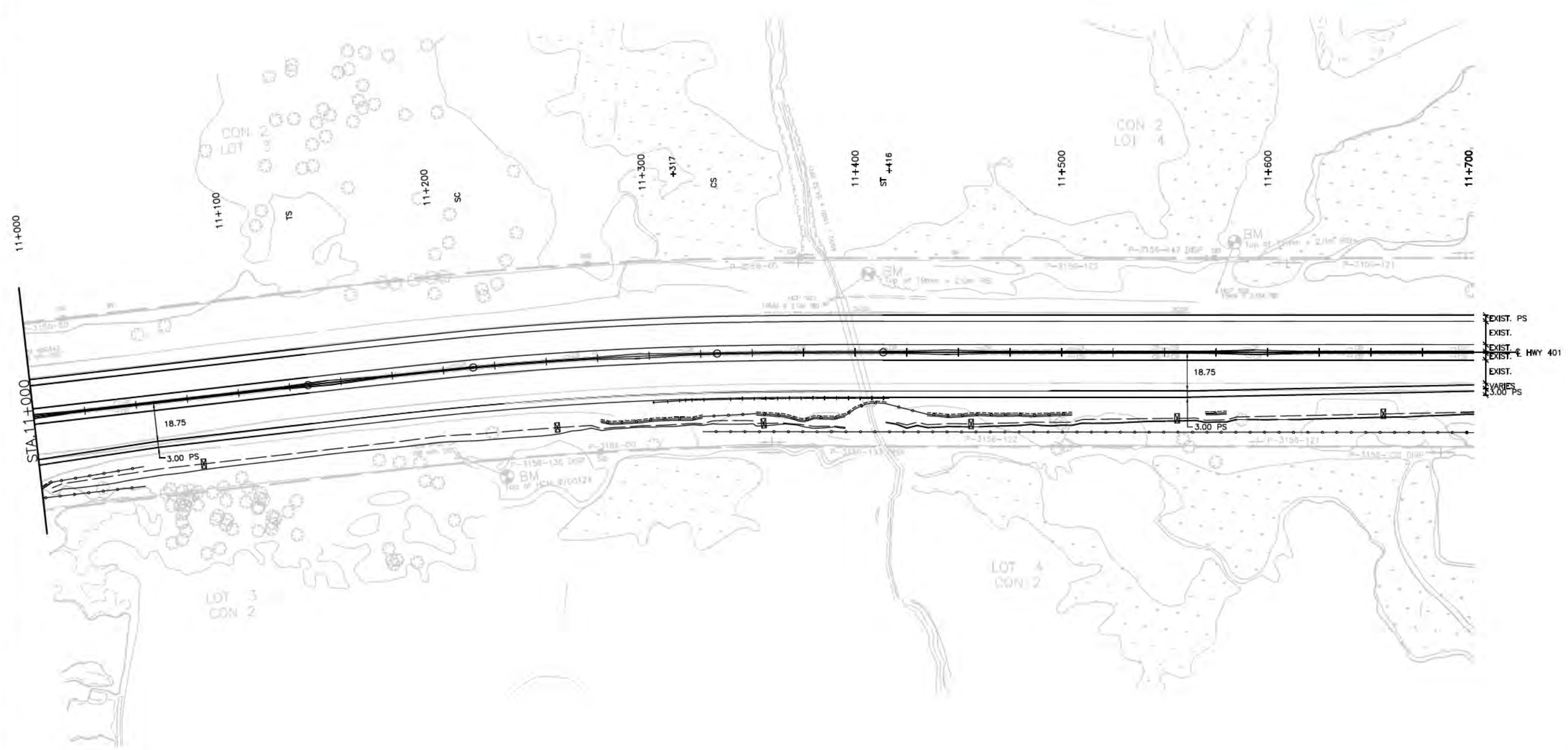


NEW CONSTRUCTION  
STA 11+000 TO STA 11+700  
Survey AUGUST 2009 Revised

SHEET  
178



NOT FOR CONSTRUCTION



METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

PLATE No  
CONT XXXX-XXXX  
WP 3222-15-00

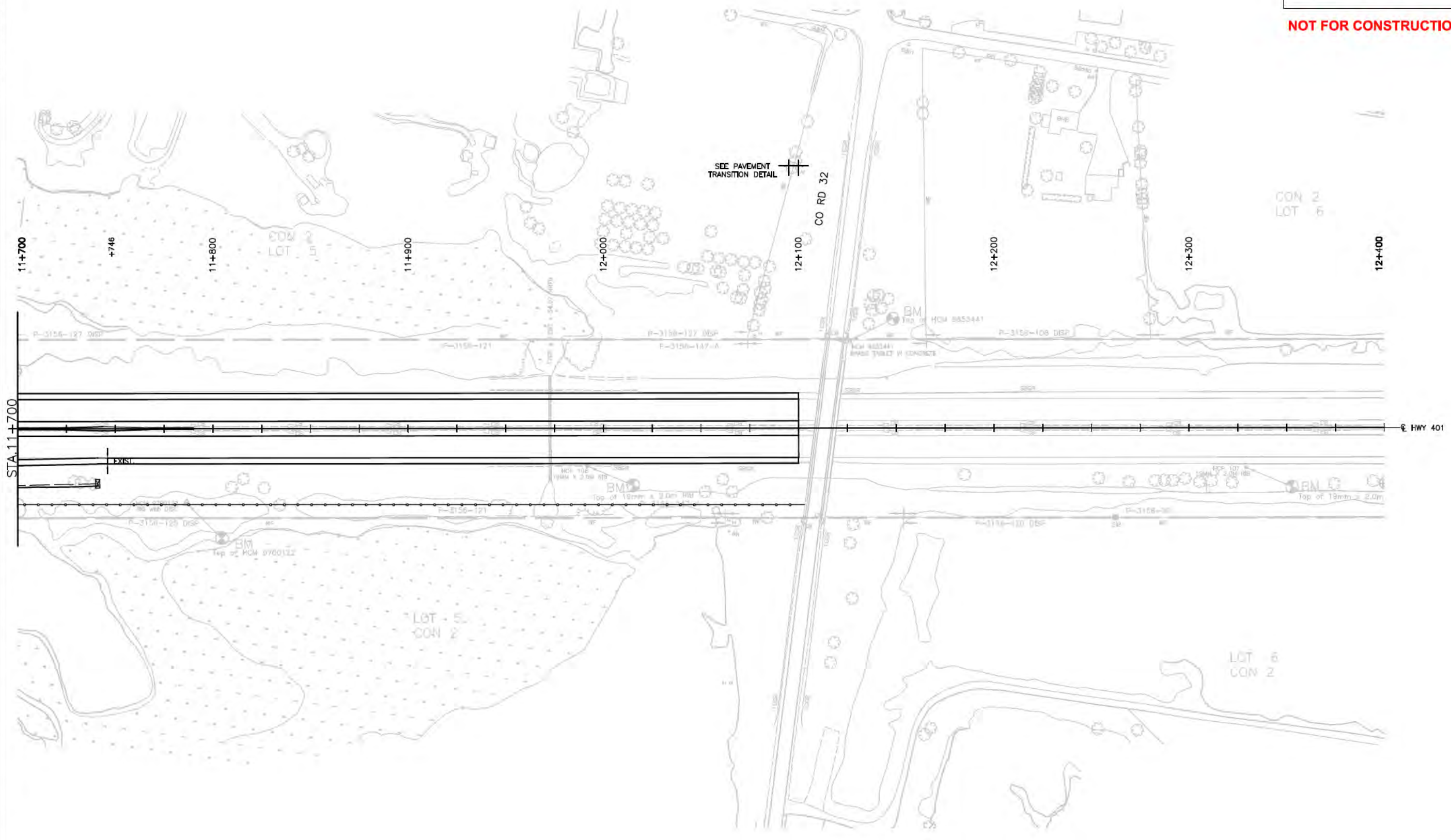


NEW CONSTRUCTION  
STA 11+700 TO STA 12+400  
Survey AUGUST 2009 Revised

SHEET  
179

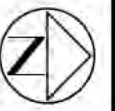


NOT FOR CONSTRUCTION



METRIC  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

HWY 401  
CONT  
WP 3222-15-01



WELLINGTON ROAD 32 UNDERPASS  
REHABILITATION  
GENERAL ARRANGEMENT

SHEET  
403

NOT FOR  
CONSTRUCTION



GENERAL NOTES

- CLASS OF CONCRETE:  
30 MPa UNLESS OTHERWISE SPECIFIED
- CLEAR COVER TO REINFORCING STEEL:  
PATCHES MATCH EXISTING  
REMAINDER 70±20 UNLESS OTHERWISE SPECIFIED
- REINFORCING STEEL:  
REINFORCING STEEL SHALL BE GRADE 400W UNLESS OTHERWISE SPECIFIED.  
TENSION LAP LENGTHS NOT INDICATED ON THE CONTRACT DRAWINGS SHALL BE CLASS B.  
BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS12-1, UNLESS INDICATED OTHERWISE.

CONSTRUCTION NOTES

- TYPICAL AREAS OF REPAIR ARE INDICATED ON DRAWINGS. WHERE REPAIR LIMITS ARE NOT SHOWN, LIMITS SHALL BE IDENTIFIED BY THE CONTRACT ADMINISTRATOR.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, DETAILS AND ELEVATIONS OF THE EXISTING STRUCTURE THAT ARE RELEVANT TO THE WORK SHOWN ON THE DRAWINGS PRIOR TO THE COMMENCEMENT OF THE WORK. ANY DISCREPANCIES SHALL BE REPORTED TO THE CONTRACT ADMINISTRATOR AND THE PROPOSED ADJUSTMENT OF THE WORK REQUIRED TO MATCH THE EXISTING STRUCTURE SHALL BE SUBMITTED FOR APPROVAL.
- EPOXY RESIN SHALL BE OBTAINED FROM DESIGNATED SOURCES FOR MATERIALS LIST DSM #9.30.25. HOLE DIA. AND RESIN INSTALLATION SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- SAWCUTS INTO CONCRETE SHALL BE 25mm DEEP OR TO FIRST LAYER OF REINFORCEMENT, WHICHEVER IS LESS, UNLESS NOTED OTHERWISE.
- ALL CONCRETE REPAIR SURFACES TO BE ABRASIVE BLAST CLEANED AND A BONDING AGENT SHALL BE APPLIED BEFORE PLACING NEW CONCRETE.
- ALL SAWCUT CONCRETE SURFACES AGAINST WHICH NEW CONCRETE IS PLACED SHALL BE ROUGHENED.
- ABRASIVE BLAST CLEAN ALL REINFORCING STEEL THAT IS TO BE RETAINED.
- ALL EXPOSED EDGES TO HAVE 20mm CHAMFER UNLESS OTHERWISE NOTED.
- DEBRIS FROM STRUCTURE REPAIRS SHALL BE PREVENTED FROM FALLING ONTO THE ROADWAY BELOW THE STRUCTURE.
- ANY DAMAGE TO THE EXISTING STRUCTURE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE CONTRACT ADMINISTRATOR AT NO COST TO THE OWNER.
- THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE ADEQUATE PROTECTION OF ALL UTILITIES, SERVICES, AND ROADWAYS DURING CONSTRUCTION OPERATIONS.
- VERTICAL CLEARANCE OVER LIVE LANES SHALL BE PROVIDED IN ACCORDANCE WITH OPSD-3390.150 AT ALL TIMES.

LIST OF DRAWINGS

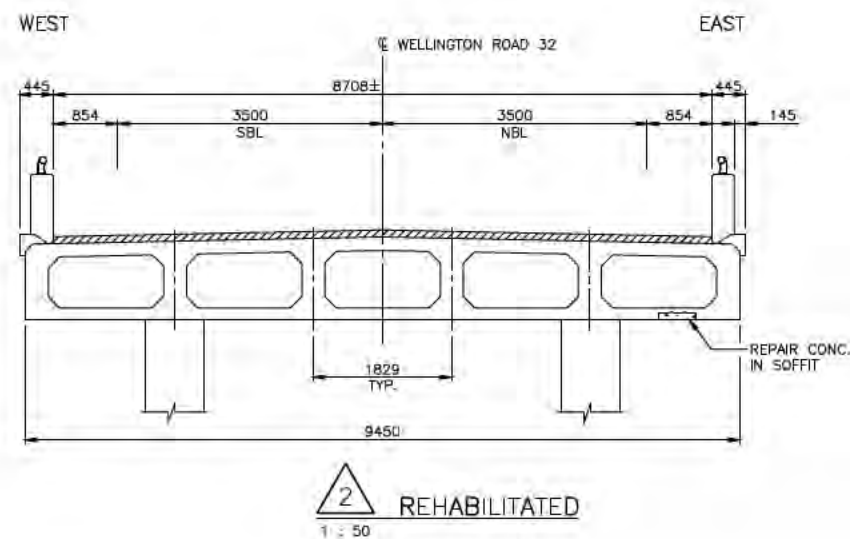
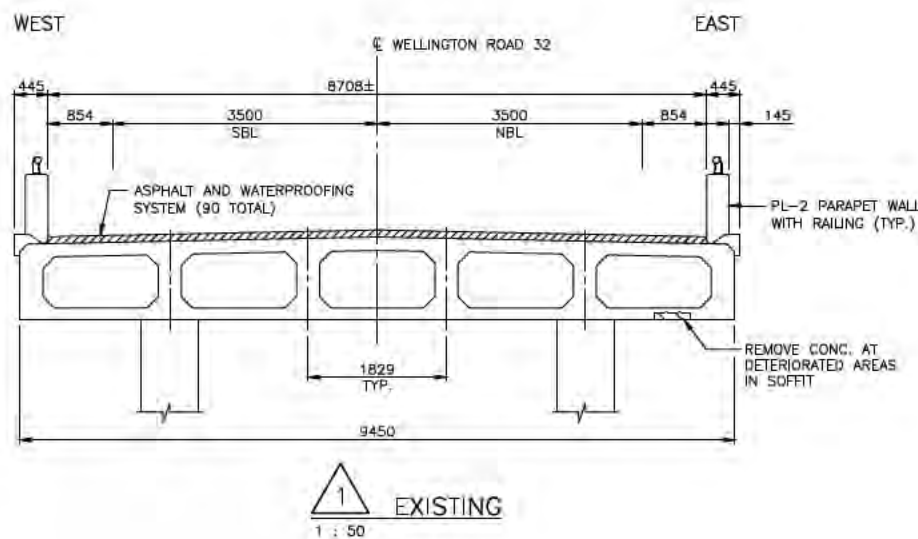
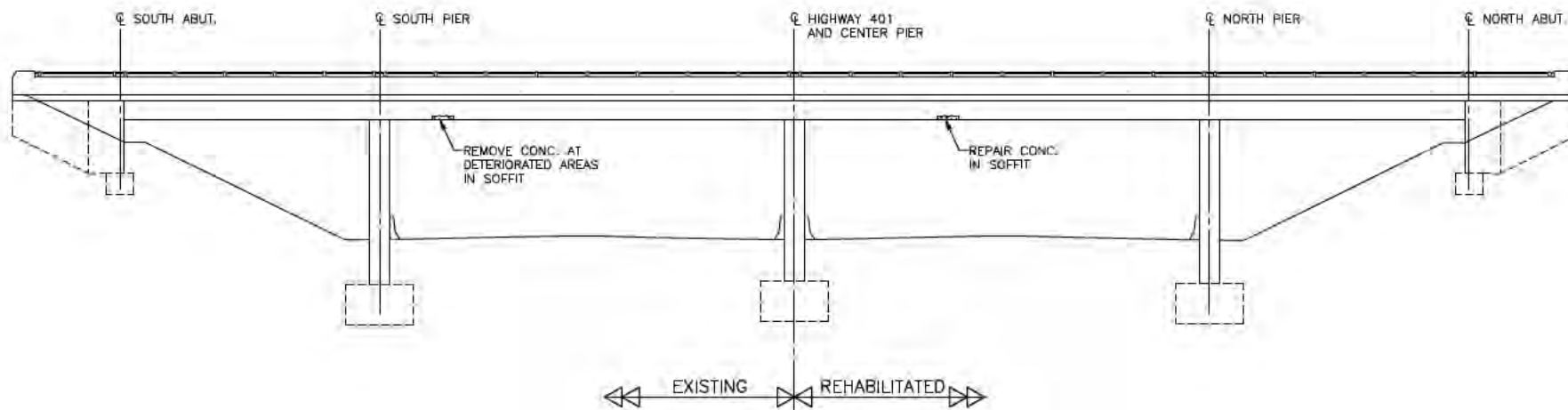
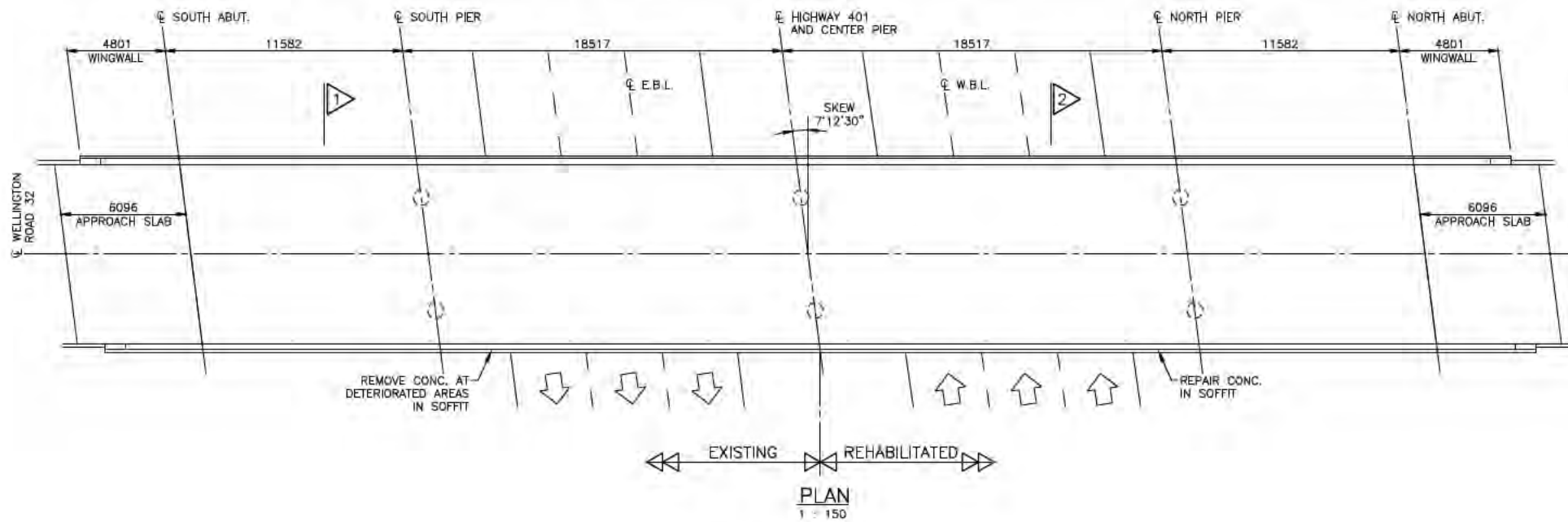
- R2-1. GENERAL ARRANGEMENT
- R2-2. CONSTRUCTION STAGING
- R2-3. CONCRETE REPAIR DETAILS

LEGEND

- TO BE REMOVED
- NEW CONCRETE

APPLICABLE STANDARD DRAWINGS

OPSD-3390.150 FLASHEWORK CLEARANCE TO TRAFFIC LANES



DRAWING NOT TO BE SCALED  
100 mm ON ORIGINAL DRAWING

REVISIONS	DATE	BY	DESCRIPTION

DESIGN M.C. CHK M.D. CODE CH8DC-2014 LOAD CL-825-ONT DATE MAR 2019  
DRAWN A.P. CHK M.D. SITE 35X-0361/80 STRUCT SCHEME DWG R2-1

165000997-35-361-01-GA.DWG, Mar. 8 2019



**METRIC**  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

HWY 401  
CONT  
WP 3222-15-02



TOWNLINER ROAD UNDERPASS  
REHABILITATION  
GENERAL ARRANGEMENT  
SHEET  
395

**NOT FOR  
CONSTRUCTION**



**GENERAL NOTES**

1. CLASS OF CONCRETE:  
30 MPa UNLESS OTHERWISE SPECIFIED
2. CLEAR COVER TO REINFORCING STEEL:  
PATCHES MATCH EXISTING  
REMAINDER 70±20 UNLESS OTHERWISE SPECIFIED
3. REINFORCING STEEL:  
REINFORCING STEEL SHALL BE GRADE 400W UNLESS OTHERWISE SPECIFIED.  
TENSION LAP LENGTHS NOT INDICATED ON THE CONTRACT DRAWINGS SHALL BE CLASS B.  
BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS12-1, UNLESS INDICATED OTHERWISE.

**CONSTRUCTION NOTES**

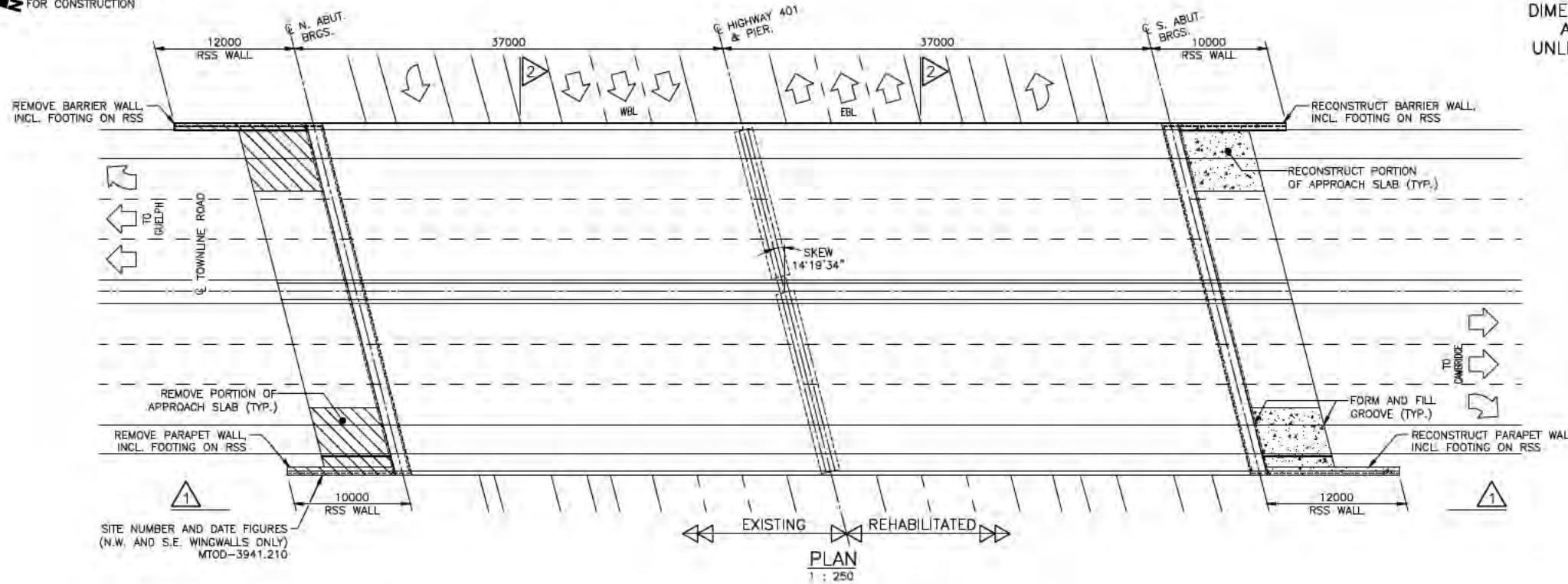
1. TYPICAL AREAS OF REPAIR ARE INDICATED ON DRAWINGS. WHERE REPAIR LIMITS ARE NOT SHOWN, LIMITS SHALL BE IDENTIFIED BY THE CONTRACT ADMINISTRATOR.
2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, DETAILS AND ELEVATIONS OF THE EXISTING STRUCTURE THAT ARE RELEVANT TO THE WORK SHOWN ON THE DRAWINGS PRIOR TO THE COMMENCEMENT OF THE WORK. ANY DISCREPANCIES SHALL BE REPORTED TO THE CONTRACT ADMINISTRATOR AND THE PROPOSED ADJUSTMENT OF THE WORK REQUIRED TO MATCH THE EXISTING STRUCTURE SHALL BE SUBMITTED FOR APPROVAL.
3. EPOXY RESIN SHALL BE OBTAINED FROM DESIGNATED SOURCES FOR MATERIALS LIST DSM #8.30.25. HOLE DIA. AND RESIN INSTALLATION SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
4. SAWCUTS INTO CONCRETE SHALL BE 25mm DEEP OR TO FIRST LAYER OF REINFORCEMENT, WHICHEVER IS LESS, UNLESS NOTED OTHERWISE.
5. ALL CONCRETE REPAIR SURFACES TO BE ABRASIVE BLAST CLEANED AND A BONDING AGENT SHALL BE APPLIED BEFORE PLACING NEW CONCRETE.
6. ALL SAWCUT CONCRETE SURFACES AGAINST WHICH NEW CONCRETE IS PLACED SHALL BE ROUGHENED.
7. ABRASIVE BLAST CLEAN ALL REINFORCING STEEL THAT IS TO BE RETAINED.
8. ALL EXPOSED EDGES TO HAVE 20mm CHAMFER UNLESS OTHERWISE NOTED.
9. DEBRIS FROM STRUCTURE REPAIRS SHALL BE PREVENTED FROM FALLING ONTO THE ROADWAY BELOW THE STRUCTURE.
10. ANY DAMAGE TO THE EXISTING STRUCTURE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE CONTRACT ADMINISTRATOR AT NO COST TO THE OWNER.
11. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE ADEQUATE PROTECTION OF ALL UTILITIES, SERVICES, AND ROADWAYS DURING CONSTRUCTION OPERATIONS.
12. EXISTING VERTICAL CLEARANCE OVER LIVE LANES SHALL BE MAINTAINED AT ALL TIMES.

**LIST OF DRAWINGS**

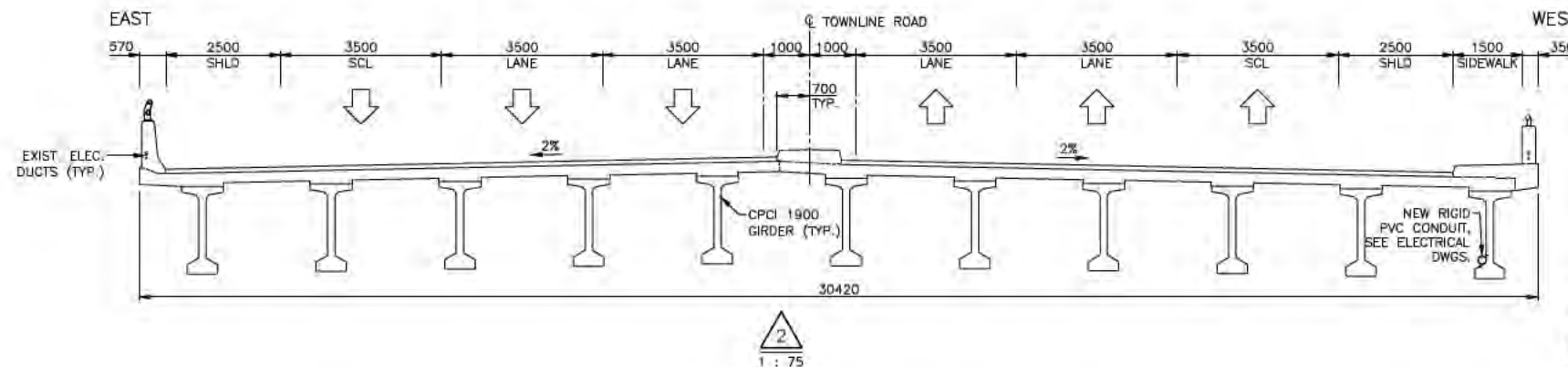
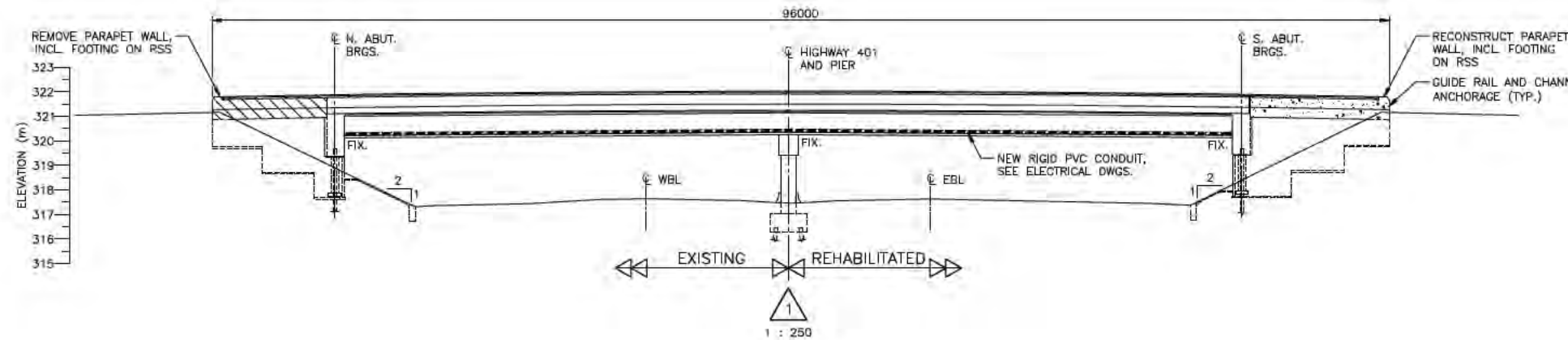
- R1-1. GENERAL ARRANGEMENT
- R1-2. CONSTRUCTION STAGING
- R1-3. REMOVAL AND REPAIR DETAILS
- R1-4. BARRIER WALL WITHOUT RAILING ON RSS WALL
- R1-5. PARAPET WALL WITH RAILING ON RSS WALL
- R1-6. APPROACH SLAB
- R1-7. MISCELLANEOUS DETAILS

**APPLICABLE STANDARD DRAWINGS**

- QPSD-3419.100 BARRIERS AND RAILINGS STEEL BEAM GUIDE RAIL AND CHANNEL ANCHORAGE
- OPSD-3370.100 DECK, WATERPROOFING HOT APPLIED ASPHALT MEMBRANE WITH PROTECTION BOARD
- OPSD-3370.101 DECK, WATERPROOFING HOT APPLIED ASPHALT MEMBRANE AT ACTIVE CRACKS GREATER THAN 2mm WIDE AND CONSTRUCTION JOINTS
- MTOD-3941.210 FIGURES IN CONCRETE, SITE NUMBER AND DATE LAYOUT



SITE NUMBER AND DATE FIGURES  
(N.W. AND S.E. CORNERS ONLY)  
MTOD-3941.210



**LEGEND**

- [Hatched pattern] TO BE REMOVED
- [Dotted pattern] NEW CONCRETE



DRAWING NOT TO BE SCALED  
100 mm ON ORIGINAL DRAWING

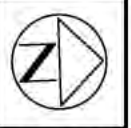
REVISIONS	DATE	BY	DESCRIPTION

DESIGN M.C. CHK M.D. CODE CHBDC-2008 [LOAD CL-625-0NT] DATE MAR 2019  
DRAWN A.P. CHK M.D. SITE 33X-0152/80 STRUCT SCHEME DWG. R1-1

185000897-33-152-R1-01.DWG Mar. 8, 2019

**METRIC**  
DIMENSIONS ARE IN METRES  
AND/OR MILLIMETRES  
UNLESS OTHERWISE SHOWN

HWY 401  
CONT  
WP 3060-11-01



HESPELER ROAD UNDERPASS  
REPLACEMENT  
GENERAL ARRANGEMENT

SHEET  
356

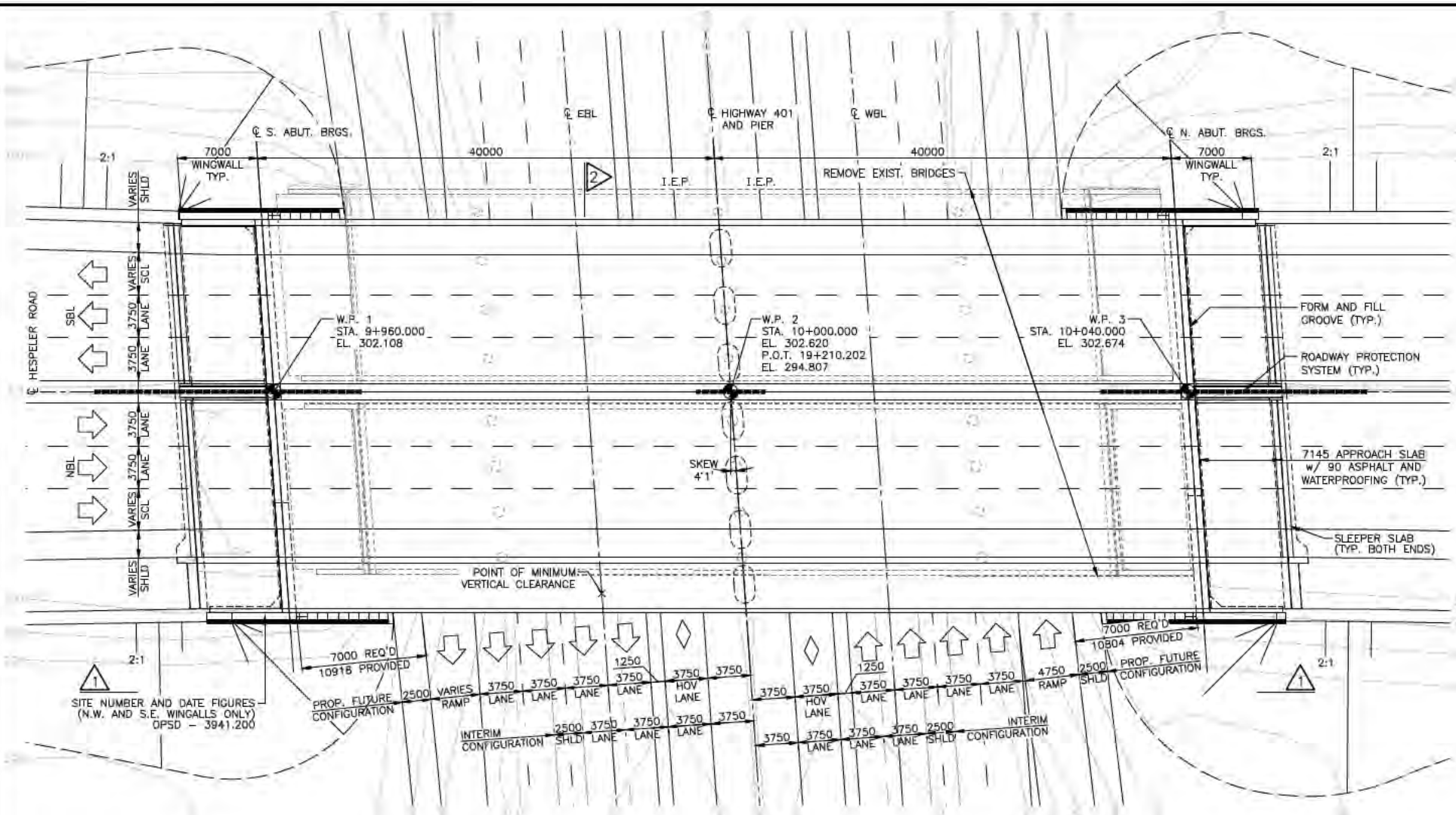


**GENERAL NOTES**

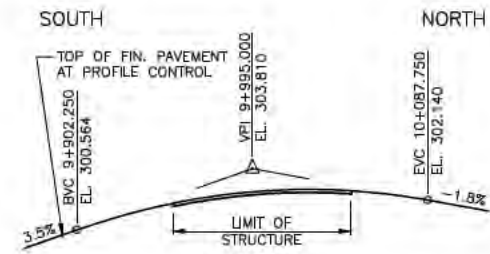
- CLASS OF CONCRETE:  
30 MPa UNLESS OTHERWISE NOTED.
- CLEAR COVER TO REINFORCING STEEL:  
CAISSONS 100±25  
DECK TOP 70±20  
BOTTOM 40±10  
REMAINDER 70±20 UNLESS OTHERWISE NOTED.
- REINFORCING STEEL:  
REINFORCING STEEL SHALL BE GRADE 400W UNLESS OTHERWISE SPECIFIED.  
STAINLESS REINFORCING STEEL SHALL BE TYPE 316LN, OR DUPLEX 2205 AND HAVE A MINIMUM YIELD STRENGTH OF 500 MPa.  
BAR MARKS WITH PREFIX 'S' DENOTE STAINLESS STEEL BARS.  
TENSION LAP LENGTHS NOT INDICATED ON THE CONTRACT DRAWINGS SHALL BE CLASS B.  
BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS12-1, UNLESS INDICATED OTHERWISE.
- CONSTRUCTION  
THE CONTRACTOR SHALL ESTABLISH THE BEARING SEAT ELEVATIONS BY DEDUCTING THE ACTUAL BEARING THICKNESSES FROM THE TOP OF BEARING ELEVATIONS. IF THE ACTUAL BEARING THICKNESSES ARE DIFFERENT FROM THOSE GIVEN WITH THE BEARING DESIGN DATA, THE CONTRACTOR SHALL ADJUST THE REINFORCING STEEL TO SUIT.  
BACKFILL BEHIND ABUTMENTS SHALL NOT BE PLACED UNTIL THE DECK HAS REACHED A STRENGTH OF 21 MPa.  
CONSTRUCT ABUTMENTS AND WINGWALLS TO THE BEARING SEAT ELEVATIONS. THE CONTRACTOR SHALL SUPPLY TEMPORARY LATERAL BRACING FOR THE ABUTMENTS. FORMWORK AND LATERAL BRACING SHALL NOT BE REMOVED UNTIL CONCRETE HAS REACHED A STRENGTH OF 21 MPa.  
BACKFILL SHALL BE PLACED SIMULTANEOUSLY BEHIND BOTH CONCRETE ABUTMENTS KEEPING THE HEIGHT OF THE BACKFILL APPROXIMATELY THE SAME. AT NO TIME SHALL THE DIFFERENCE IN ELEVATION BE GREATER THAN 500mm.  
ROADWAY PROTECTION SYSTEMS SHALL BE DESIGNED FOR PERFORMANCE LEVEL 2.

**LIST OF DRAWINGS**

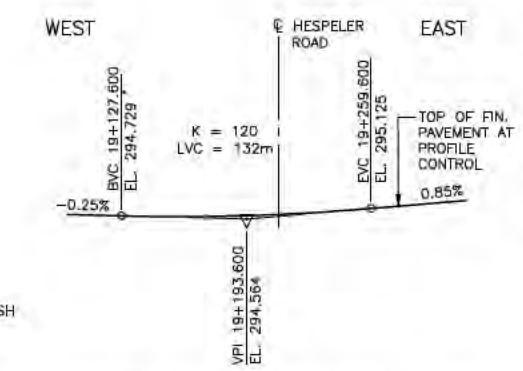
- GENERAL ARRANGEMENT
- BOREHOLE LOCATION AND SOIL STRATA
- CONSTRUCTION STAGING
- SUPERSTRUCTURE CONSTRUCTION STAGING
- FOUNDATION LAYOUT
- PIER FOUNDATION LAYOUT AND REINFORCING
- ABUTMENT LAYOUT
- ABUTMENT REINFORCING
- WINGWALLS
- PIER LAYOUT AND REINFORCING
- BEARINGS
- STRUCTURAL STEEL I
- STRUCTURAL STEEL II
- STRUCTURAL STEEL III
- STRUCTURAL STEEL IV
- DECK DETAILS AND SCREED ELEVATIONS I
- DECK DETAILS AND SCREED ELEVATIONS II
- PRECAST DECK PANELS I
- PRECAST DECK PANELS II
- DECK REINFORCING I
- DECK REINFORCING II
- BARRIER WALL
- SEPARATION WALL ON SIDEWALK
- PARAPET WALL ON SIDEWALK
- RAILING ON PARAPET
- APPROACH SLAB
- NORTH EXPANSION JOINT AND SLEEPER SLAB
- SOUTH EXPANSION JOINT AND SLEEPER SLAB
- EXPANSION JOINT DETAILS
- EXPANSION JOINT ASSEMBLY
- EXPANSION JOINT SEQUENCE OF INSTALLATION
- AESTHETIC DETAILS
- CONCRETE SLOPE PAVING
- AS CONSTRUCTED ELEVATIONS AND DIMENSIONS
- PILE DRIVING CONTROL
- MISCELLANEOUS DETAILS



PLAN  
1 : 250



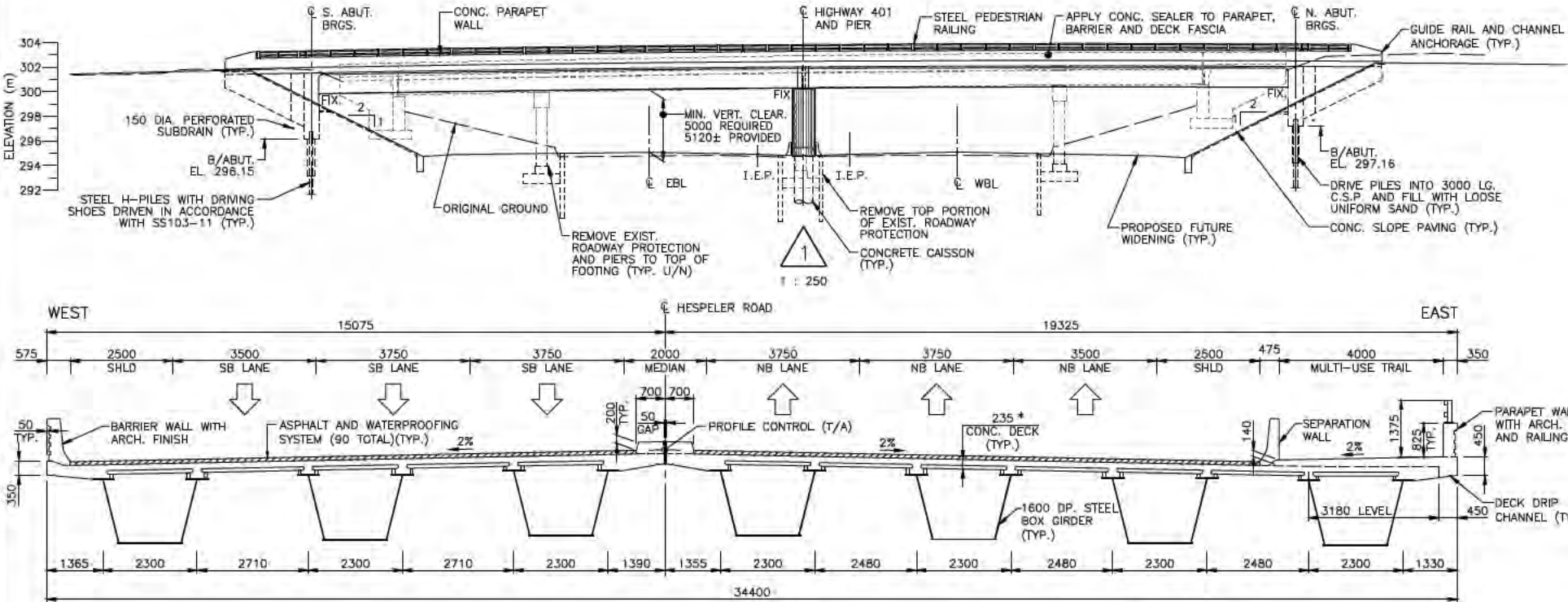
PROFILE OF HESPELER ROAD  
N.T.S.



PROFILE OF HIGHWAY 401 - I.E.P.  
N.T.S.

**NOT FOR CONSTRUCTION**

DRAWING NOT TO BE SCALED  
100 mm ON ORIGINAL DRAWING



2  
1 : 75

\*90 PRECAST PANELS + 145 OVERLAY

**LEGEND**

- W.P. DENOTES WORKING POINT
- P.O.T. DENOTES POINT ON TANGENT
- T/A DENOTES TOP OF ASPHALT
- I.E.P. DENOTES INSIDE EDGE OF PAVEMENT

**APPLICABLE STANDARD DRAWINGS**

- OPSD-912.430 GUIDE RAIL SYSTEM, STEEL BEAM STRUCTURE CONNECTION
- OPSD-3101.150 WALLS ABUTMENT, BACKFILL MINIMUM GRANULAR REQUIREMENT
- OPSD-3370.100 DECK, WATERPROOFING HOT APPLIED ASPHALT MEMBRANE WITH PROTECTION BOARD
- OPSD-3370.101 DECK, WATERPROOFING HOT APPLIED ASPHALT MEMBRANE AT ACTIVE CRACKS GREATER THAN 2mm WIDE AND CONSTRUCTION JOINTS
- OPSD-3390.100 DECK, DRIP CHANNEL
- OPSD-3419.100 BARRIERS AND RAILINGS STEEL GUIDE RAIL AND CHANNEL ANCHORAGE
- OPSD-3941.200 FIGURES IN CONCRETE, SITE NUMBER AND DATE LAYOUT
- OPSD-3950.100 JOINTS, CONCRETE EXPANSION & CONSTRUCTION ON STRUCTURE



REVISIONS	DATE	BY	DESCRIPTION

DESIGN M.C. CHK M.D. CODE CHRDC-2014 | LOAD CL-625-0NT | DATE MAR 2019  
DRAWN E.M. CHK M.D. SITE 33X-0150/80 STRUCT SCHEME DWG. 1

165000897-33-150-1.DWG - Mar 5, 2019



Design with community in mind